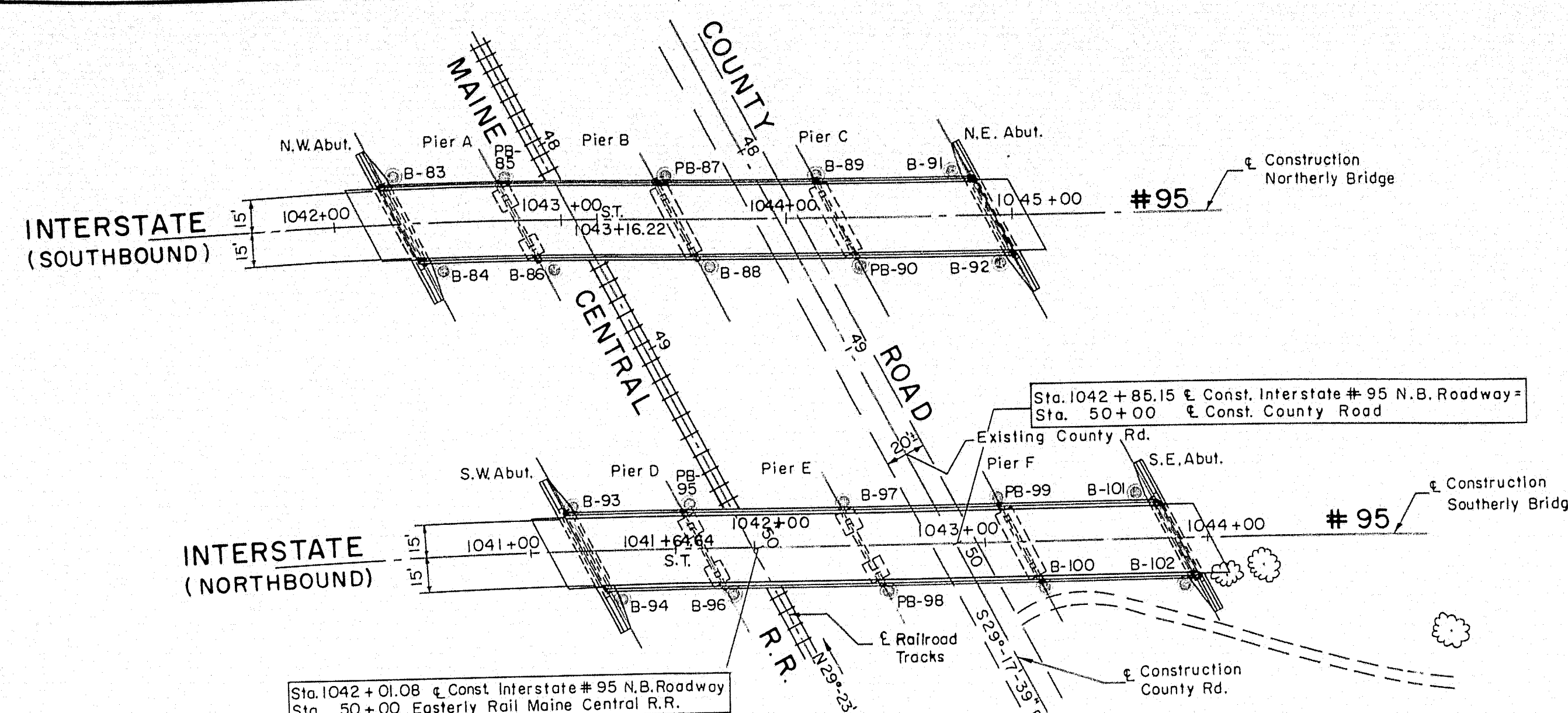
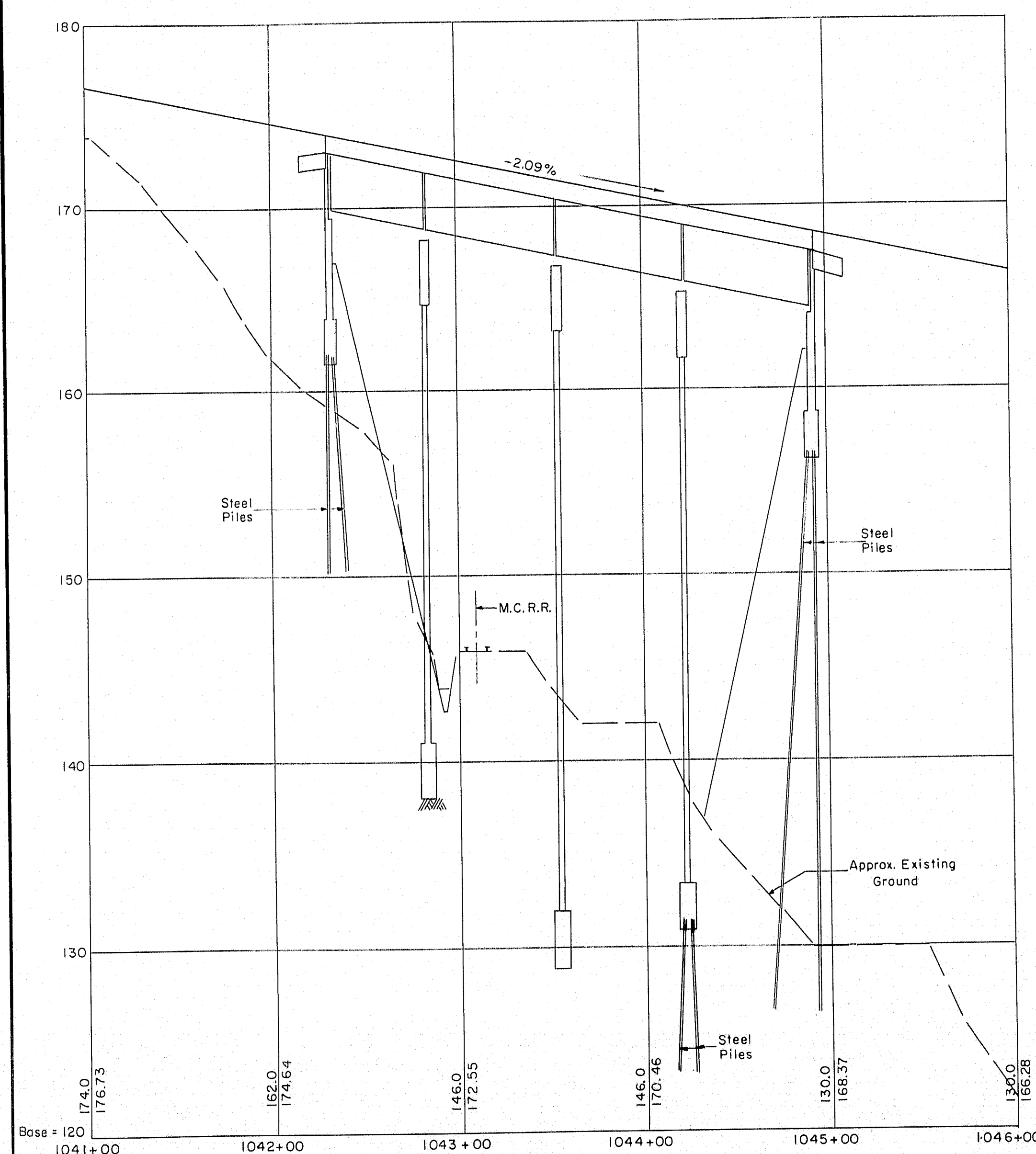


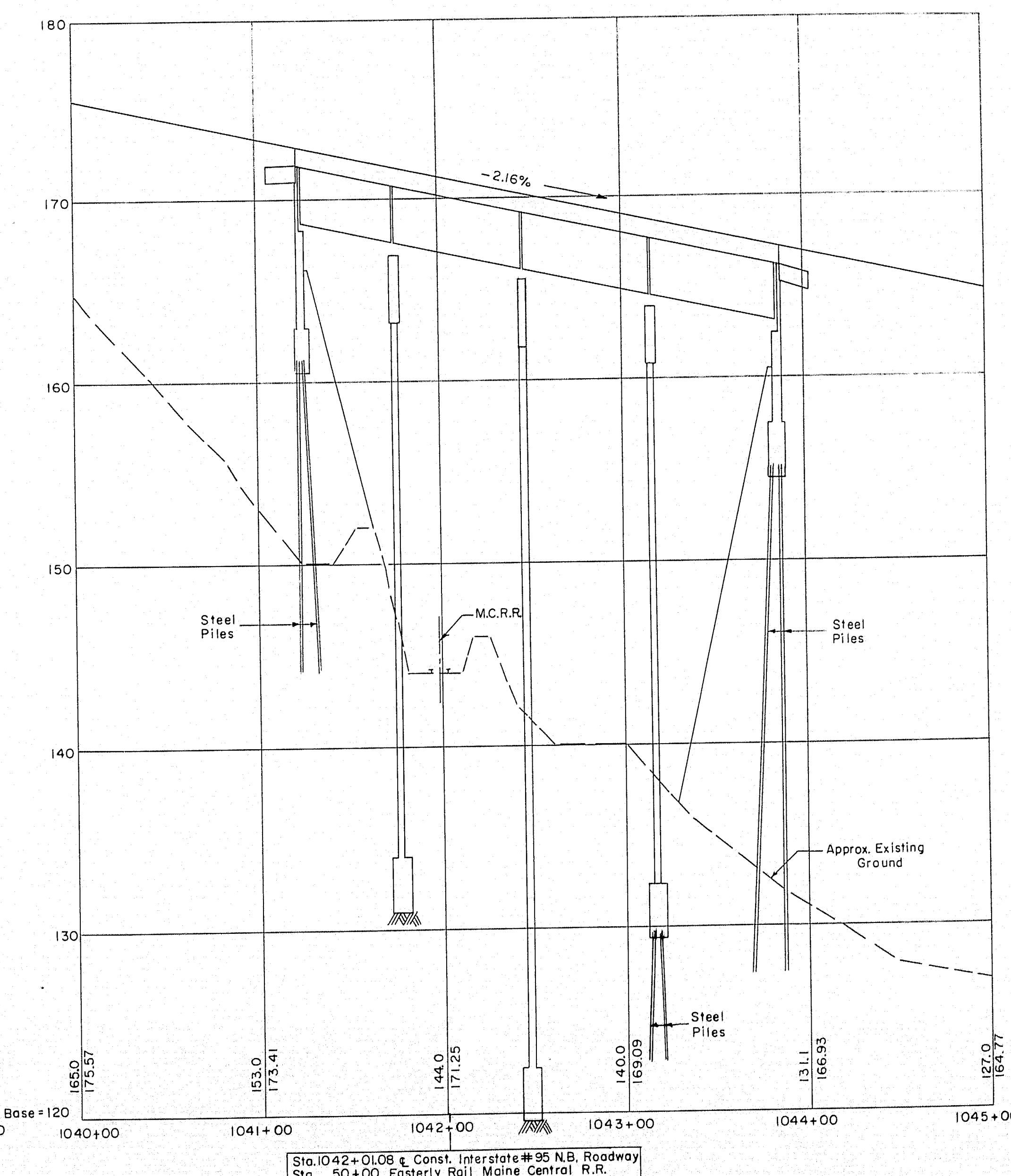
LOCATION MAP
Not to Scale



KEY PLAN
Scale: 1" = 50'-0"



PROFILE ALONG INTERSTATE #95 (NORTHERLY BRIDGE)
Scale: Hor. 1" = 50'
Vert. 1" = 5'



PROFILE ALONG INTERSTATE #95 (SOUTHERLY BRIDGE)
Scale: Hor. 1" = 50'
Vert. 1" = 5'

NOTE: 3-Ply Membrane Waterproofing and Bituminous Concrete Surfacing not in this contract.

APPROVED BY _____ DATE _____
THE CLARKSON ENGINEERING CO., INC.
CONSULTING ENGINEERS MASSACHUSETTS
BOSTON

GENERAL NOTES

FOUNDATION:
Foundations may be altered, if necessary, to suit conditions encountered in construction.

DESIGN:
In accordance with the Specifications of the American Association of State Highway Officials for H20-S16-44 loading (1957 Edition) modified for military requirements.
Design Stresses: Structural Steel $f_y = 18,000$ psi.
Reinforcing Steel $f_y = 18,000$ psi.
Concrete ($n=10$) $f_c = 1,200$ psi.

CONSTRUCTION:
State of Maine Standard Specifications to be followed except as noted in Special Provisions.

REINFORCEMENT:
All bars shall have deformations conforming to A.S.T.M. Designation A305.
Unless otherwise shown on plans, reinforcing bars shall be lapped 20 diameters to make a splice, except that main reinforcing bars near the top of slabs and beams having more than 12" of concrete under the bars shall be lapped 35 diameters to make a splice.

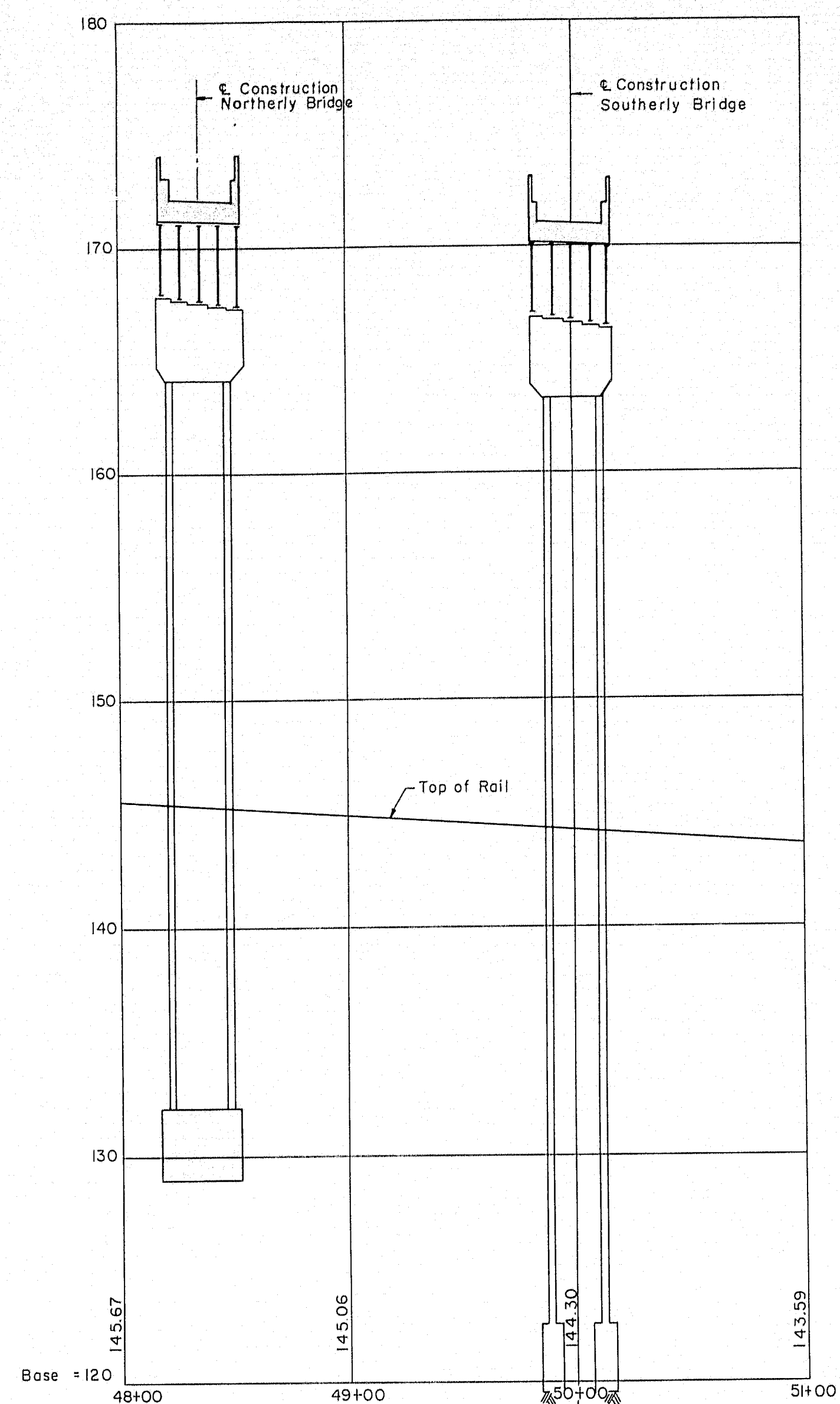
STRUCTURAL STEEL:
Wherever cover plates and / or shear connectors are welded to beams, beams and plates shall be weldable structural steel A.S.T.M. Designation A373.

BENCH MARK:
BM-46A Top of east rail opposite tel. pole with spike and shiner facing B.M.
Sta. 1042+10 (S.B.) 170' L.I.
Elevation 146.63 U.S.G.S. Datum

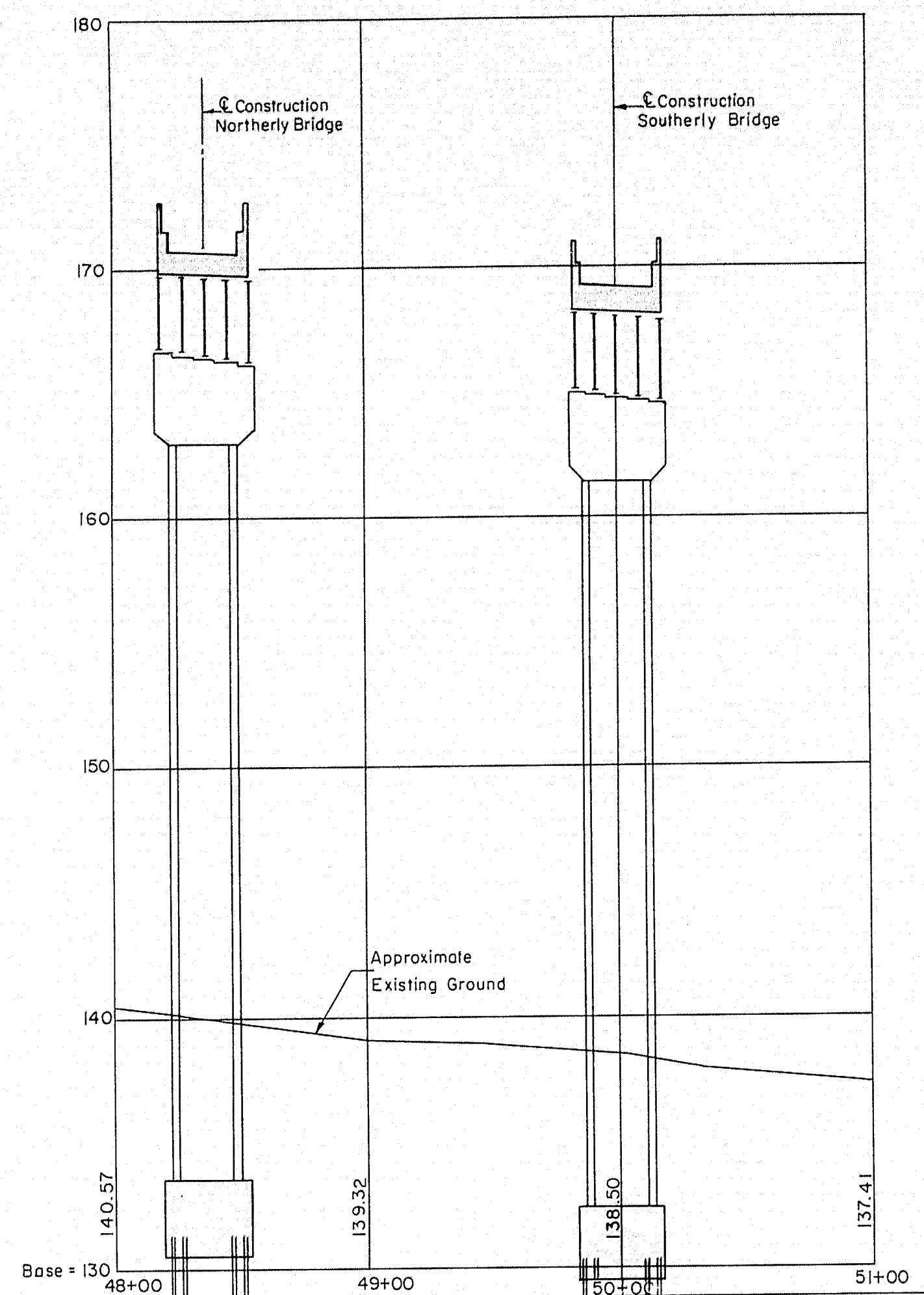
ESTIMATED QUANTITIES
(NOT GUARANTEED)

STRUCTURAL EARTH EXCAVATION, ABUTMENTS AND RETAINING WALLS	750	CU. YDS.	21.17
STRUCTURAL EARTH EXCAVATION, PIERS	890	CU. YDS.	16.79
STRUCTURAL ROCK EXCAVATION, PIERS	20	CU. YDS.	2.00
GRAVEL BASE COURSE - IN PLACE MEASUREMENT	1460	CU. YDS.	17.97
PORTLAND CEMENT CONCRETE, ABUTMENTS AND RETAINING WALLS	320	CU. YDS.	11.37
PORTLAND CEMENT CONCRETE, PIERS	420	CU. YDS.	16.16
PORTLAND CEMENT CONCRETE, ROADWAY & SIDEWALK SLABS	660	CU. YDS.	2.20
ON STEEL BRIDGES	2100	BBLS.	21.00
BRIDGE DRAINAGE	1	LUMP SUM	
STRUCTURAL STEEL, FABRICATED & DELIVERED	626,000	LBS.	62.60
STRUCTURAL STEEL, ERECTION	626,000	LBS.	62.60
REINFORCING STEEL, DELIVERED	263,200	LBS.	26.32
REINFORCING STEEL, PLACING	263,200	LBS.	26.32
SHEAR CONNECTORS, DELIVERED & PLACED	1	LUMP SUM	
STEEL H-BEAM PILES 42 LBS./FOOT	4,480	LIN. FT.	3.20
FRENCH DRAINS	150	CU. YDS.	2.25
ALUMINUM RAILING	1050	LIN. FT.	9.00
SLOPE PAVING FOR BRIDGES	1520	SQ. YDS.	17.20
STRUCTURAL EARTH EXCAVATION - DRAINAGE	20	CU. YDS.	2.00
COMMON BORROW	1100	CU. YDS.	2.90
OVERHAUL (IN PLACE MEASURE)	3650	CU. YDS.	9.40
STRIPPING PITS	140	CU. YDS.	1.40
HAND LAID RIPRAP	8	CU. YDS.	0.08
LOAM BORROW	180	CU. YDS.	2.70
SODDING	40	SQ. YDS.	1.00
SEEDING - PARKWAY MIXTURE	19	UNIT	
HAY MULCH	2	TON	
PROJECT MARKERS	4	EACH	

DESIGN	CHECK H.P.	BRIDGE NO.
DRAWN V.S.	APPROVED WAH-CJM	SURVEY PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
OVER		
MAINE CENTRAL R.R. & COUNTY ROAD		
IN THE CITY OF		
WATERVILLE		
KENNEBEC COUNTY		
KEY PLAN & PROFILES		
APPROVED BY	DATE	
THE CLARKSON ENGINEERING CO., INC.		
CONSULTING ENGINEERS MASSACHUSETTS		
BOSTON	SHEET 1 OF 12 SHEETS	AUGUSTA, MAINE



PROFILE ALONG MAINE CENTRAL R.R.
(EASTERLY RAIL)
Scales: Hor. 1" = 50'-0"
Vert. 1" = 5'-0"



PROFILE ALONG COUNTY ROAD

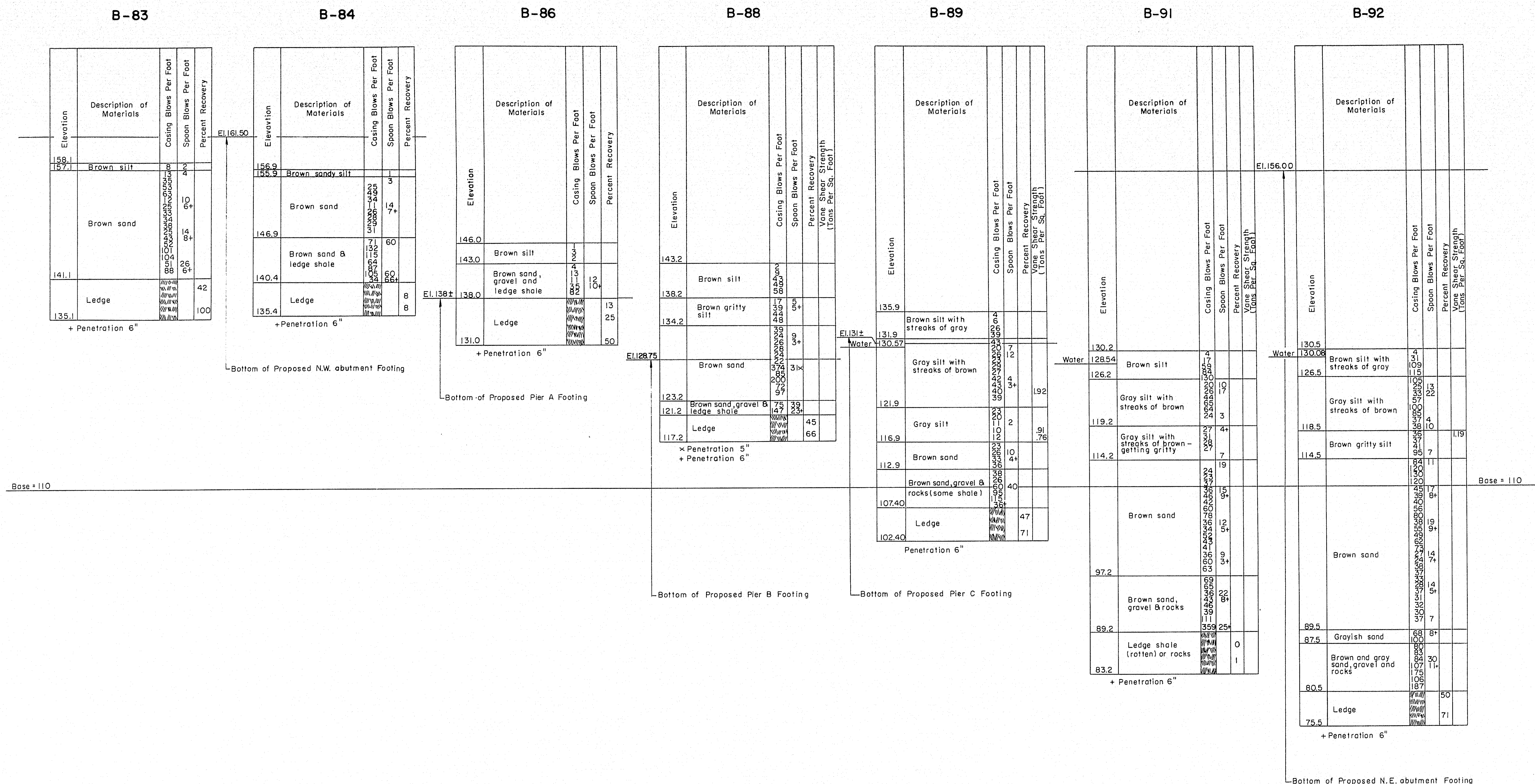
Scales: Hor. 1" = 50'-0"
Vert. 1" = 5'-0"

BANK DATA			
Northerly Bridge	Southerly Bridge	Roadway slope in feet/foot *	
		Left	Right
1042 + 00	1040 + 50	+ .0125	- .0175
1042 + 50	1041 + 00	0	- .0175
1043 + 00	1041 + 50	- .00875	- .0175
1043 + 50	1042 + 00	- .0175	- .0175

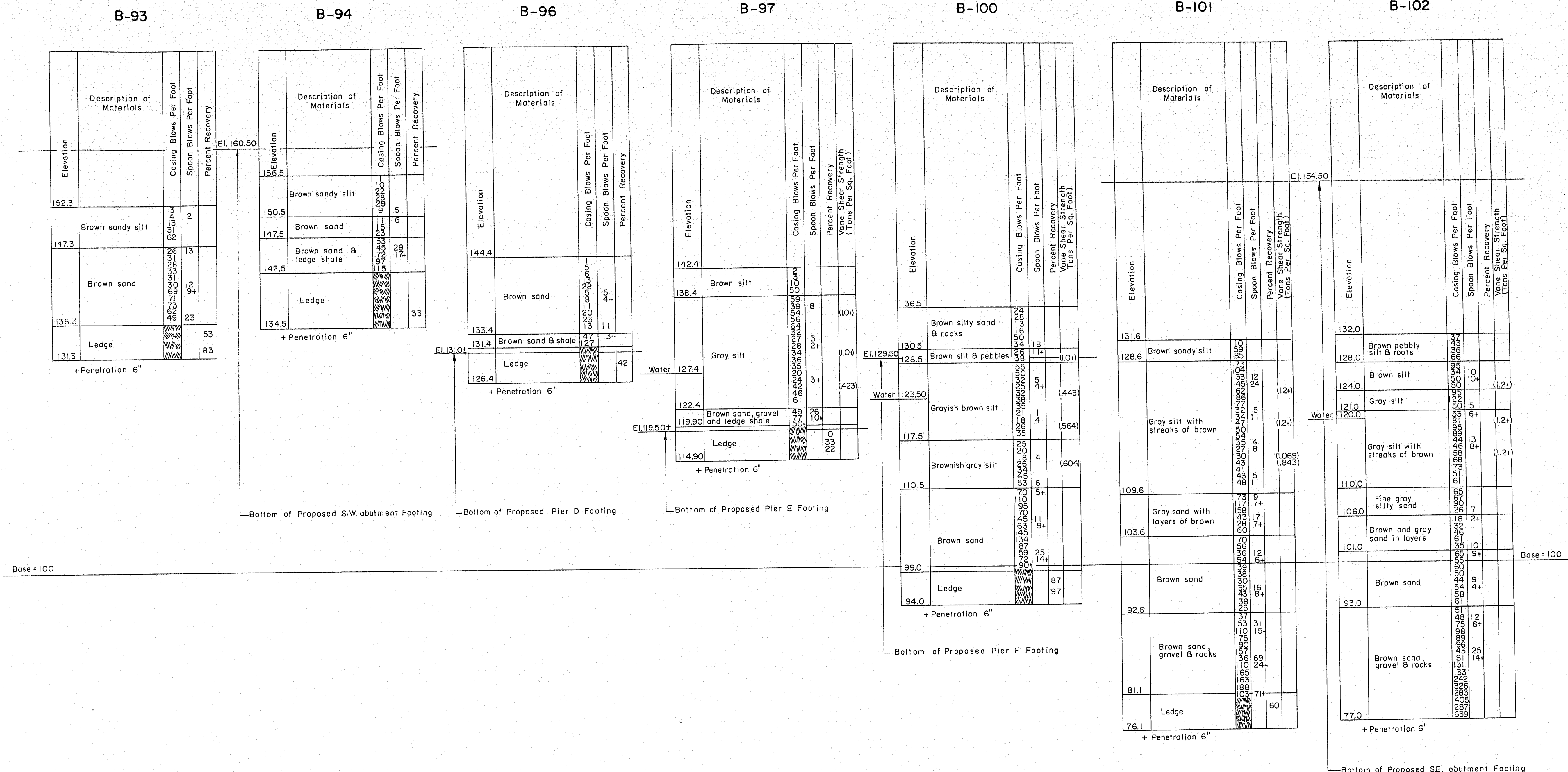
* + slope is up from P.G.
- slope is down from P.G.

THE CLARKESON ENGINEERING CO., INC.		
DESIGN	CHECK H.P.	BRIDGE NO.
DRAWN V.S.	APPROVED WAH-CJM	SURVEY PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
OVER		
MAINE CENTRAL R.R.		
& COUNTY ROAD		
IN THE CITY OF		
WATERVILLE		
KENNEBEC COUNTY		
PROFILES		
SHEET 2 OF 12 SHEETS		AUGUSTA, MAINE

WATERVILLE INTERSTATE



Elev	Depth in Feet	No. of Blows	Remarks
PB-85			
146.0	4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63 64 65 66 67 68 69 70 71 72 73 74 75 76 77 78 79 80 81 82 83 84 85 86 87 88 89 90 91 92 93 94 95 96 97 98 99 100 101 102 103 104 105 106 107 108 109 110 111 112 113 114 115 116 117 118 119 120 121 122 123 124 125 126 127 128 129 130 131 132 133 134 135 136 137 138 139 140 141 142 143 144 145 146 147 148 149 150 151 152 153 154 155 156 157 158 159 160 161 162 163 164 165 166 167 168 169 170 171 172 173 174 175 176 177 178 179 180 181 182 183 184 185 186 187 188 189 190 191 192 193 194 195 196 197 198 199 200 201 202 203 204 205 206 207 208 209 210 211 212 213 214 215 216 217 218 219 220 221 222 223 224 225 226 227 228 229 230 231 232 233 234 235 236 237 238 239 240 241 242 243 244 245 246 247 248 249 250 251 252 253 254 255 256 257 258 259 260 261 262 263 264 265 266 267 268 269 270 271 272 273 274 275 276 277 278 279 280 281 282 283 284 285 286 287 288 289 290 291 292 293 294 295 296 297 298 299 300 301 302 303 304 305 306 307 308 309 310 311 312 313 314 315 316 317 318 319 320 321 322 323 324 325 326 327 328 329 330 331 332 333 334 335 336 337 338 339 340 341 342 343 344 345 346 347 348 349 350 351 352 353 354 355 356 357 358 359 360 361 362 363 364 365 366 367 368 369 370 371 372 373 374 375 376 377 378 379 380 381 382 383 384 385 386 387 388 389 390 391 392 393 394 395 396 397 398 399 400 401 402 403 404 405 406 407 408 409 410 411 412 413 414 415 416 417 418 419 420 421 422 423 424 425 426 427 428 429 430 431 432 433 434 435 436 437 438 439 440 441 442 443 444 445 446 447 448 449 450 451 452 453 454 455 456 457 458 459 460 461 462 463 464 465 466 467 468 469 470 471 472 473 474 475 476 477 478 479 480 481 482 483 484 485 486 487 488 489 490 491 492 493 494 495 496 497 498 499 500 501 502 503 504 505 506 507 508 509 510 511 512 513 514 515 516 517 518 519 520 521 522 523 524 525 526 527 528 529 530 531 532 533 534 535 536 537 538 539 540 541 542 543 544 545 546 547 548 549 550 551 552 553 554 555 556 557 558 559 560 561 562 563 564 565 566 567 568 569 570 571 572 573 574 575 576 577 578 579 580 581 582 583 584 585 586 587 588 589 590 591 592 593 594 595 596 597 598 599 600 601 602 603 604 605 606 607 608 609 610 611 612 613 614 615 616 617 618 619 620 621 622 623 624 625 626 627 628 629 630 631 632 633 634 635 636 637 638 639 640 641 642 643 644 645 646 647 648 649 650 651 652 653 654 655 656 657 658 659 660 661 662 663 664 665 666 667 668 669 670 671 672 673 674 675 676 677 678 679 680 681 682 683 684 685 686 687 688 689 690 691 692 693 694 695 696 697 698 699 700 701 702 703 704 705 706 707 708 709 710 711 712 713 714 715 716 717 718 719 720 721 722 723 724 725 726 727 728 729 730 731 732 733 734 735 736 737 738 739 740 741 742 743 744 745 746 747 748 749 750 751 752 753 754 755 756 757 758 759 760 761 762 763 764 765 766 767 768 769 770 771 772 773 774 775 776 777 778 779 780 781 782 783 784 785 786 787 788 789 790 791 792 793 794 795 796 797 798 799 800 801 802 803 804 805 806 807 808 809 810 811 812 813 814 815 816 817 818 819 820 821 822 823 824 825 826 827 828 829 830 831 832 833 834 835 836 837 838 839 840 841 842 843 844 845 846 847 848 849 850 851 852 853 854 855 856 857 858 859 860 861 862 863 864 865 866 867 868 869 870 871 872 873 874 875 876 877 878 879 880 881 882 883 884 885 886 887 888 889 890 891 892 893 894 895 896 897 898 899 900 901 902 903 904 905 906 907 908 909 910 911 912 913 914 915 916 917 918 919 920 921 922 923 924 925 926 927 928 929 930 931 932 933 934 935 936 937 938 939 940 941 942 943 944 945 946 947 948 949 950 951 952 953 954 955 956 957 958 959 960 961 962 963 964 965 966 967 968 969 970 971 972 973 974 975 976 977 978 979 980 981 982 983 984 985 986 987 988 989 990 991 992 993 994 995 996 997 998 999 1000 1001 1002 1003 1004 1005 1006 1007 1008 1009 1010 1011 1012 1013 1014 1015 1016 1017 1018 1019 1020 1021 1022 1023 1024 1025 1026 1027 1028 1029 1030 1031 1032 1033 1034 1035 1036 1037 1038 1039 1040 1041 1042 1043 1044 1045 1046 1047 1048 1049 1050 1051 1052 1053 1054 1055 1056 1057 1058 1059 1060 1061 1062 1063 1064 1065 1066 1067 1068 1069 1070 1071 1072 1073 1074 1075 1076 1077 1078 1079 1080 1081 1082 1083 1084 1085 1086 1087 1088 1089 1090 1091 1092 1093 1094 1095 1096 1097 1098 1099 1100 1101 1102 1103 1104 1105 1106 1107 1108 1109 1110 1111 1112 1113 1114 1115 1116 1117 1118 1119 1120 1121 1122 1123 1124 1125 1126 1127 1128 1129 1130 1131 1132 1133 1134 1135 1136 1137 1138 1139 1140 1141 1142 1143 1144 1145 1146 1147 1148 1149 1150 1151 1152 1153 1154 1155 1156 1157 1158 1159 1160 1161 1162 1163 1164 1165 1166 1167 1168 1169 1170 1171 1172 1173 1174 1175 1176 1177 1178 1179 1180 1181 1182 1183 1184 1185 1186 1187 1188 1189 1190 1191 1192 1193 1194 1195 1196 1197 1198 1199 1200 1201 1202 1203 1204 1205 1206 1207 1208 1209 1210 1211 1212 1213 1214 1215 1216 1217 1218 1219 1220 1221 1222 1223 1224 1225 1226 1227 1228 1229 1230 1231 1232 1233 1234 1235 1236 1237 1238 1239 1240 1241 1242 1243 1244 1245 1246 1247 1248 1249 1250 1251 1252 1253 1254 1255 1256 1257 1258 1259 1260 1261 1262 1263 1264 1265 1266 1267 1268 1269 1270 1271 1272 1273 1274 1275 1276 1277 1278 1279 1280 1281 1282 1283 1284 1285 1286 1287 1288 1289 1290 1291 1292 1293 1294 1295 1296 1297 1298 1299 1300 1301 1302 1303 1304 1305 1306 1307 1308 1309 1310 1311 1312 1313 1314 1315 1316 1317 1318 1319 1320 1321 1322 1323 1324 1325 1326 1327 1328 1329 1330 1331 1332 1333 1334 1335 1336 1337 1338 1339 1340 1341 1342 1343 1344 1345 1346 1347 1348 1349 1350 1351 1352 1353 1354 1355 1356 1357 1358 1359 1360 1361 1362 1363 1364 1365 1366 1367 1368 1369 1370 1371 1372 1373 1374 1375 1376 1377 1378 1379 1380 1381 1382 1383 1384 1385 1386 1387 1388 1389 1390 1391 1392 1393 1394 1395 1396 1397 1398 1399 1400 1401 1402 1403 1404 1405 1406 1407 1408 1409 1410 1411 1412 1413 1414 1415 1416 1417 1418 1419 1420 1421 1422 1423 1424 1425 1426 1427 1428 1429 1430 1431 1432 1433 1434 1435 1436 1437 1438 1439 1440 1441 1442 1443 1444 1445 1446 1447 1448 1449 1450 1451 1452 1453 1454 1455 1456 1457 1458 1459 1460 1461 1462 1463 1464 1465 1466 1467 1468 1469 1470 1471 1472 1473 1474 1475 1476 1477 1478 1479 1480 1481 1482 1483 1484 1485 1486 1487 1488 1489 1490 1491 1492 1493 1494 1495 1496 1497 1498 1499 1500 1501 1502 1503 1504 1505 1506 1507 1508 1509 1510 1511 1512 1513 1514 1515 1516 1517 1518 1519 1520 1521 1522 1523 1524 1525 1526 1527 1528 1529 1530 1531 1532 1533 1534 1535 1536 1537 1538 1539 1540 1541 1542 1543 1544 1545 1546 1547 1548 1549 1550 1551 1552 1553 1554 1555 1556 1557 1558 1559 1560 1561 1562 1563 1564 1565 1566 1567 1568 1569 1570 1571 1572 1573 1574 1575 1576 1577 1578 1579 1580 1581 1582 1583 1584 1585 1586 1587 1588 1589 1590 1591 1592 1593 1594 1595 1596 1597 1598 1599 1600 1601 1602 1603 1604 1605 1606 1607 1608 1609 1610 1611 1612 1613 1614 1615 1616 1617 1618 1619 1620 1621 1622 1623 1624 1625 1626 1627 1628 1629 1630 1631 1632 1633 1634 1635 1636 1637 1638 1639 1640 1641 1642 1643 1644 1645 1646 1647 1648 1649 1650 1651 1652 1653 1654 1655 1656 1657 1658 1659 1660 1661 1662 1663 1664 1665 1666 1667 1668 1669 1670 1671 1672 1673 1674 1675 1676 1677 1678 1679 1680 1681 1682 1683 1684 1685 1686 1687 1688 1689 1690 1691 1692 1693 1694 1695 1696 1697 1698 1699 1700 1701 1702 1703 1704 1705 1706 1707 1708 1709 1710 1711 1712 1713 1714 1715 1716 1717 1718 1719 1720 1721 1722 1723 1724 1725 1726 1727 1728 1729 1730 1731 1732 1733 1734 1735 1736 1737 1738 1739 1740 1741 1742 1743 1744 1745 1746 1747 1748 1749 1750 1751 1752 1753 1754 1755 1756 1757 1758 1759 1760 1761 1762 1763 1764 1765 1766 1767 1768 1769 1770 1771 1772 1773 1774 1775 1776 1777 1778 1779 1780 1781 1782 1783 1784 1785 1786 1787 1788 1789 1790 1791 1792 1793 1794 1795 1796 1797 1798 1799 1800 1801 1802 1803 1804 1805 1806 1807 1808 1809 1810 1811 1812 1813 1814 1815 1816 1817 1818 1819 1820 1821 1822 1823 1824 1825 1826 1827 1828 1829 1830 1831 1832 1833 1834 1835 1836 1837 1838 1839 1840 1841 1842 1843 1844 1845 1846 1847 1848 1849 1850 1851 1852 1853 1854 1855 1856 1857 1858 1859 1860 1861 1862 1863 1864 1865 1866 1867 1868 1869 1870 1871 1872 1873 1874 1875 1876 1877 1878 1879 1880 1881 1882 1883 1884 1885 1886 1887 1888 1889 1890 1891 1892 1893 1894 1895 1896 1897 1898 1899 1900 1901 1902 1903 1904 1905 1906 1907 1908 1909 1910 1911 1912 1913 1914 1915 1916 1917 1918 1919 1920 1921 1922 1923 1924 1925 1926 1927 1928 1929 1930 1931 1932 1933 1934 1935 1936 1937 1938 1939 1940 1941 1942 1943 1944 1945 1946 1947 1948 1949 1950 1951 1952 1953 1954 1955 1956 1957 1958 1959 1960 1961 1962 1963 1964 1965 1966 1967 1968 1969 1970 1971 1972 1973 1974 1975 1976 1977 1978 1979 1980 1981 1982 1983 1984 1985 1986 1987 1988 1989 1990 1991 1992 1993 1994 1995 1996 1997 1998 1999 2000 2001 2002 2003 2004 2005 2006 2007 2008 2009 2010 2011 2012 2013 2014 2015 2016 2017 2018 2019 2020 2021 2022 2023 2024 2025 2026 2027 2028 2029 2030 2031 2032 2033 2034 2035 2036 2037 2038 2039 2040 2041 2042 2043 2044 2045 2046 2047 2048 2049 2050 2051 2052 2053 2054 2055 2056 2057 2058 2059 2060 2061 2062 2063 2064 2065 2066 2067 2068 2069 2070 2071 2072 2073 2074 2075 2076 2077 2078 2079 2080 2081 2082 2083 2084 2085 2086 2087 2088 2089 2090 2091 2092 2093 2094 2095 2096 2097 2098 2099 2100 2101 2102 2103 2104 2105 2106 2107 2108 2109 2110 2111 2112 2113 2114 2115 2116 2117 2118 2119 2120 2121 2122 2123 2124 2125 2126 2127 2128 2129 2130 2131 2132 2133 2134 2135 2136 2137 2138 2139 2140 2141 2142 2143 2144 2145 2146 2147 2148 2149 2150 2151 2152 2153 2154 2155 2156 2157 2158 2159 2160 2161 2162 2163 2164 2165 2166 2167 2168 2169 2170 2171 2172 2173 2174 2175 2176 2177 2178 2179 2180 2181 2182 2183 2184 2185 2186 2187 2188 2189 2190 2191 2192 2193 2194 2195 2196 2197 2198 2199 2200 2201 2202 2203 2204 2205 2206 2207 2208 2209 2210 2211 2212 2213 2214 2215 2216 2217 2218 2219 2220 2221 2222 2223 2224 2225 2226 2227 2228 2229 2230 2231 2232 2233 2234 2235 2236 2237 2238 2239 2		



Elev.	Depth in Feet	No. of Blows	Remarks
144.3	0	31	Exist. Ground
140.2	0	39	Exist. Ground
135.5	0	16	Exist. Ground

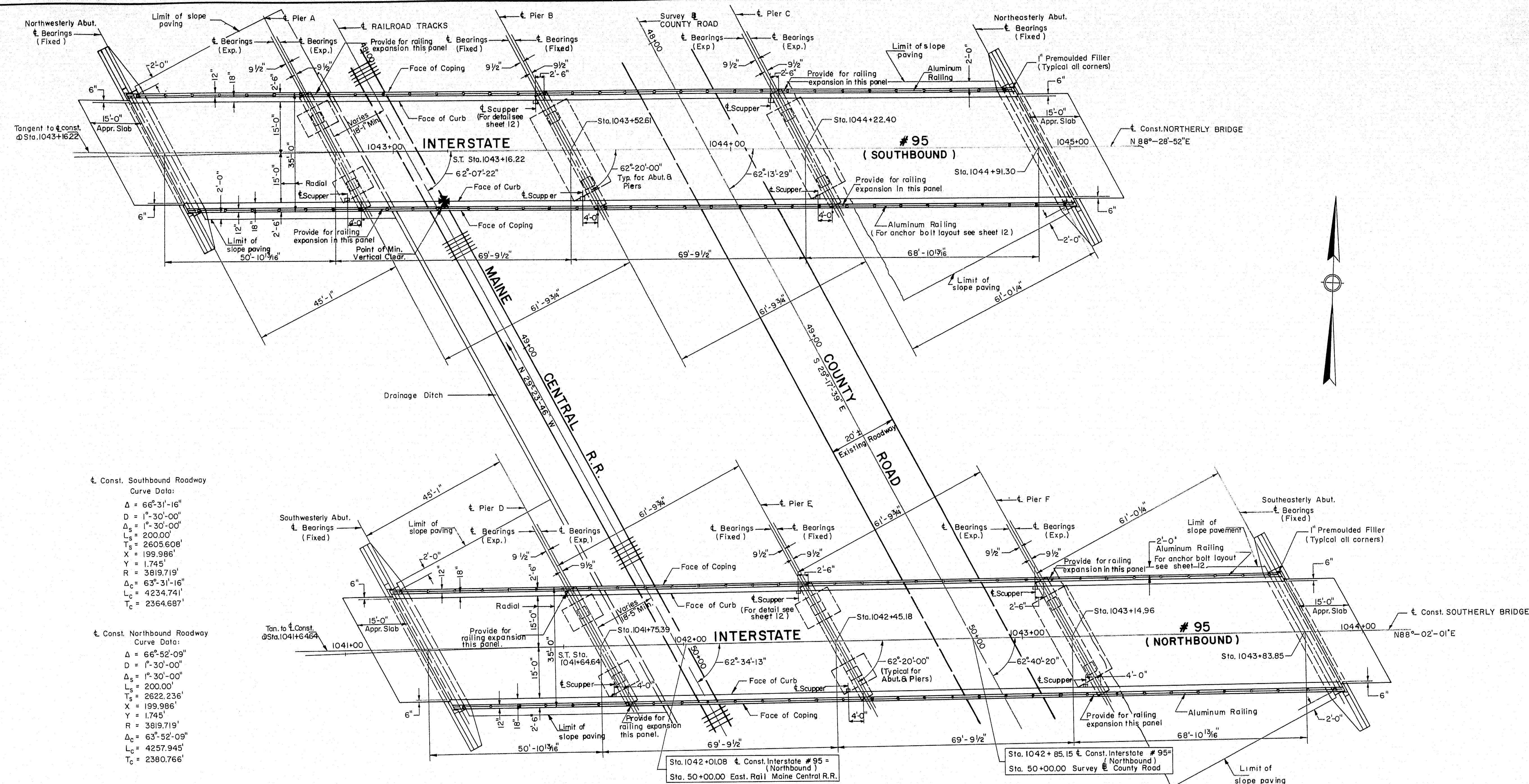
SOUTHERLY BRIDGE BORING LOG

Scale: 1/8" = 1'-0"
For Boring Log notes see Sheet 3

THE CLARKESON ENGINEERING CO., INC.		
DESIGN	CHECK H.P.	BRIDGE NO.
DRAWN V.S.	APPROVED W.A.H.-C.J.M.	SURVEY PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
OVER		
MAINE CENTRAL R.R.		
& COUNTY ROAD		
IN THE CITY OF		
WATERVILLE		
KENNEBEC COUNTY		
BORING DATA		
SHEET 4 OF 12 SHEETS		AUGUSTA, MAINE

B. P. R.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(23)	11	24

WATERVILLE INTERSTATE

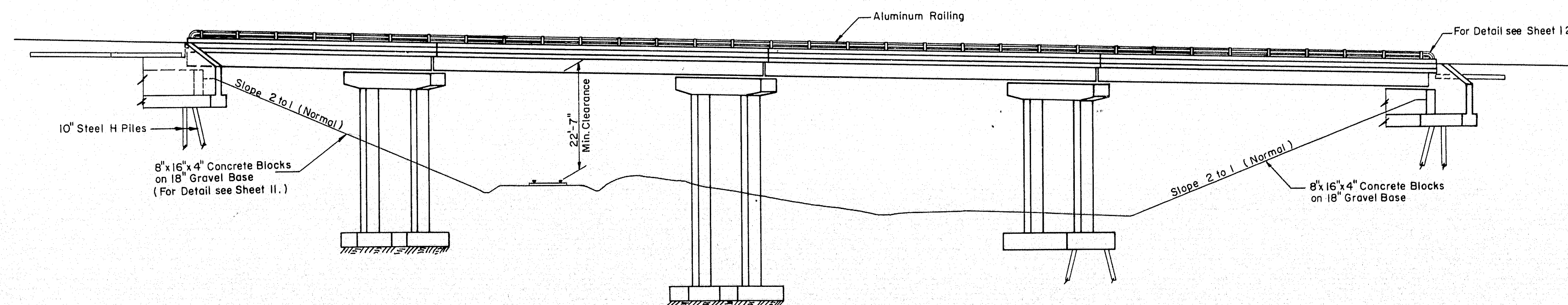


Const. Southbound Roadway
Curve Data:
Δ = 66°-31'-16"
D = 1°-30'-00"
L_s = 200.00'
T_s = 2609.608'
X = 199.986'
Y = 1.745'
R = 3819.719'
Δ_c = 63°-31'-16"
L_c = 4234.741'
T_c = 2364.687'

Const. Northbound Roadway
Curve Data:
Δ = 66°-52'-09"
D = 1°-30'-00"
L_s = 200.00'
T_s = 2622.236'
X = 199.986'
Y = 1.745'
R = 3819.719'
Δ_c = 63°-52'-09"
L_c = 4257.945'
T_c = 2380.766'

GENERAL PLAN

Scale: 1/16" = 1'-0"



GENERAL ELEVATION

Scale: 1/16" = 1'-0"

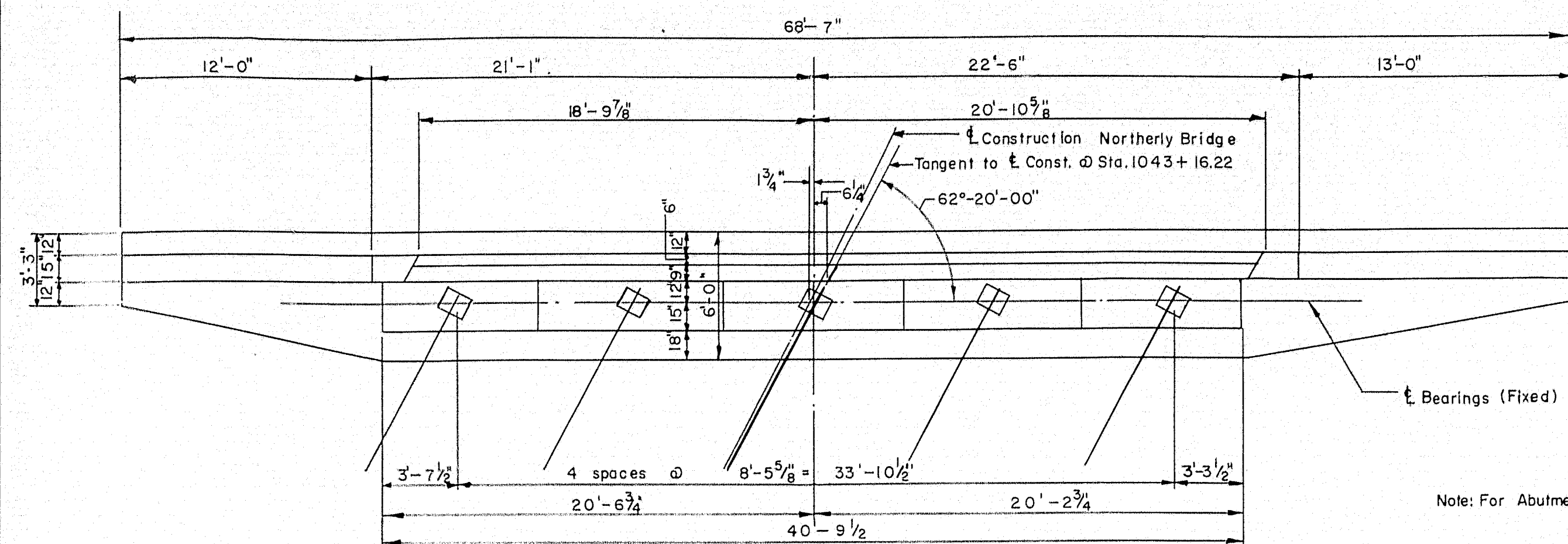
THE CLARKSON ENGINEERING CO., INC.			
DESIGN H. L.	CHECK G. B. & H. P.	BRIDGE NO.	
DRAWN E. K.	APPROVED W. H. & C. J. M.	SURVEY PLOT	
STATE HIGHWAY COMMISSION			
INTERSTATE #95			
OVER			
MAINE CENTRAL R.R. & COUNTY ROAD			
IN THE CITY OF			
WATERVILLE			
KENNEBEC COUNTY			
GENERAL PLAN & ELEVATION			
SHEET 5 OF 12 SHEETS		AUGUSTA, MAINE	

75-112

0 1 2 3 4 5 INCHES

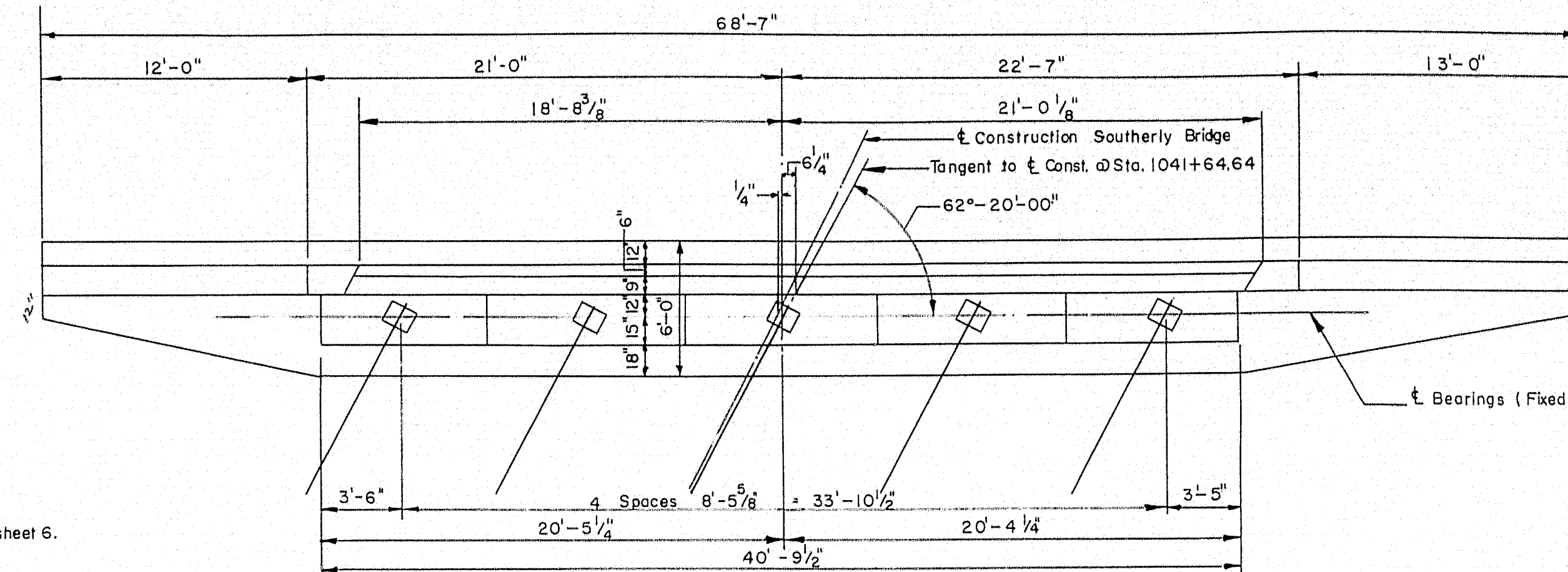
B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-6(23)	13	78

WATERVILLE INTERSTATE



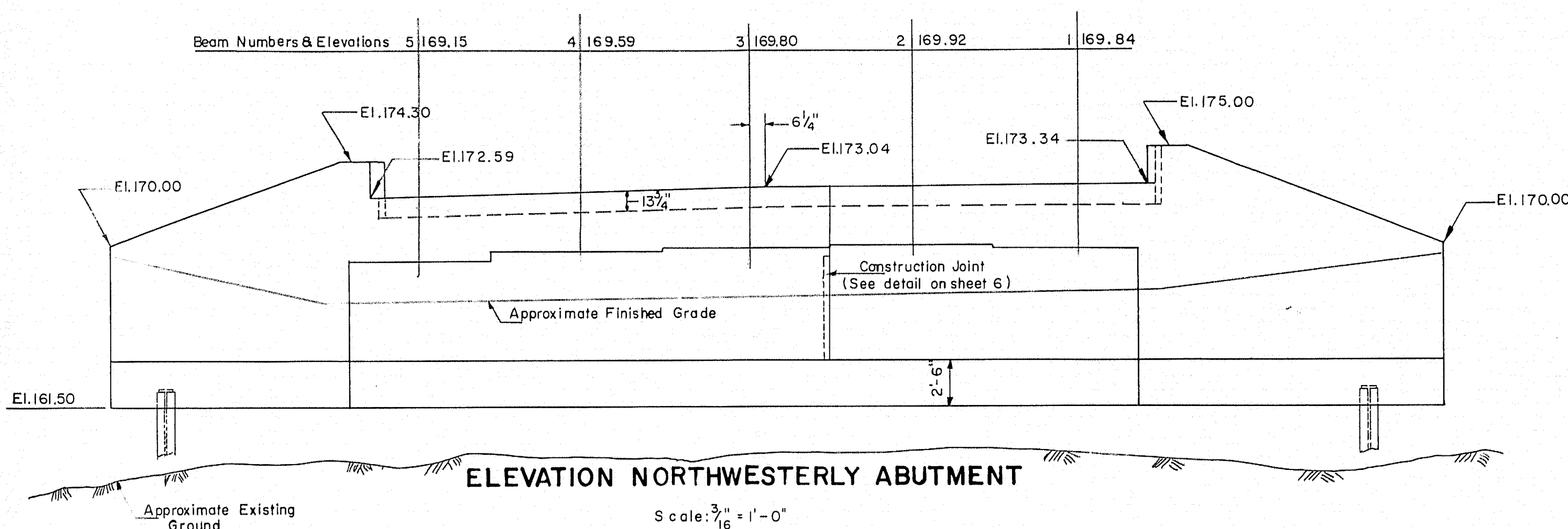
PLAN NORTHWESTERLY ABUTMENT

Scale: $\frac{3}{16}'' = 1'-0''$



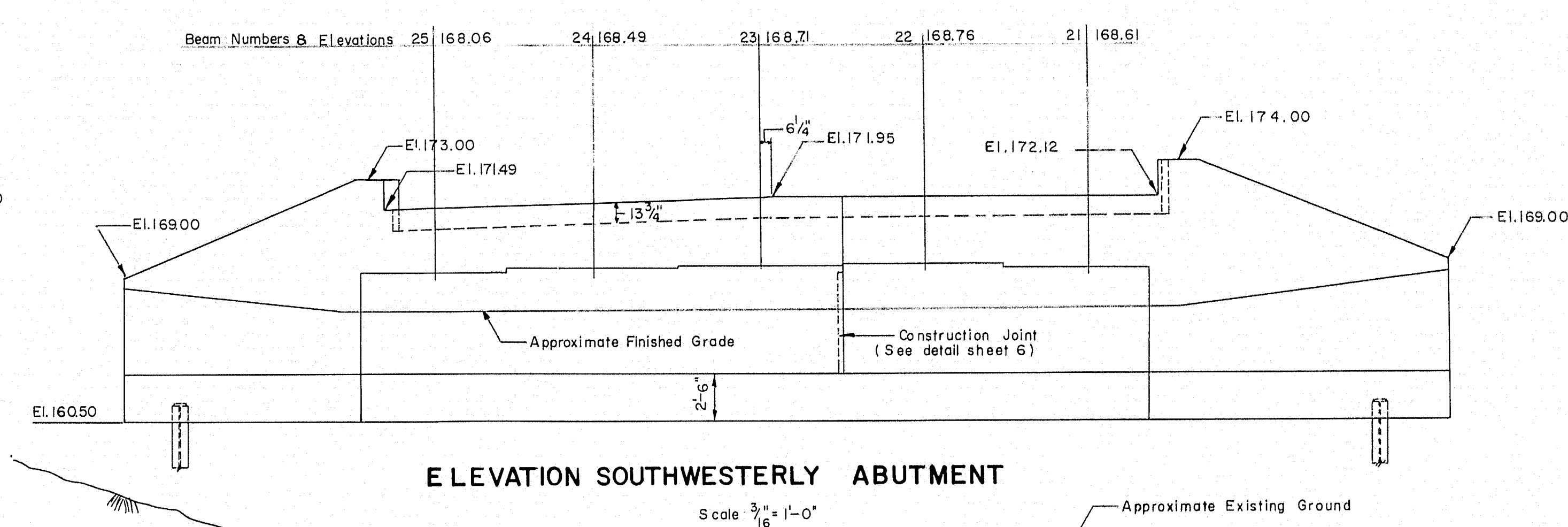
PLAN SOUTHWESTERLY ABUTMENT

Scale: $\frac{3}{16}'' = 1'-0''$



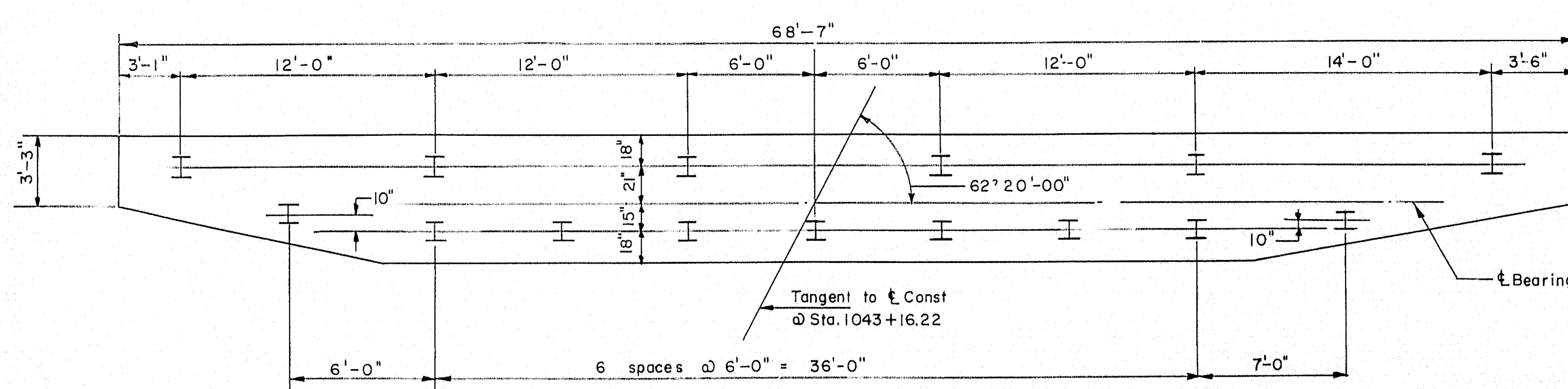
ELEVATION NORTHWESTERLY ABUTMENT

Scale: $\frac{3}{16}'' = 1'-0''$



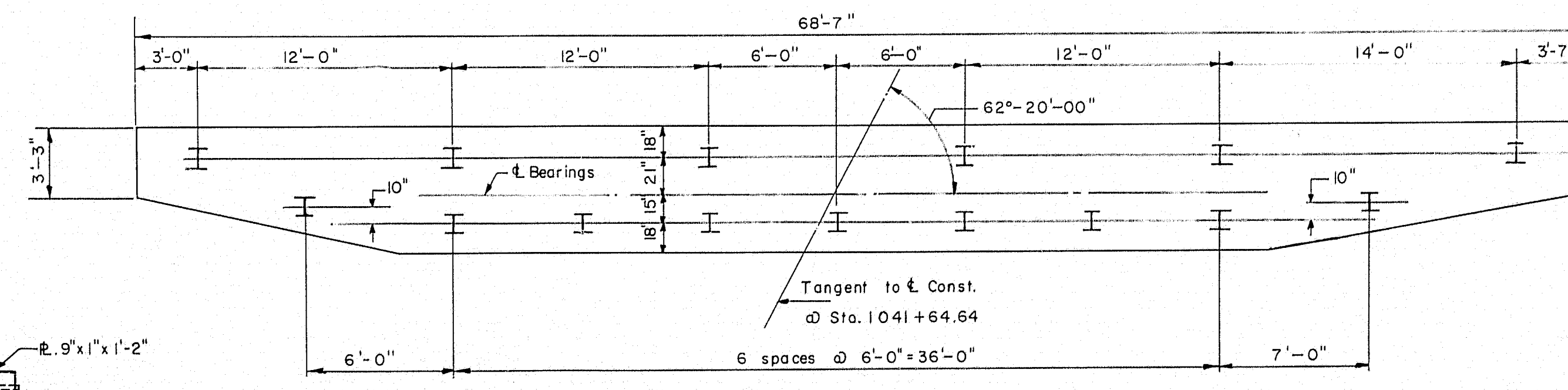
ELEVATION SOUTHWESTERLY ABUTMENT

Scale: $\frac{3}{16}'' = 1'-0''$



PILE PLAN NORTHWESTERLY ABUTMENT

Scale: $\frac{3}{16}'' = 1'-0''$

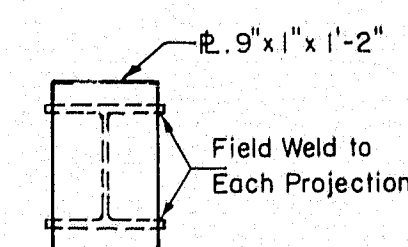


PILE PLAN SOUTHWESTERLY ABUTMENT

Scale: $\frac{3}{16}'' = 1'-0''$

Notes:

1. All piles to be 10BP42.
2. Maximum pile load = 32 Tons.
3. Front piles to be battered 3 on 12.
4. All piles shall be driven to ledge or practical refusal.
5. Cap all piles (See detail).

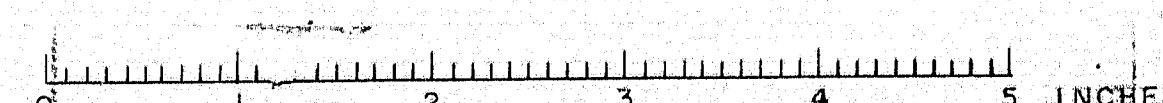


PILE CAP DETAILS

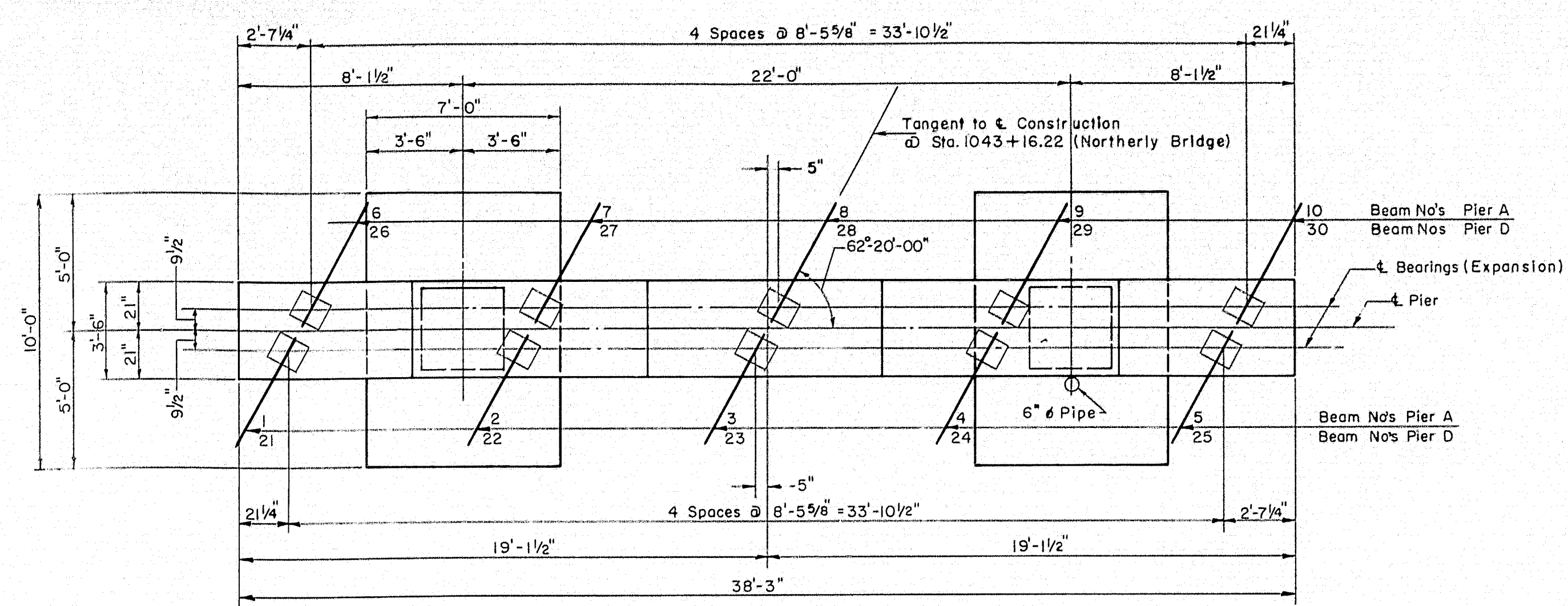
Scale: $\frac{3}{4}'' = 1'-0''$

THE CLARKSON ENGINEERING CO., INC.			
DESIGN	J.T.	CHECK J.M. & H.P.	BRIDGE NO.
DRAWN	D.A.T.	APPROVED WAH-C.J.M.	SURVEY PLOT
STATE HIGHWAY COMMISSION			
INTERSTATE #95			
OVER			
MAINE CENTRAL R.R.			
& COUNTY ROAD			
IN THE CITY OF			
WATERVILLE			
KENNEBEC COUNTY			
WESTERLY ABUTMENTS			
SHEET 7 OF 12 SHEETS			AUGUSTA, MAINE

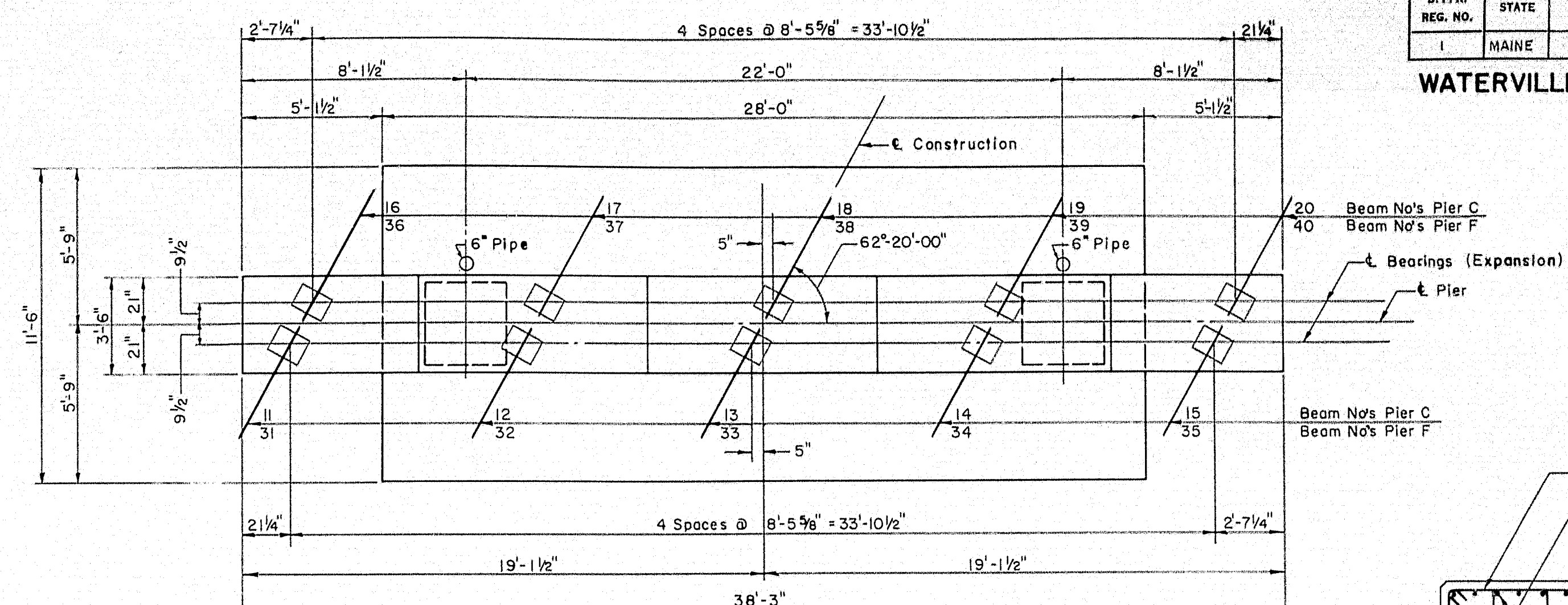
75-114



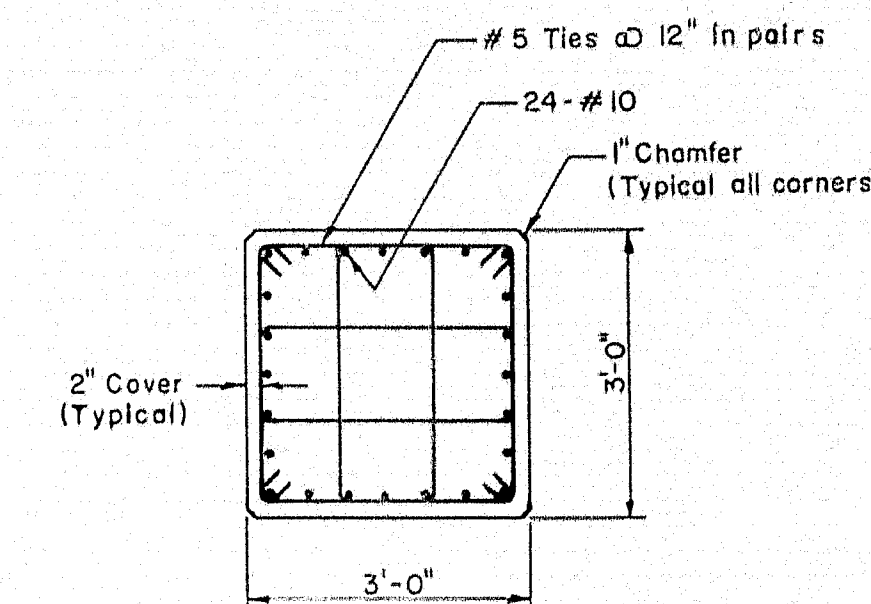
WATERVILLE INTERSTATE



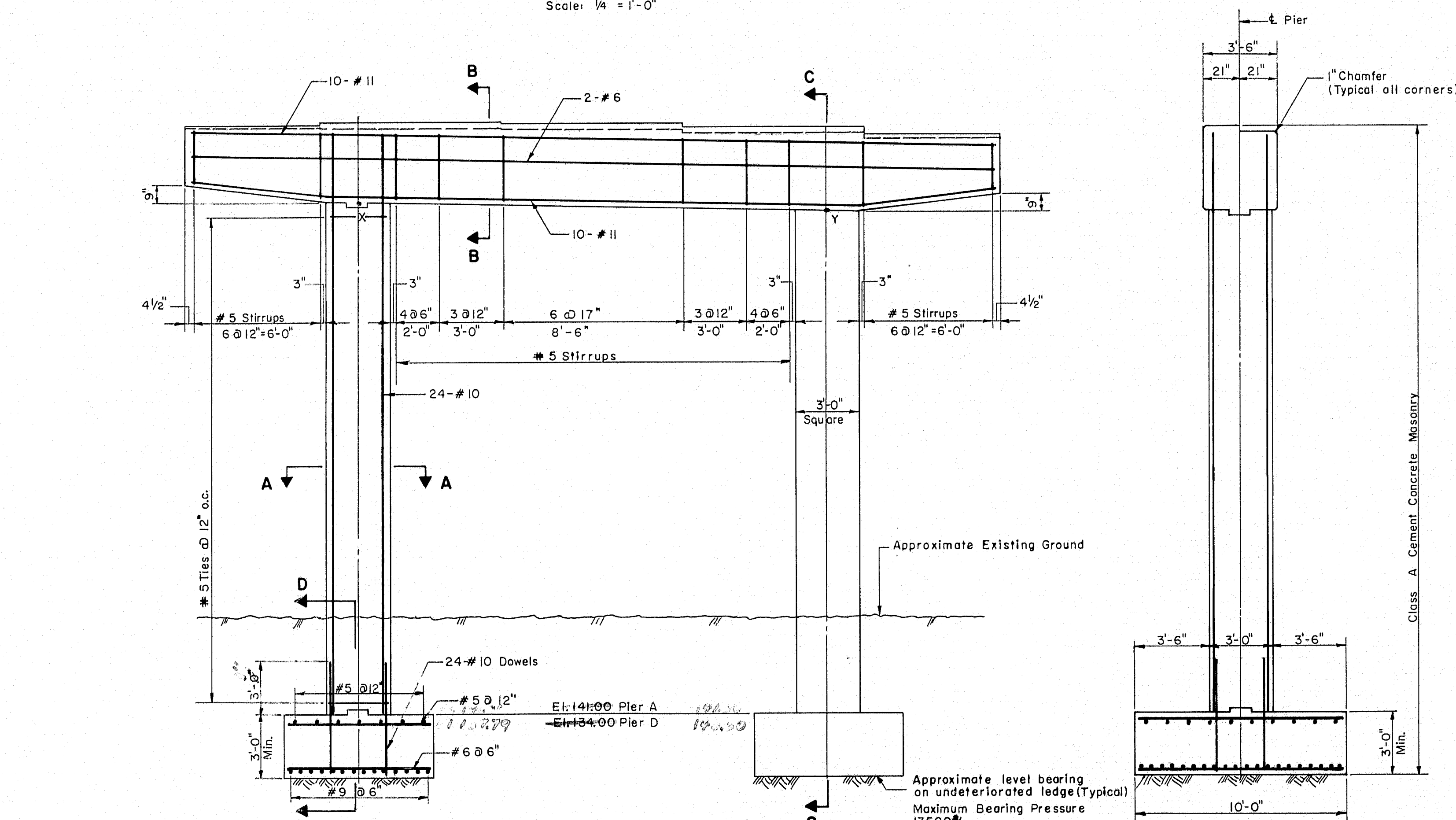
PLAN PIER A
PLAN PIER D SIMILAR
Scale: 1/4" = 1'-0"



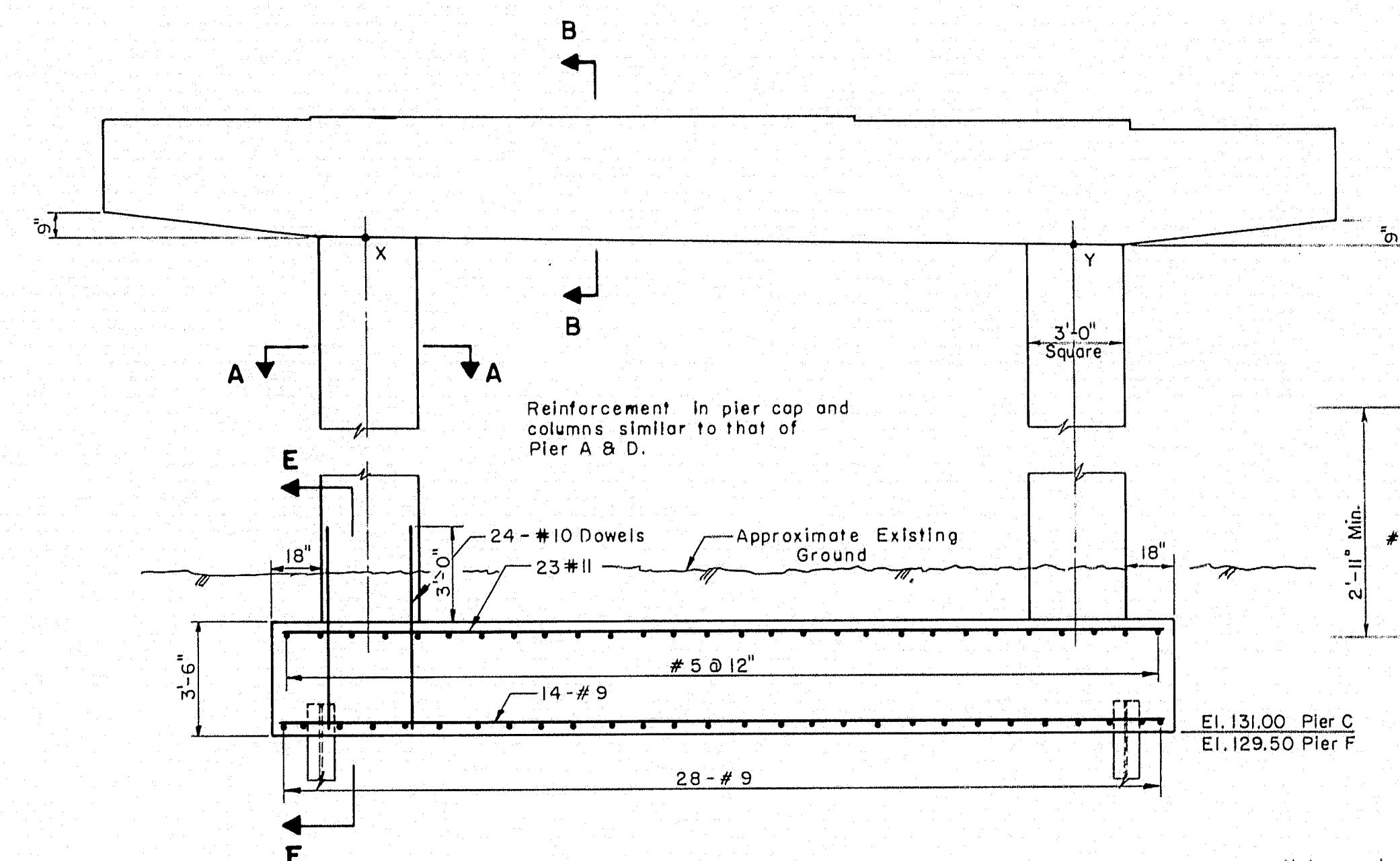
PLAN PIER C
PLAN PIER F SIMILAR
Scale: 1/4" = 1'-0"



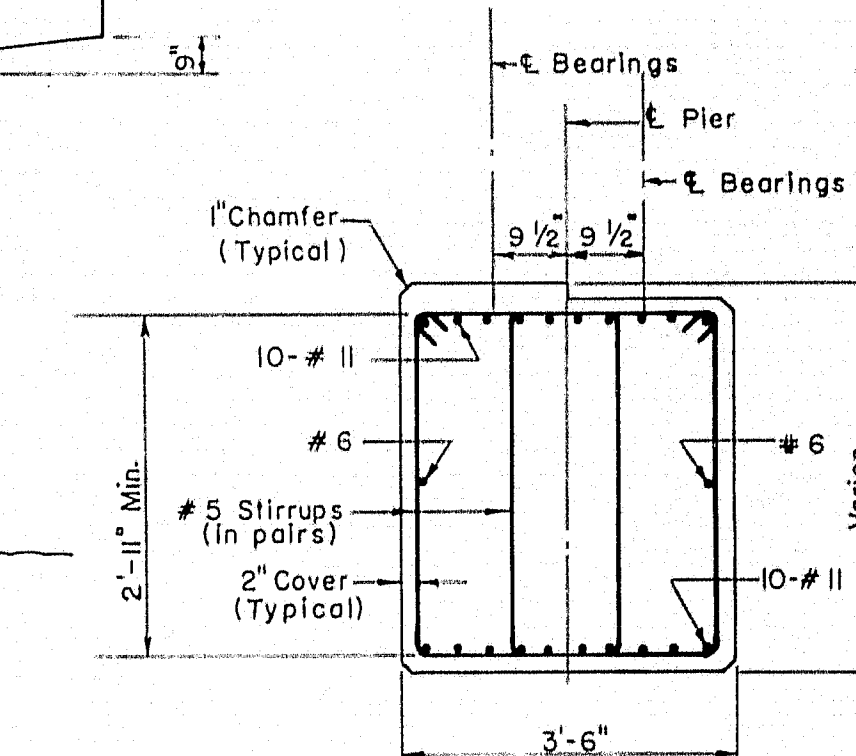
SECTION A-A
Scale: 1/2" = 1'-0"



ELEVATION PIER A
ELEVATION PIER D SIMILAR
Scale: 1/4" = 1'-0"

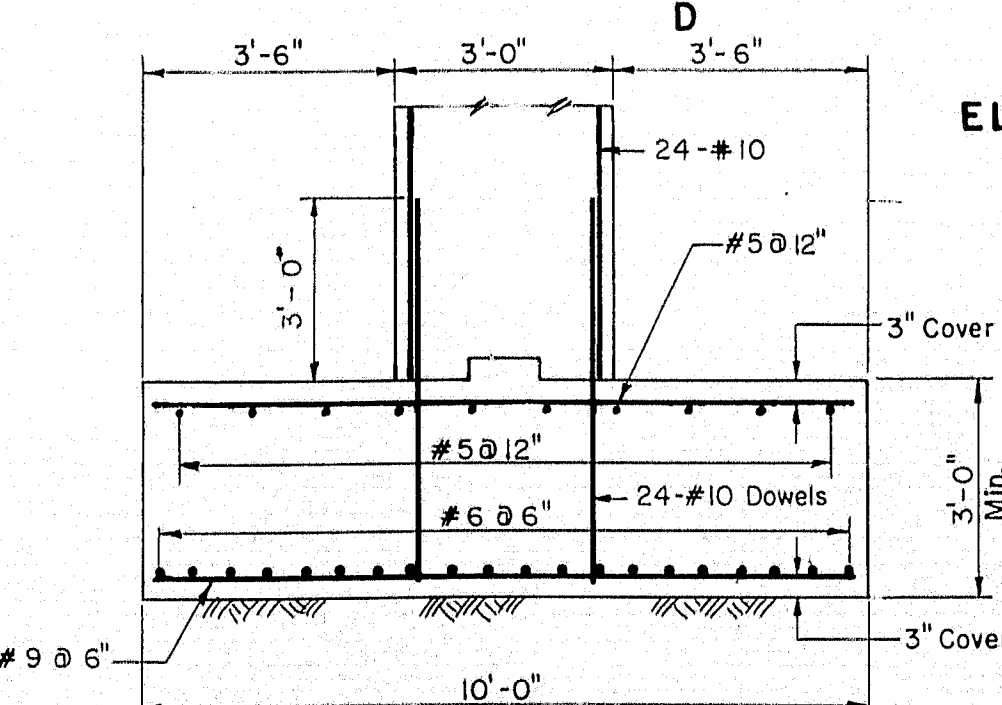


ELEVATION PIER C
ELEVATION PIER F SIMILAR
Scale: 1/4" = 1'-0"

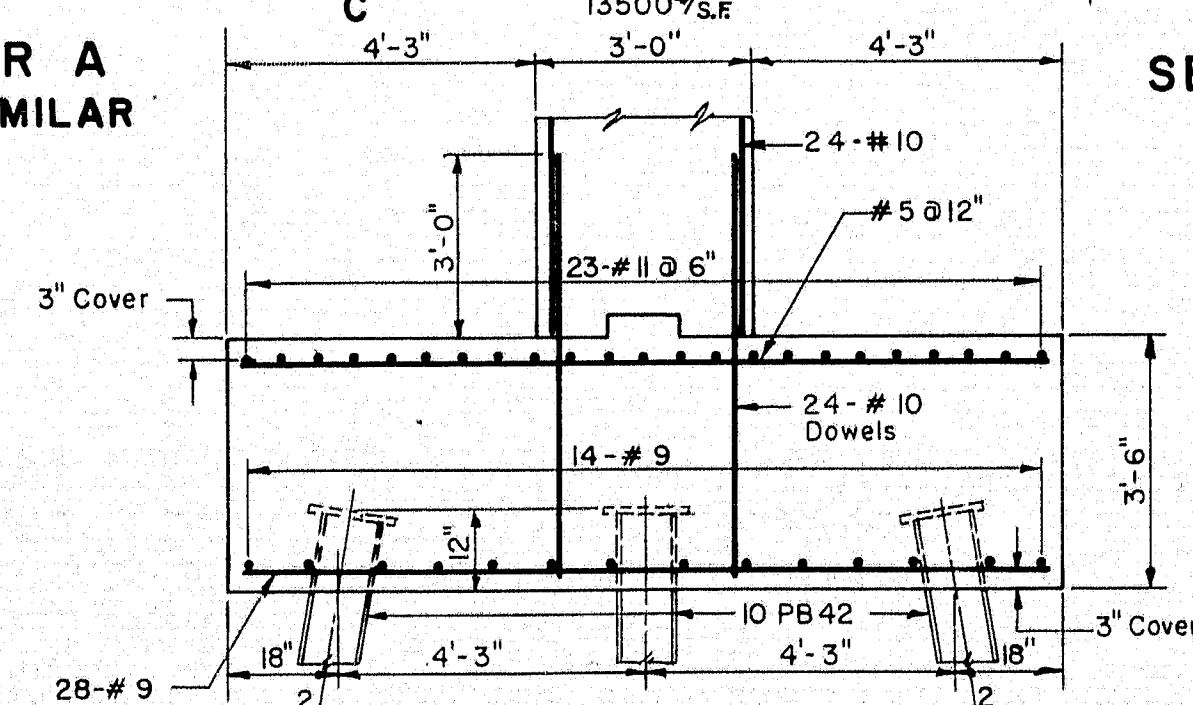


SECTION B-B
Scale: 1/2" = 1'-0"

- Notes:
1. All piles to be 10BP42.
 2. Maximum pile load 35 Tons.
 3. Exterior rows of piles battered 2 on 12.
 4. All piles shall be driven to ledge or practical refusal.
 5. Cap all piles (See Detail on Sheet 7)

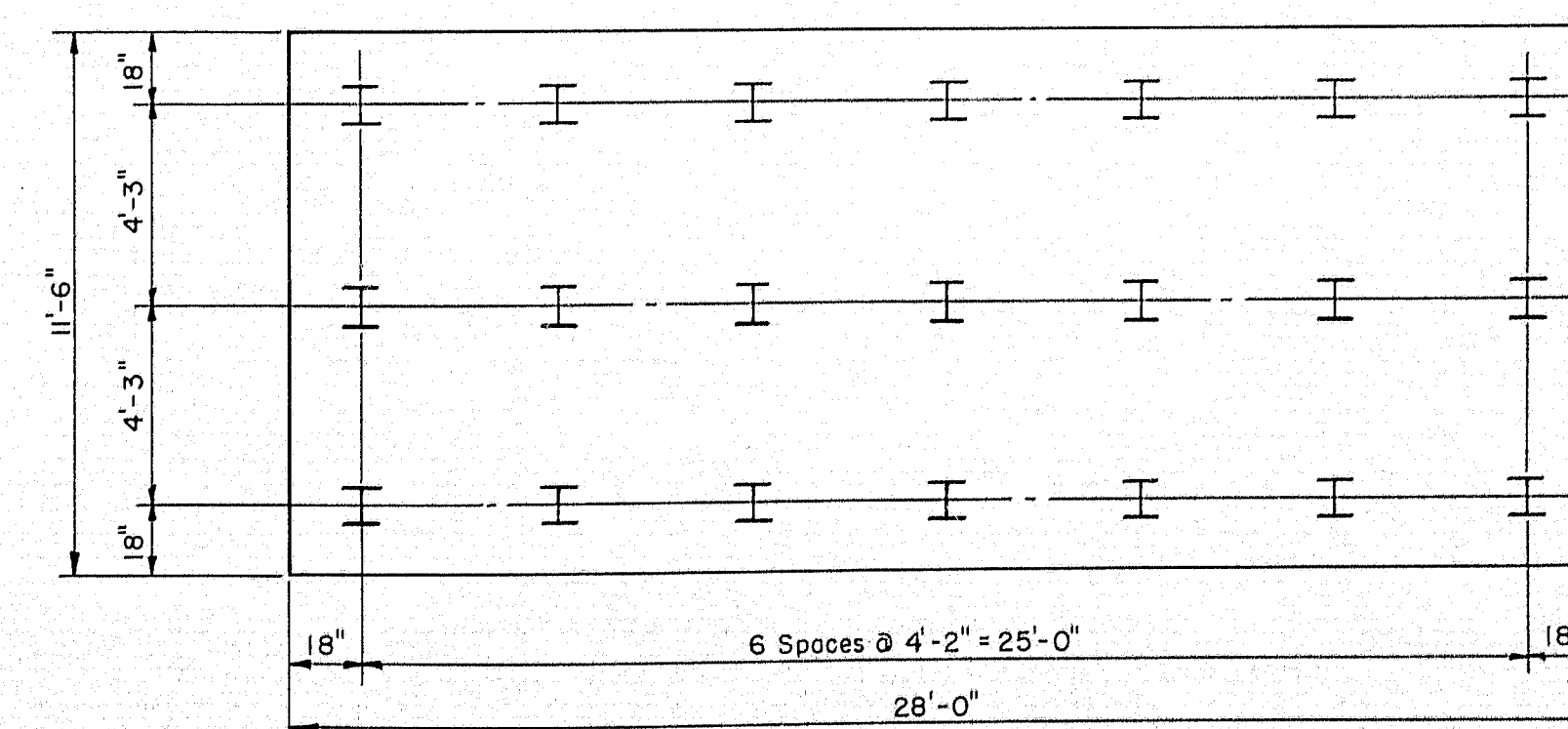


SECTION D-D
Scale: 3/8" = 1'-0"



SECTION E-E
Scale: 3/8" = 1'-0"

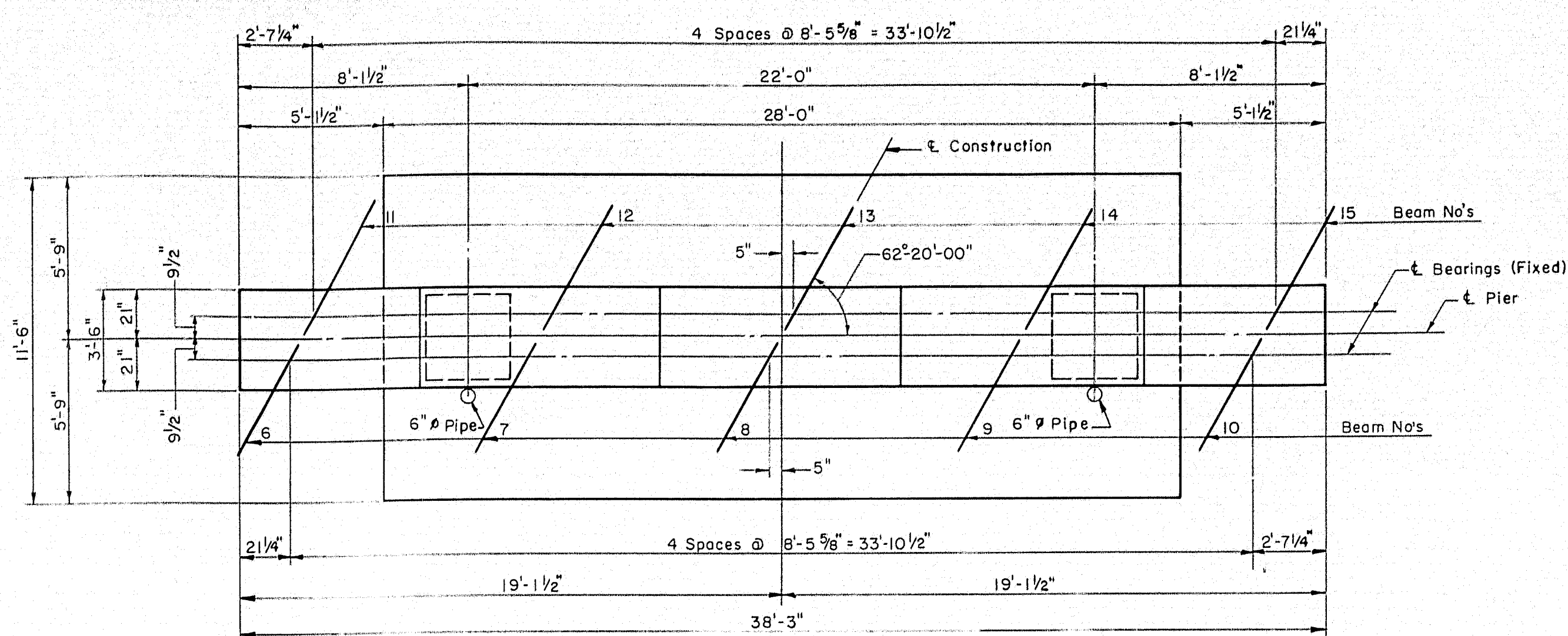
SECTION C-C
Scale: 1/4" = 1'-0"



PILE PLAN PIER C
PILE PLAN PIER F SIMILAR
Scale: 1/4" = 1'-0"

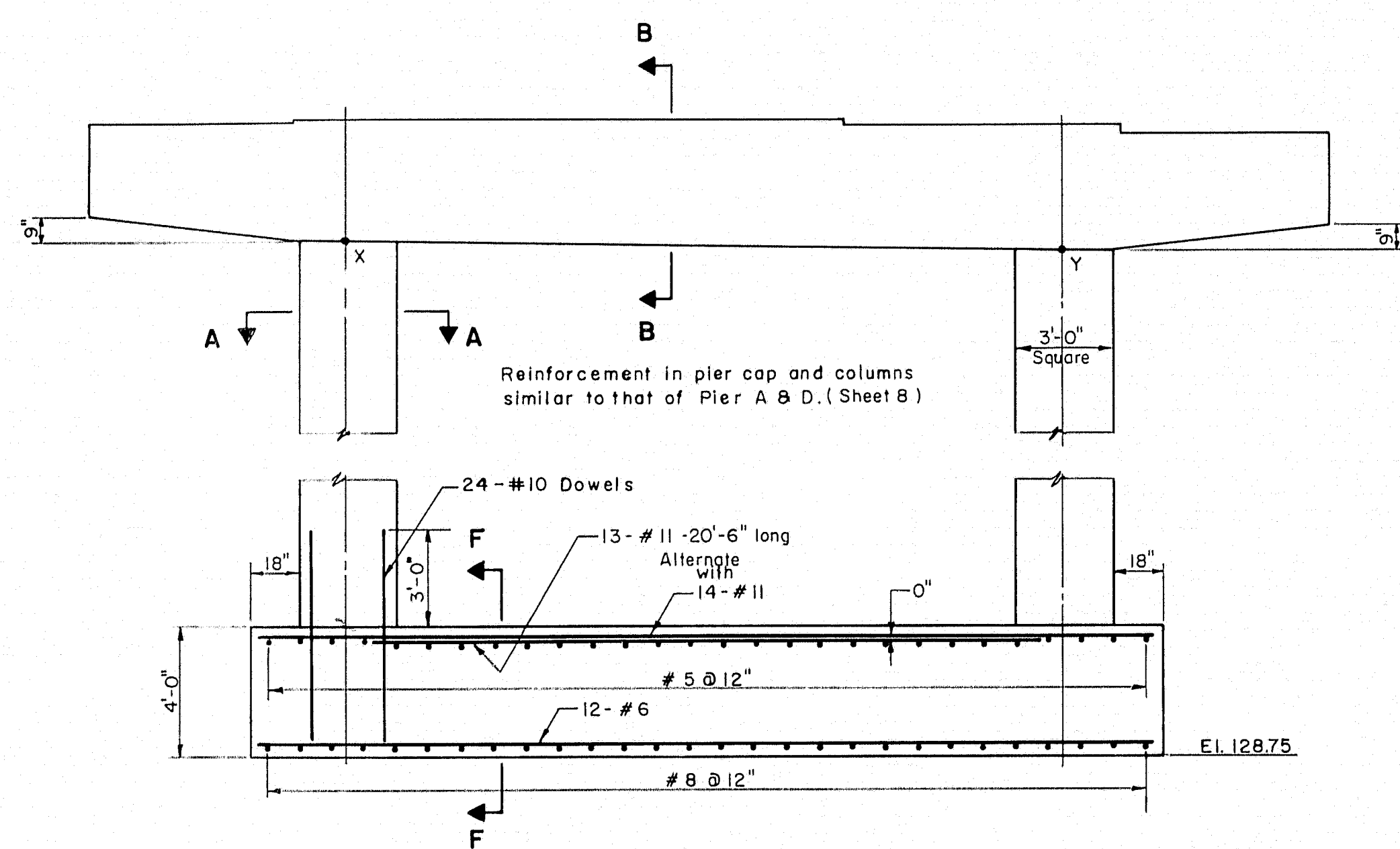
THE CLARKSON ENGINEERING CO., INC.		
DESIGN D.M.S.	CHECK J.I.M. - H.R.	BRIDGE NO.
DRAWN E.K.	APPROVED W.A.H. - C.J.M.	SURVEY
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
OVER		
MAINE CENTRAL R.R. & COUNTY ROAD		
IN THE CITY OF		
WATERVILLE		
KENNEBEC COUNTY		
PIERS A, C, D & F		
SHEET 8 OF 12, SHEETS		
AUGUSTA, MAINE		

WATERVILLE INTERSTATE



PLAN PIER B

Scale: 1/4" = 1'-0"



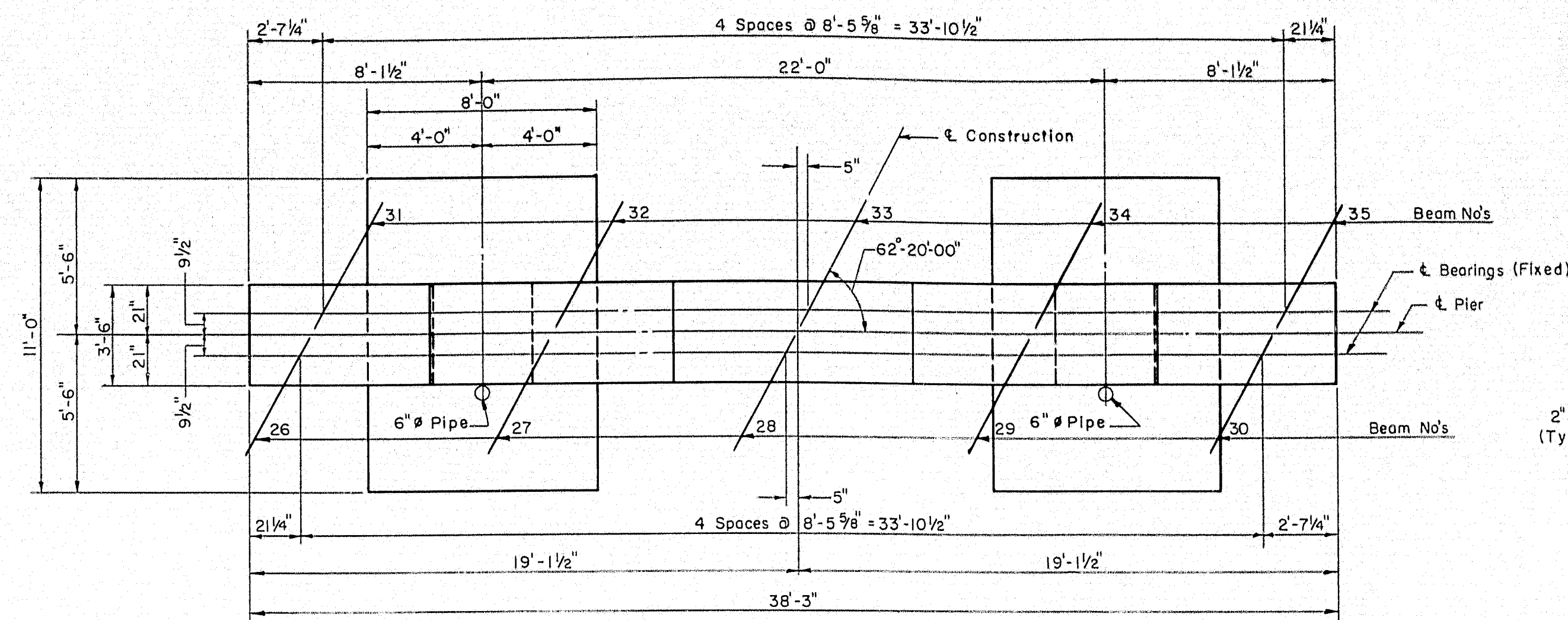
ELEVATION PIER B

Scale: 1/4" = 1'-0"

Maximum Bearing Pressure 5,200 p.s.f.

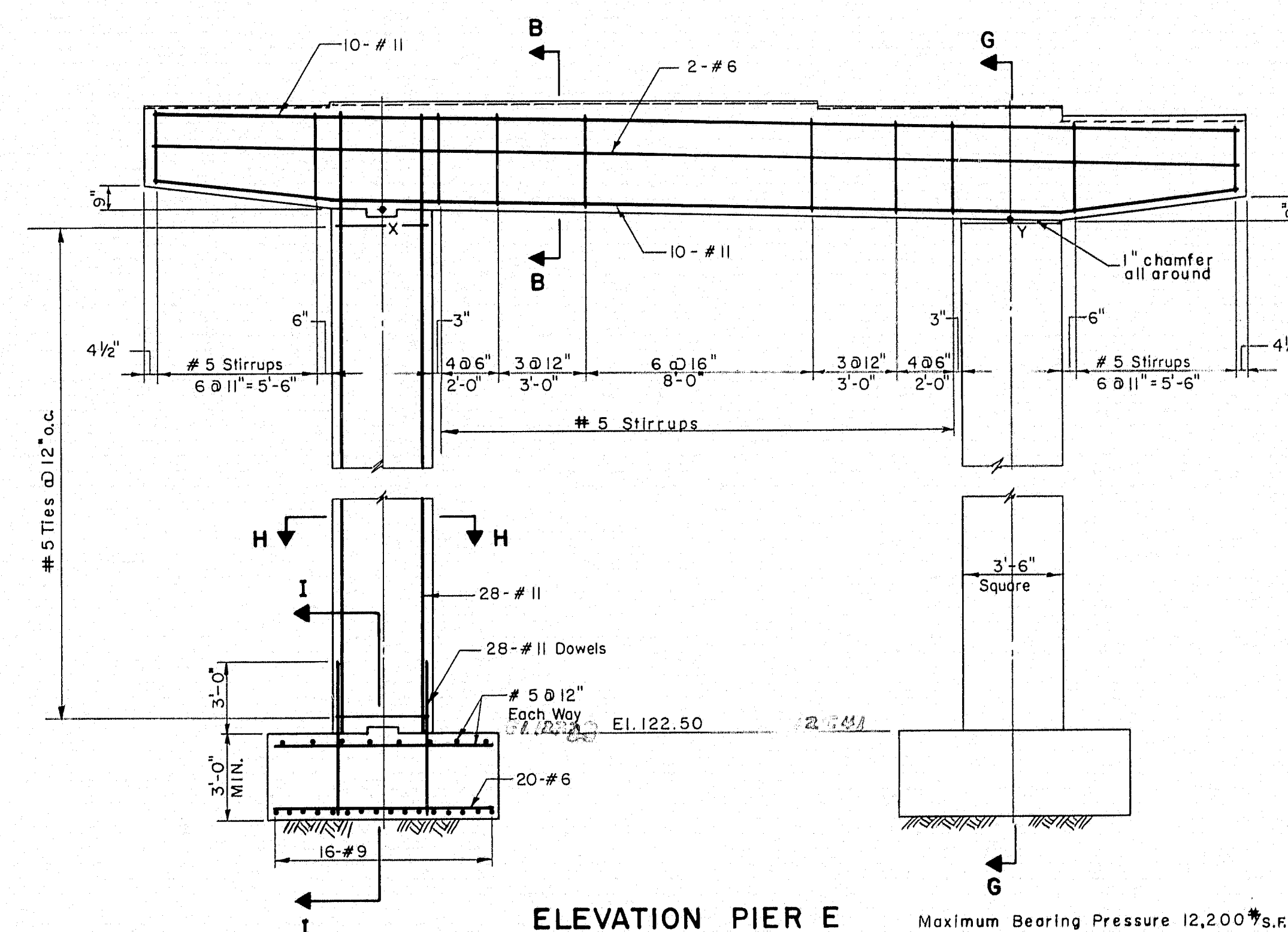
SCHEDULE OF BEAM SEAT ELEVATIONS												
Beam No.	Pier A	Beam No.	Pier B	Beam No.	Pier C	Beam No.	Pier D	Beam No.	Pier E	Beam No.	Pier F	Beam No.
1	168.63	6	166.86	11	165.42	16	167.40	21	167.27	26	165.70	31
2	168.80	7	166.98	12	165.53	17	167.62	22	167.27	27	165.81	32
3	168.75	8	166.98	13	165.53	18	167.63	23	167.28	28	165.81	33
4	168.54	9	166.81	14	165.36	19	167.41	24	167.06	29	165.64	34
5	168.11	10	166.54	15	165.09	20	166.98	25	166.85	30	165.37	35

ELEVATION AT TOP OF COLUMNS		
Pier	X	Y
A	165.00	164.50
B	163.30	163.00
C	161.90	161.50
D	163.70	163.30
E	162.10	161.80
F	160.60	160.30



PLAN PIER E

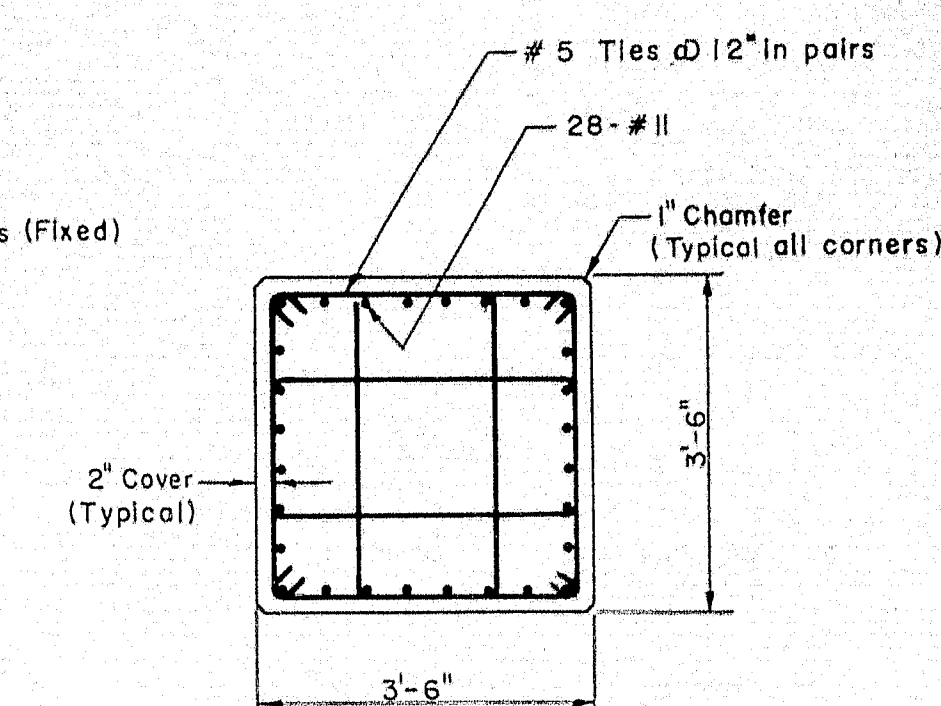
Scale: 1/4" = 1'-0"



ELEVATION PIER E

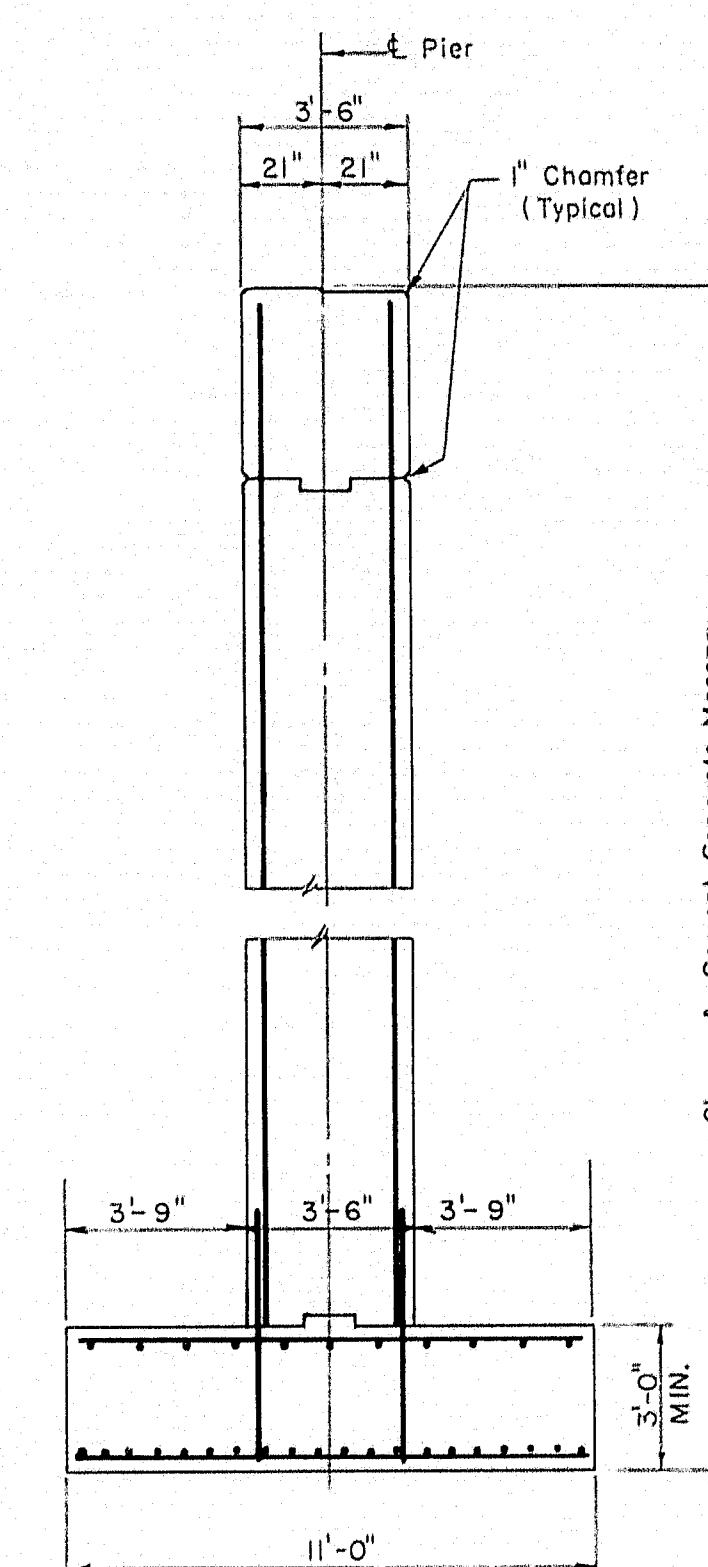
Scale: 1/4" = 1'-0"

Maximum Bearing Pressure 12,200 p.s.f.



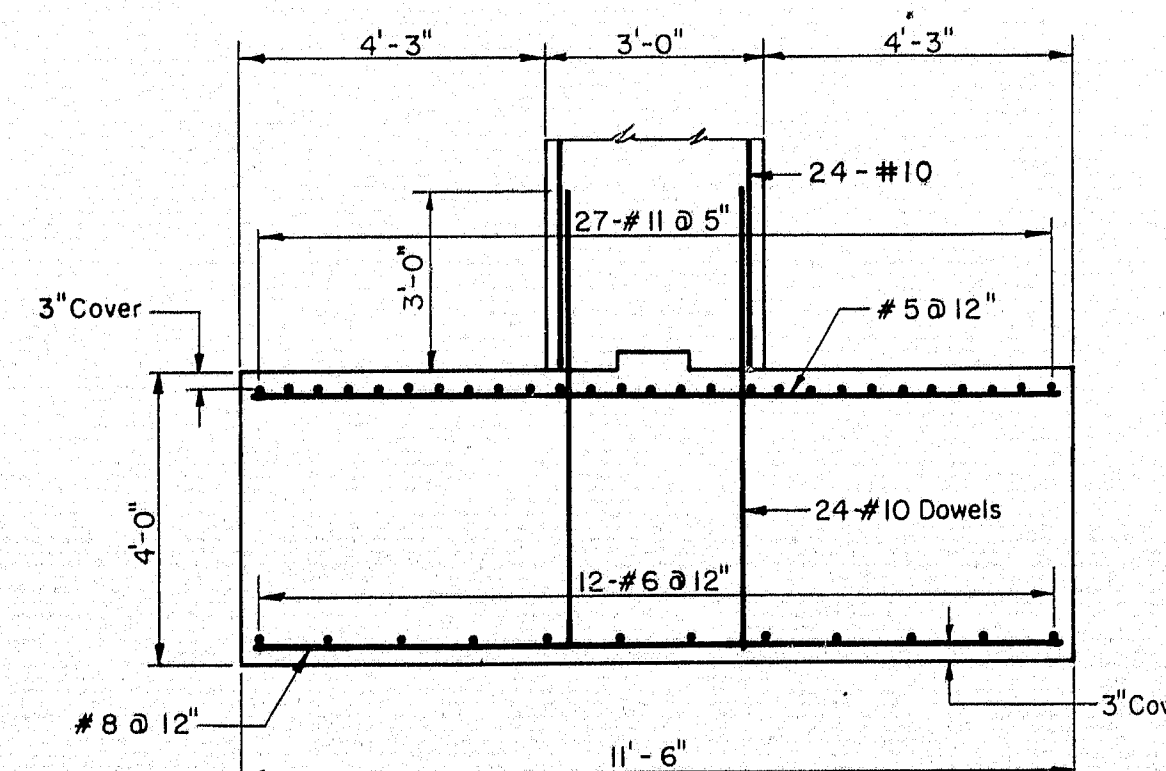
SECTION H-H

Scale: 1/2" = 1'-0"



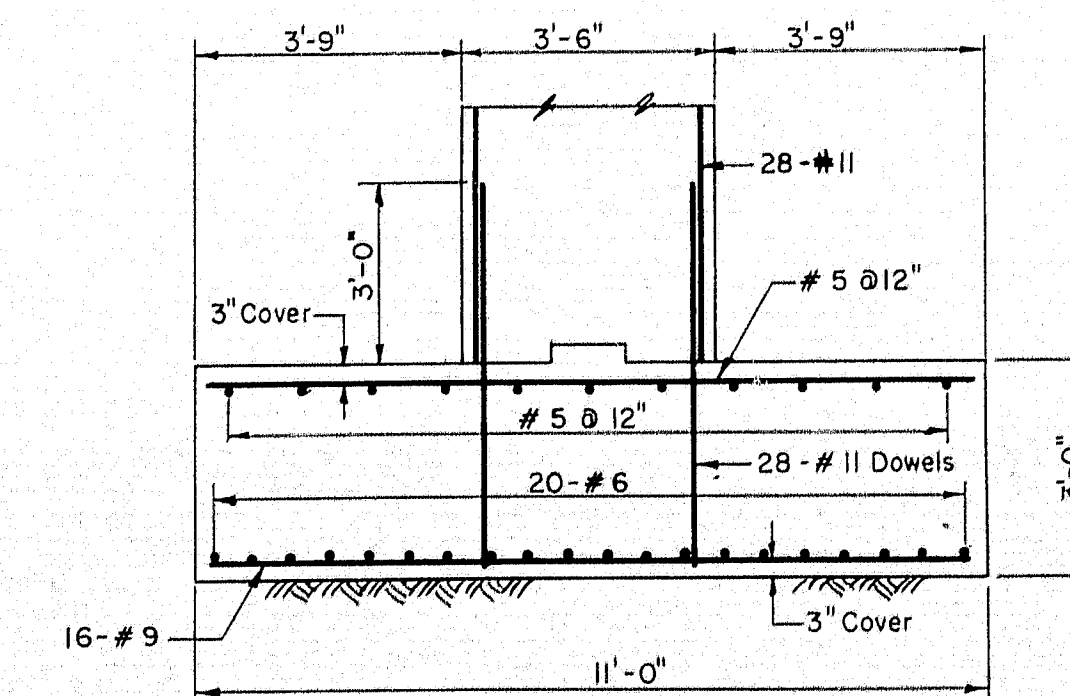
SECTION G-G

Scale: 1/4" = 1'-0"



SECTION F-F

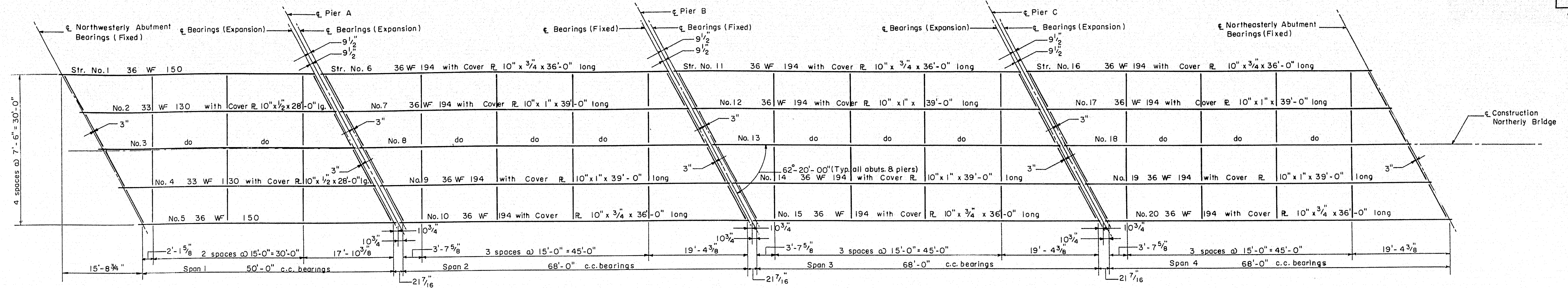
Scale: 3/8" = 1'-0"



SECTION I-I

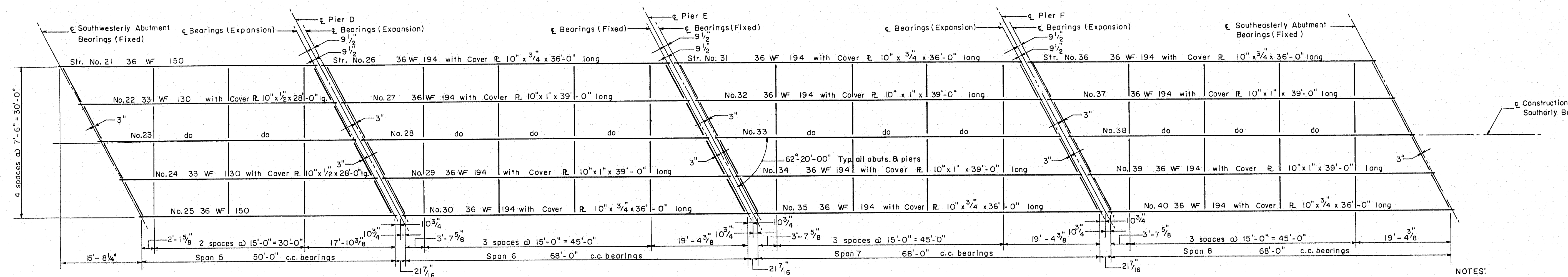
Scale: 3/8" = 1'-0"

THE CLARKSON ENGINEERING CO., INC.		
DESIGN D.M.S.	CHECK J.L.M. - H.P.	BRIDGE NO. SURVEY PLOT
DRAWN E.K.	APPROVED W.A.H. - C.W.M.	
STATE HIGHWAY COMMISSION INTERSTATE #95 OVER MAINE CENTRAL R.R. & COUNTY ROAD IN THE CITY OF WATERVILLE KENNEBEC COUNTY PIERS B & E		
SHEET 9 OF 12 SHEETS		AUGUSTA, MAINE



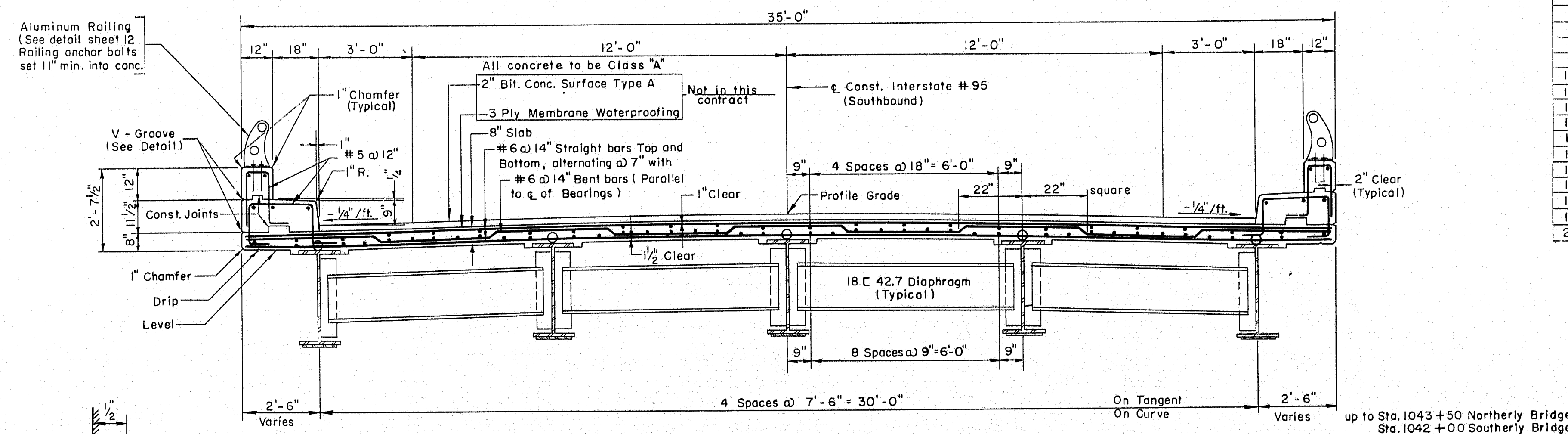
FRAMING PLAN NORTHERLY BRIDGE

Scale: $\frac{3}{32}'' = 1'-0''$



FRAMING PLAN SOUTHERLY BRIDGE

Scale: $\frac{3}{32}'' = 1'-0''$



TYPICAL CROSS SECTION

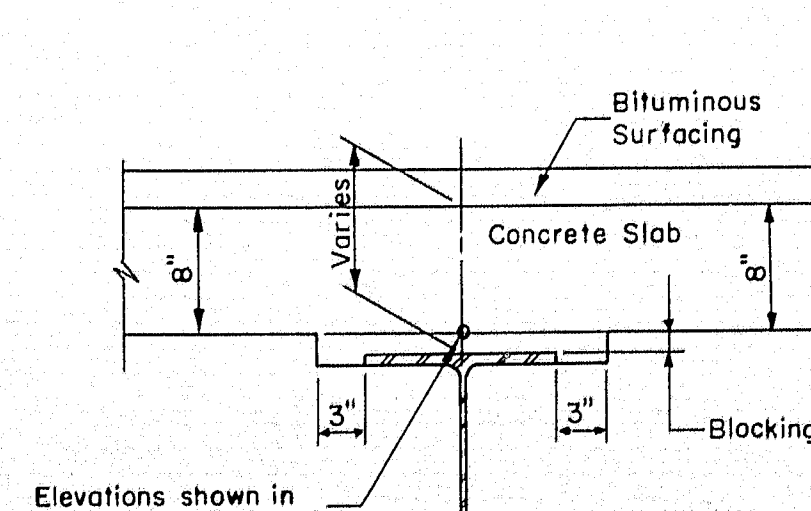
Scale: $\frac{3}{8}'' = 1'-0''$

- Notes:
- All longitudinal reinforcing to be #4 Bars spaced as shown (Parallel to ϵ Construction)
 - Normal cross section as shown from Sta. 1043+50' Northernly Bridge and Sta. 1042+00 Southernly Bridge. Banked section up to these stations (see Bank Data on Sheet 2)

V-GROOVE DETAIL

Not to Scale

ELEVATIONS OF BOTTOM OF ROADWAY SLAB											
Beam No.	ϵ Brg.	$\frac{1}{4}$ Point	$\frac{1}{2}$ Point	$\frac{3}{4}$ Point	ϵ Brg.	Beam No.	ϵ Brg.	$\frac{1}{4}$ Point	$\frac{1}{2}$ Point	$\frac{3}{4}$ Point	ϵ Brg.
1	173.38	173.11	172.82	172.51	172.17	21	172.15	171.88	171.59	171.27	170.94
2	173.24	173.00	172.73	172.43	172.12	22	172.08	171.83	171.56	171.26	170.94
3	173.12	172.89	172.64	172.37	172.07	23	172.03	171.79	171.53	171.25	170.95
4	172.91	172.68	172.43	172.16	171.86	24	171.81	171.59	171.32	171.04	170.73
5	172.69	172.46	172.21	171.94	171.65	25	171.60	171.36	171.10	170.82	170.52
6	172.13	171.79	171.43	170.97	170.50	26	170.90	170.57	170.20	169.79	169.34
7	172.08	171.78	171.40	170.97	170.61	27	170.90	170.60	170.26	169.87	169.44
8	172.04	171.74	171.41	171.02	170.60	28	170.91	170.60	170.26	169.87	169.44
9	171.82	171.54	171.22	170.85	170.44	29	170.69	170.40	170.07	169.69	169.27
10	171.61	171.31	170.98	170.59	170.17	30	170.48	170.17	169.82	169.43	169.00
11	170.46	170.17	169.84	169.46	169.05	31	169.30	168.99	168.65	168.26	167.83
12	170.57	170.28	169.95	169.57	169.16	32	169.40	169.10	168.76	168.36	167.94
13	170.57	170.28	169.95	169.57	169.16	33	169.40	169.09	168.75	168.36	167.93
14	170.41	170.11	169.79	169.41	169.00	34	169.23	168.93	168.59	168.19	167.77
15	170.13	169.84	169.51	169.13	168.72	35	168.96	168.65	168.31	167.92	167.49
16	169.01	168.72	168.38	168.01	167.59	36	167.79	167.49	167.14	166.75	166.32
17	169.12	168.83	168.49	168.12	167.70	37	167.90	167.59	167.25	166.86	166.43
18	169.12	168.82	168.49	168.11	167.70	38	167.89	167.59	167.25	166.85	166.42
19	168.96	168.66	168.33	167.95	167.53	39	167.73	167.42	167.08	166.69	166.26
20	168.68	168.39	168.05	167.68	167.26	40	167.45	167.15	166.80	166.41	165.98



DETAIL OF HAUNCH

Scale: 1" = 1'-0"

NOTE:
After the structural steel is erected and before forms are built or concrete is placed elevations on the top flange of the beams are to be obtained at the points indicated in the table. The difference between these elevations and those shown in the table give the actual blocking distance from top of beam to bottom of slab. See Haunch Detail.

THE CLARKSON ENGINEERING CO., INC.

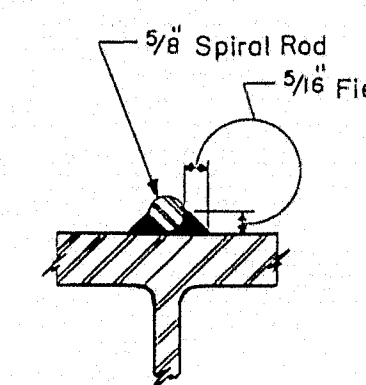
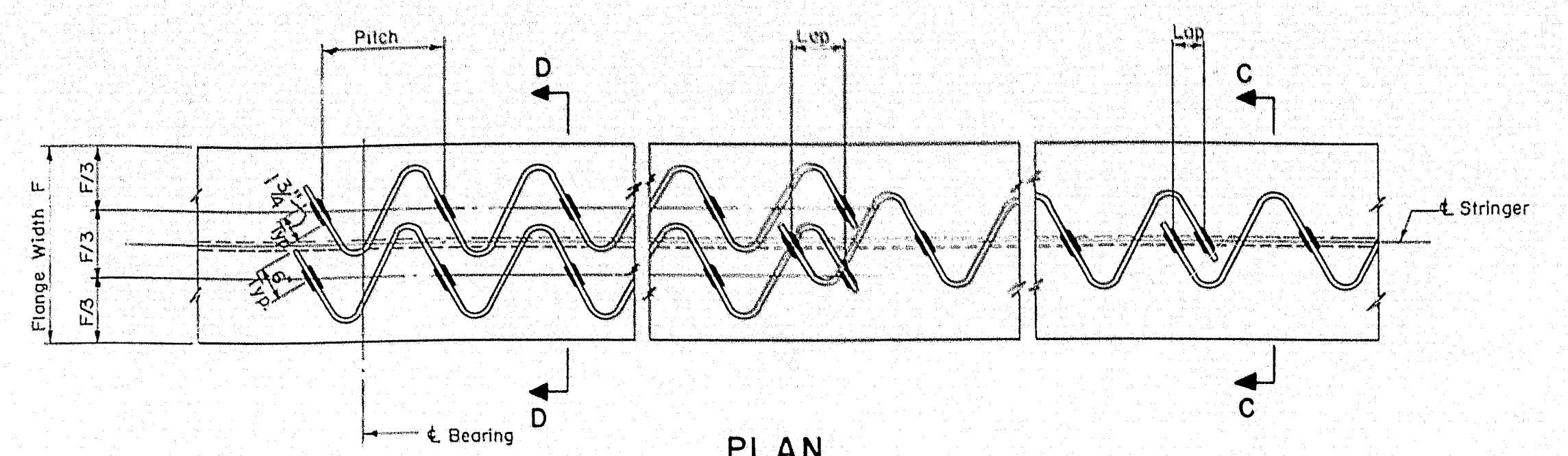
DESIGN J.T.M.	CHECK J.T.-H.P.	BRIDGE NO. SURVEY
DRAWN J.T.-V.S.	APPROVED W.A.H.-C.J.M.	PLOT

STATE HIGHWAY COMMISSION

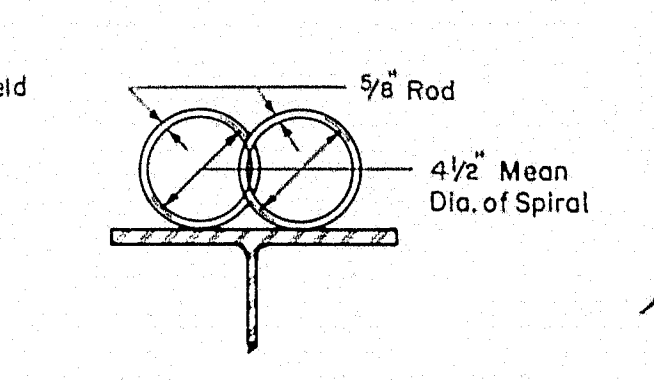
INTERSTATE #95
OVER
MAINE CENTRAL R.R. & COUNTY ROAD
IN THE CITY OF
WATERVILLE
KENNEBEC COUNTY
FRAMING PLAN & TYPICAL CROSS SECTION
SHEET 10 OF 12 SHEETS AUGUSTA, MAINE

R.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-6(23)	17	24

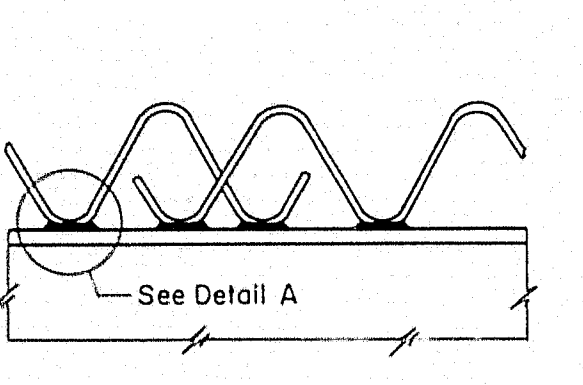
WATERVILLE INTERSTATE



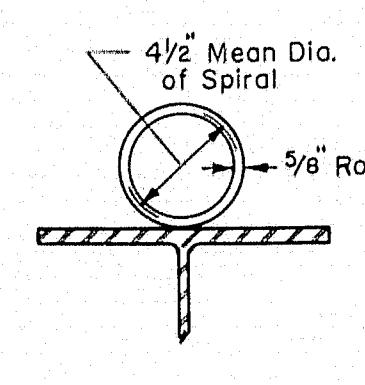
DETAIL A



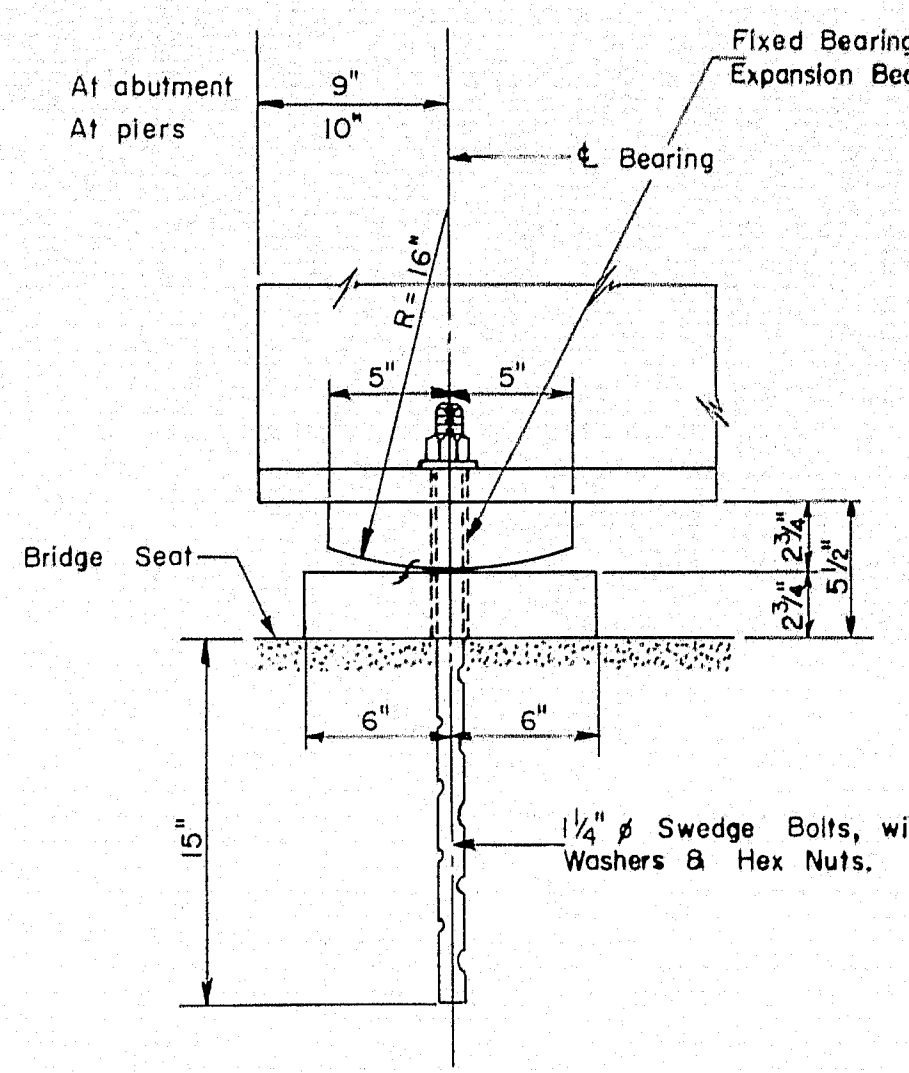
SECTION D-D



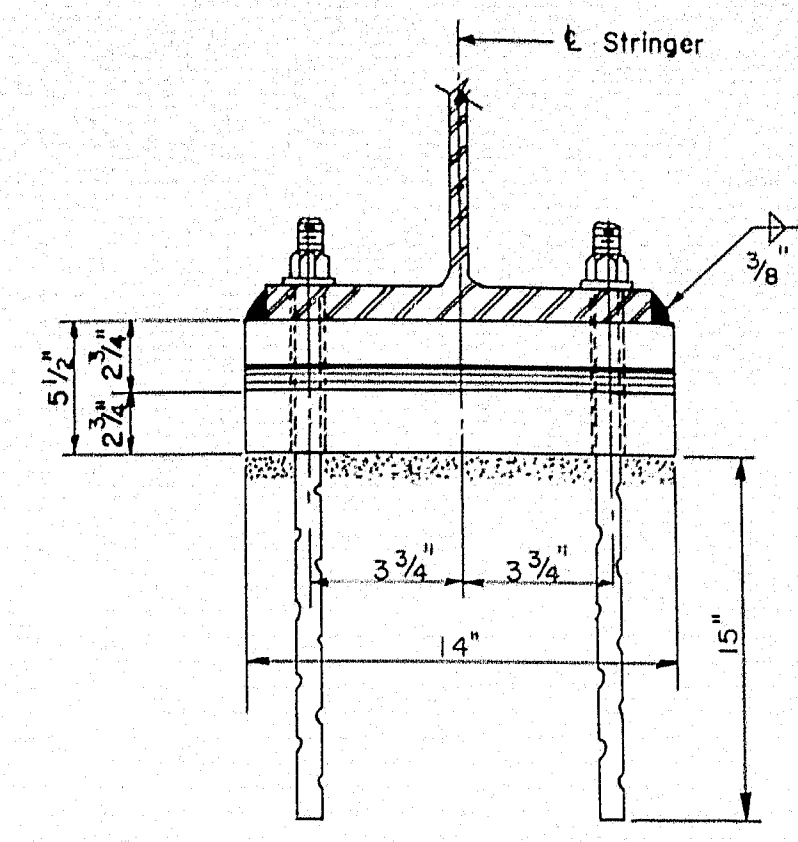
ELEVATION



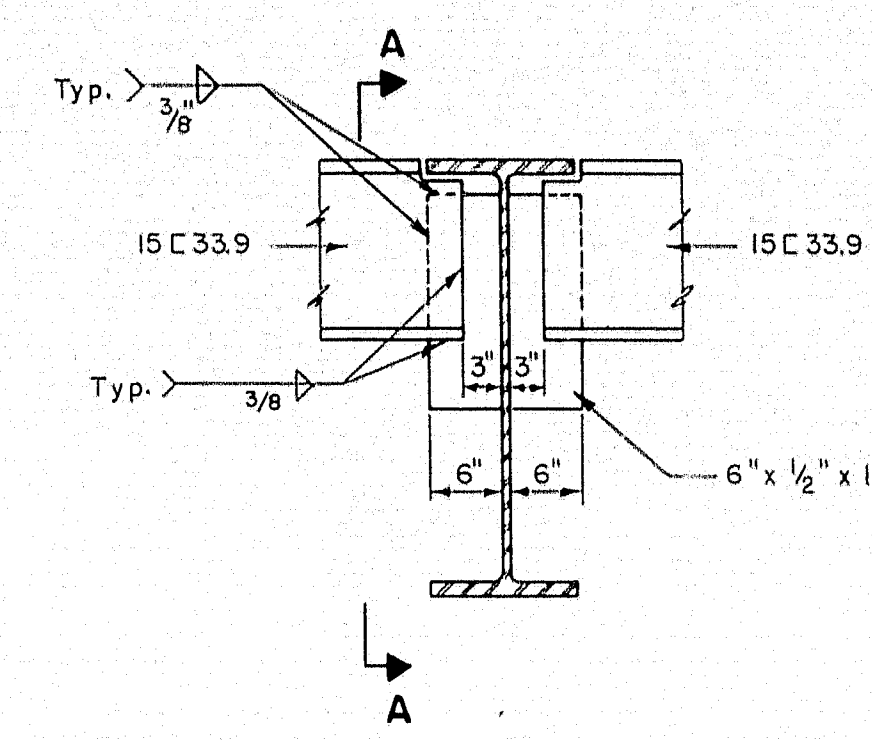
SECTION C-C



EXPANSION & FIXED BEARING DETAIL



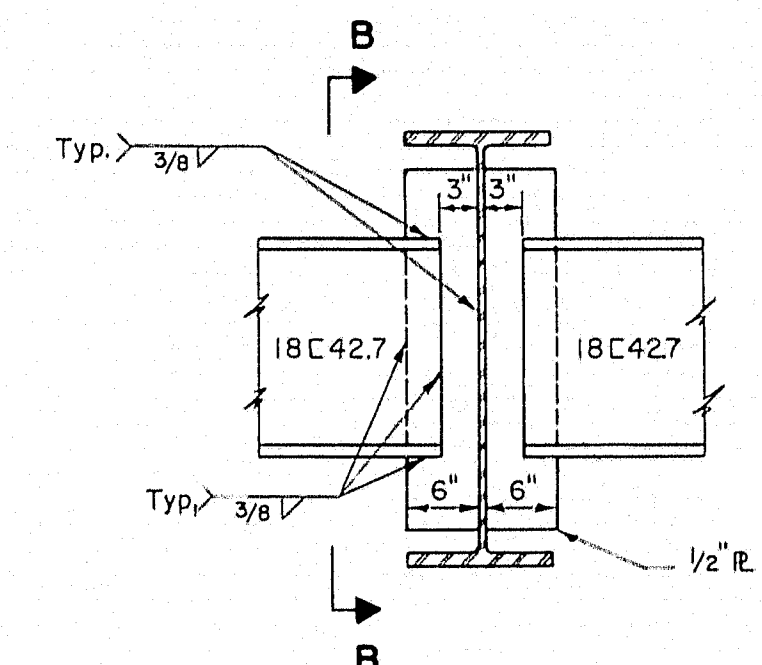
Note: All anchor bolts to be centered true before bearing plates are set in position.



SECTION A-A

WELDED END DIAPHRAGM CONNECTION

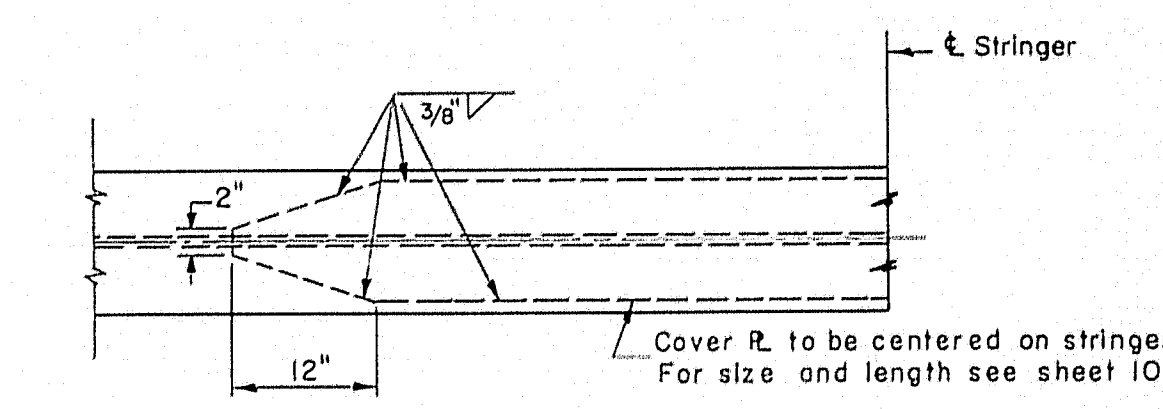
Scale: 3/4" = 1'-0"
Note: Welds to be shop or field welded.



SECTION B-B

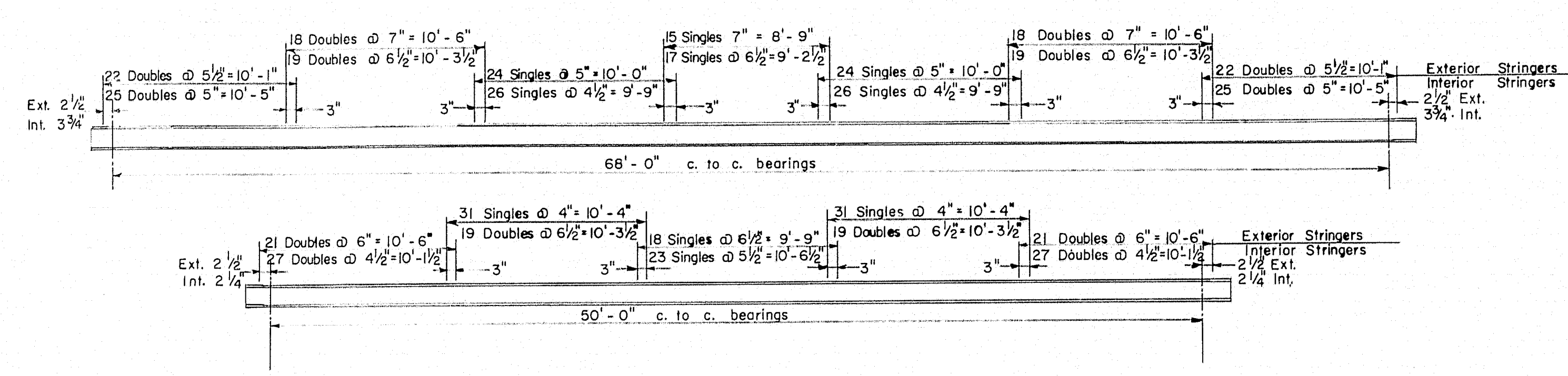
WELDED DIAPHRAGM CONNECTION

Scale: 3/4" = 1'-0"



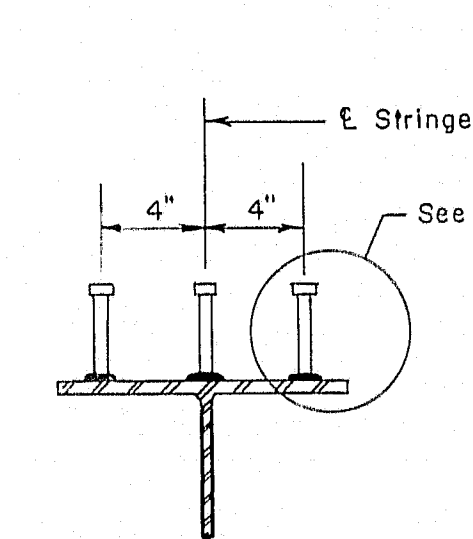
COVER PLATE DETAIL

NOT TO SCALE

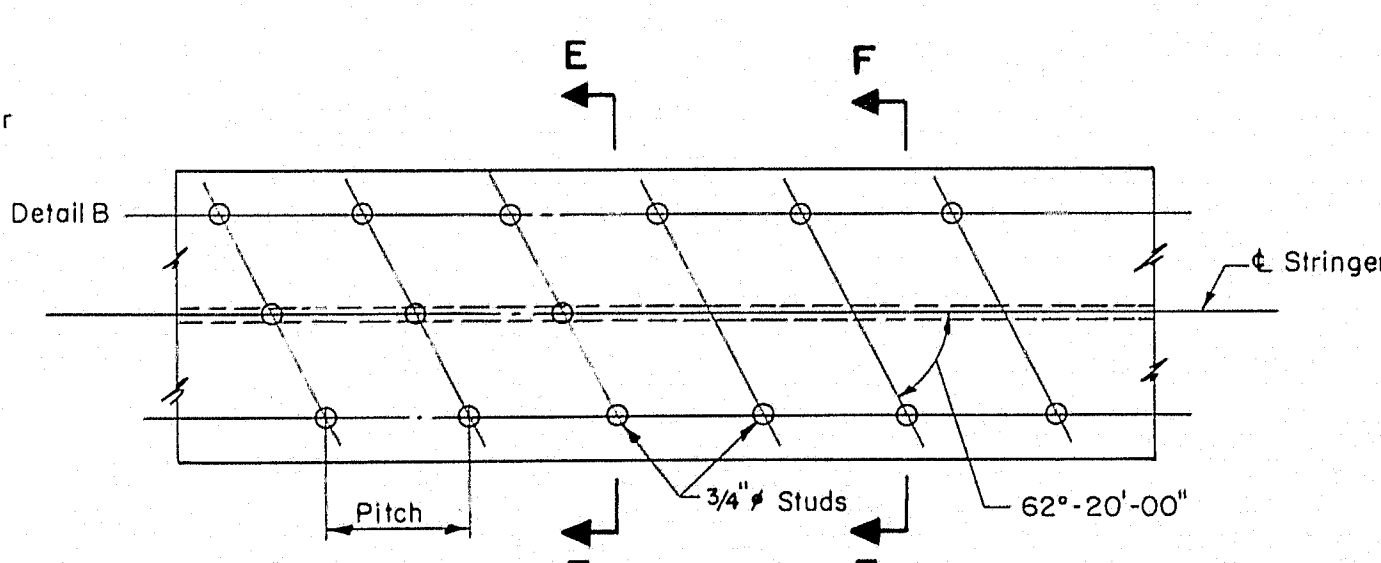


TYPICAL SPIRAL SHEAR CONNECTORS (DETAILS & LAYOUT)

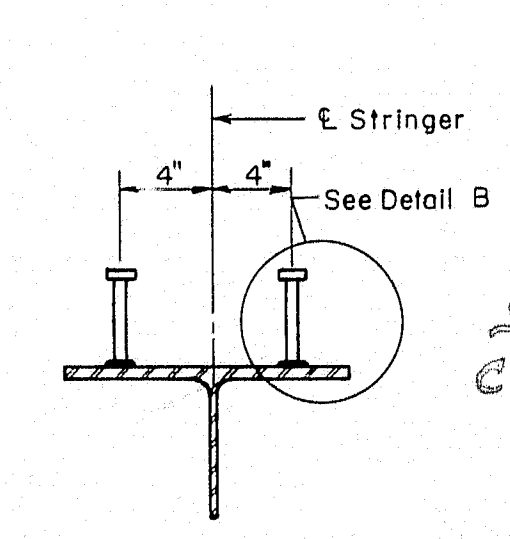
NOT TO SCALE



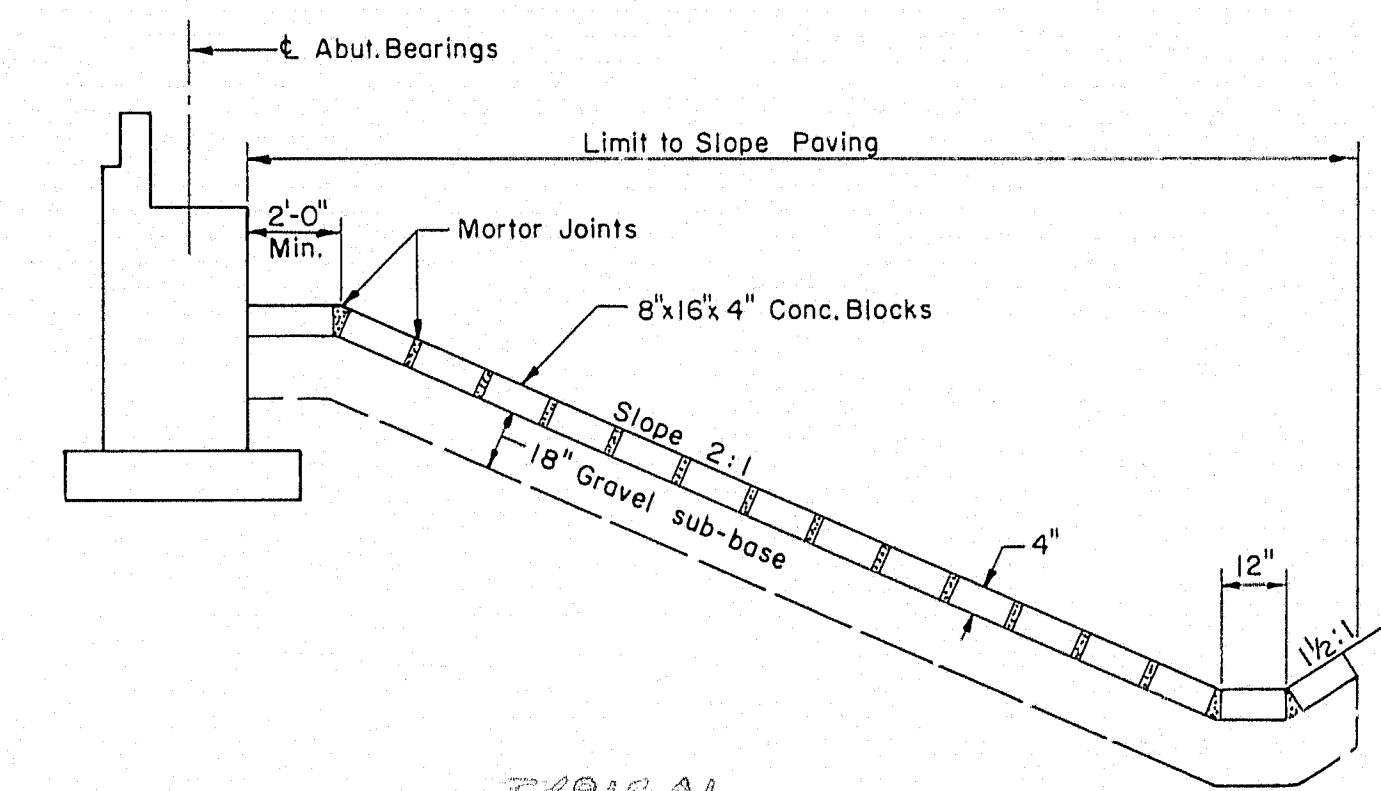
SECTION E-E



PLAN



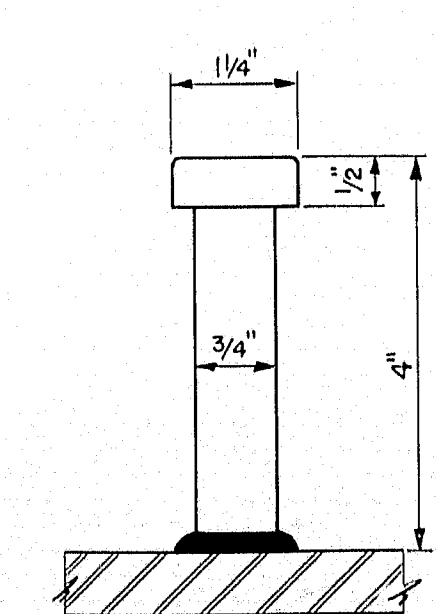
SECTION F-F



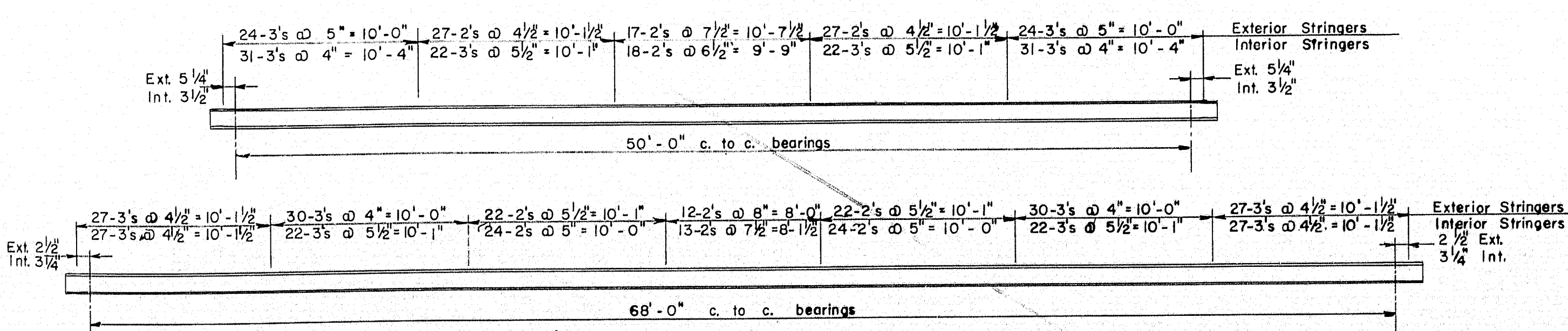
TYPICAL SLOPE PAVING DETAIL (WESTERLY ABUTMENTS)

Not to Scale

Note: At Easterly Abutments stop slope paving at existing ground.



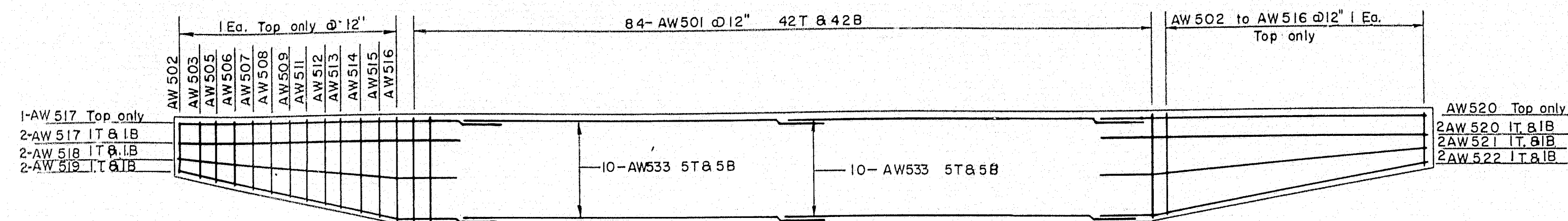
DETAIL B



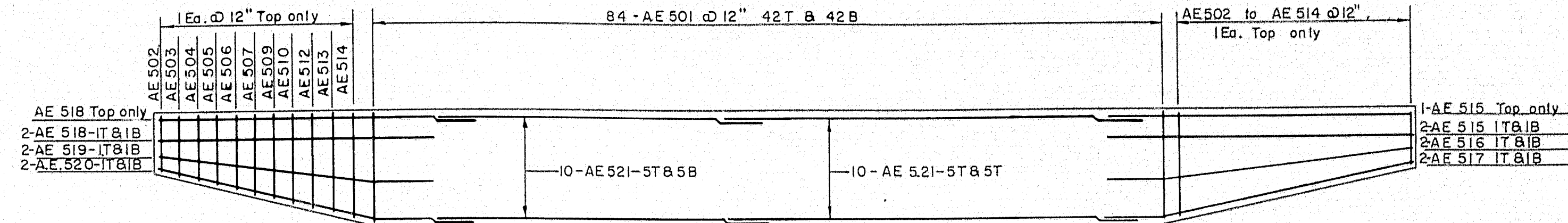
TYPICAL STUD SHEAR CONNECTORS (DETAILS & LAYOUT)

NOT TO SCALE

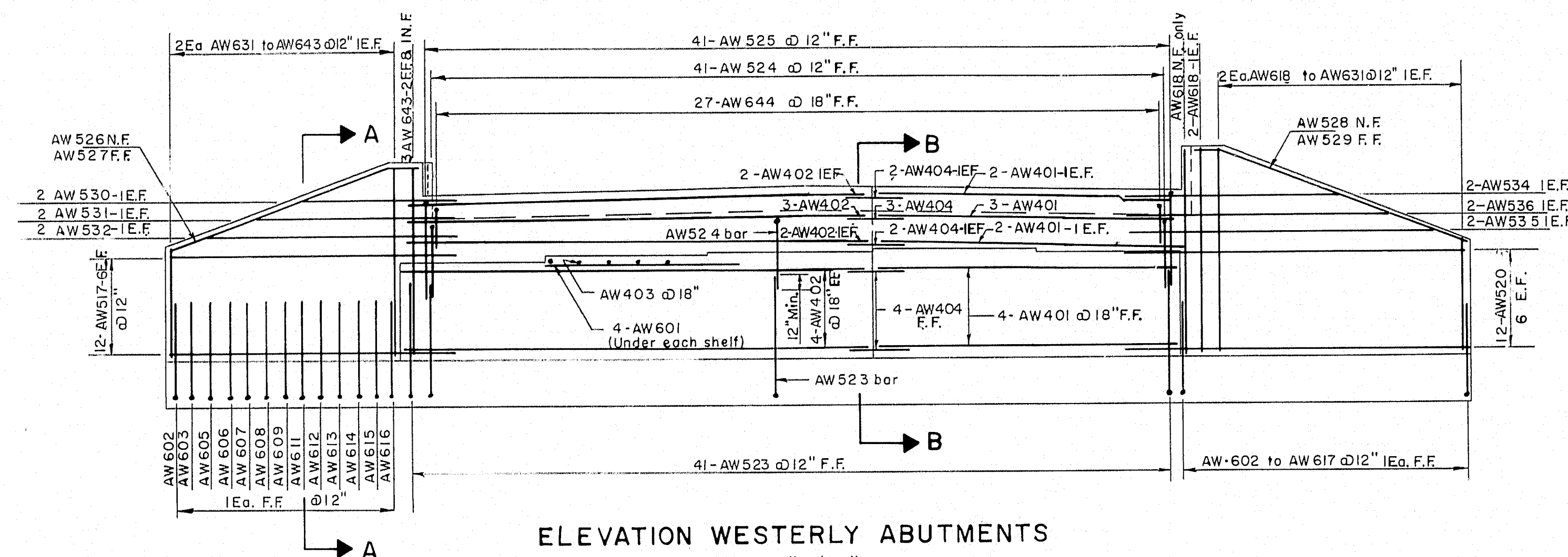
THE CLARKSON ENGINEERING CO., INC.		
DESIGN R.F.-J.T.	CHECK J.I.M. -H.R.	BRIDGE NO. SURVEY
DRAWN E.K.	APPROVED WAH-CJM	PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
OVER		
MAINE CENTRAL R.R. & COUNTY ROAD		
IN THE CITY OF WATERVILLE		
KENNEBEC COUNTY		
SUPERSTRUCTURE DETAILS		
SHEET 11 OF 12 SHEETS		AUGUSTA, MAINE



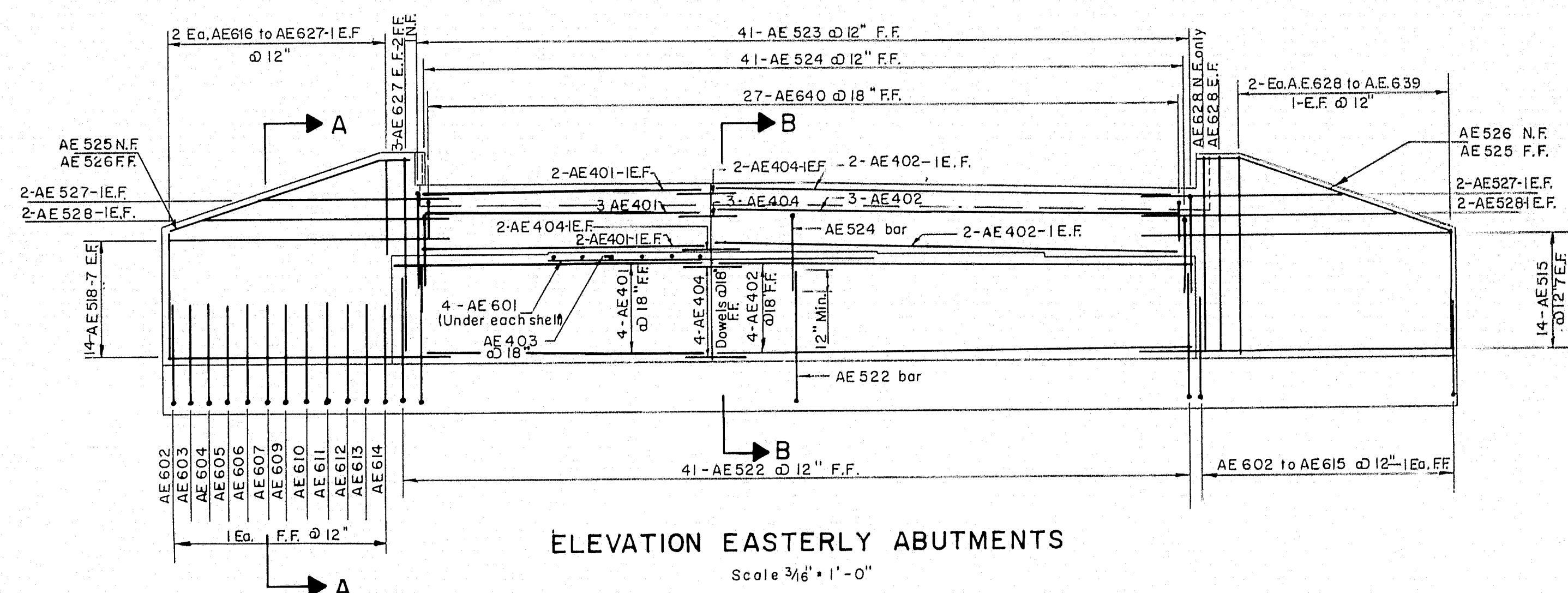
PLAN WESTERLY ABUTMENTS
Scale $\frac{3}{16}'' = 1' - 0''$



PLAN EASTERLY ABUTMENTS
Scale $\frac{3}{16}'' = 1' - 0''$



ELEVATION WESTERLY ABUTMENTS
Scale $\frac{3}{16}'' = 1' - 0''$

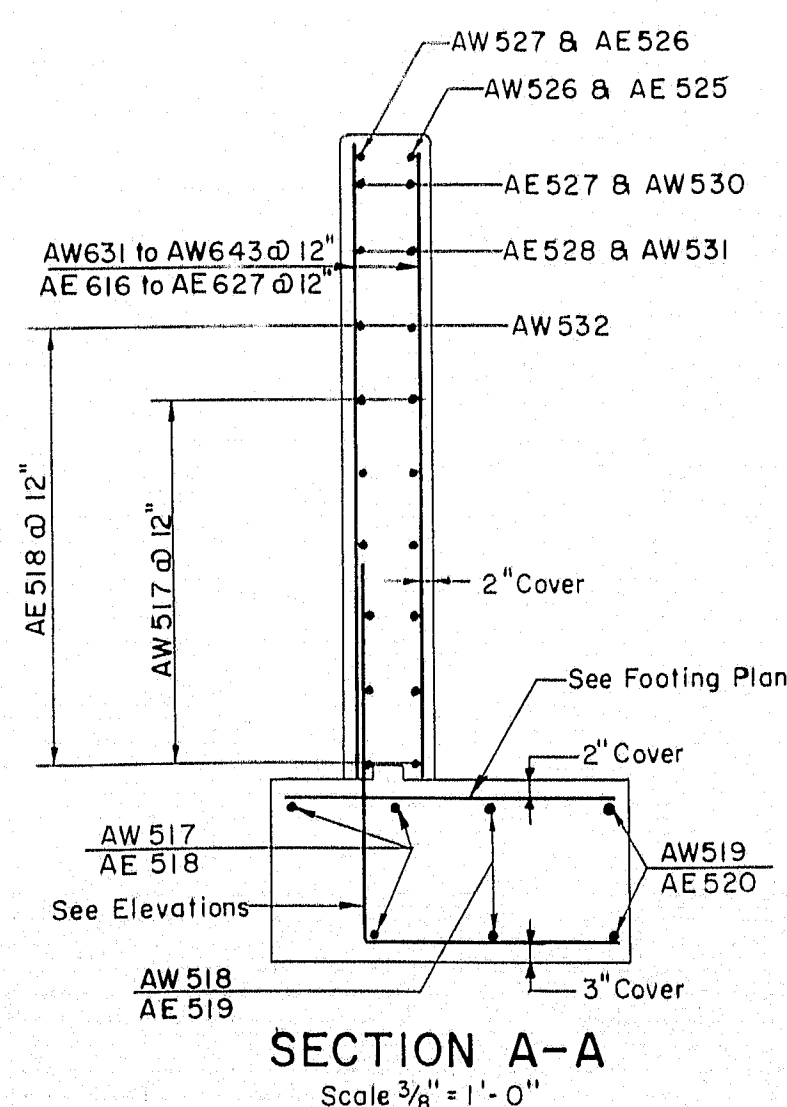


ELEVATION EASTERLY ABUTMENTS
Scale $\frac{3}{16}'' = 1' - 0''$

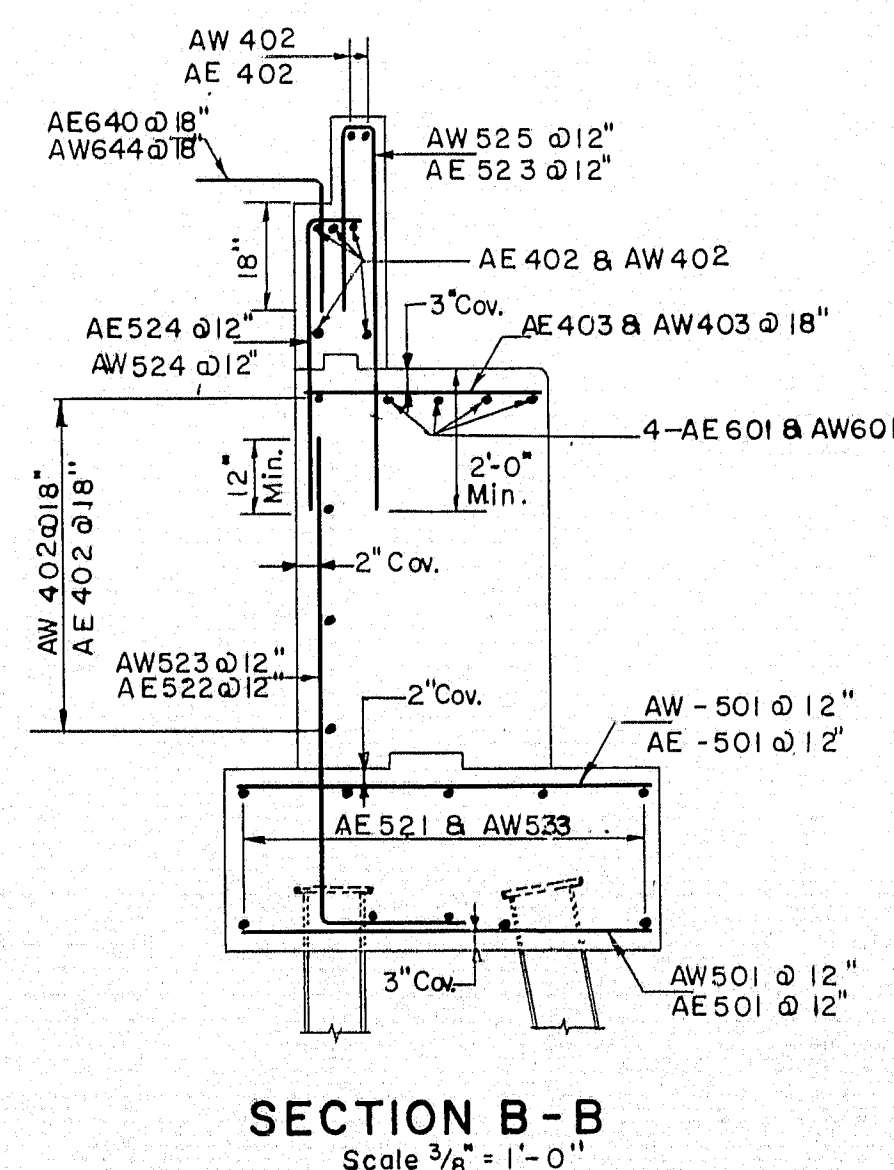
NOTE: Bars Prefixed AW are for Westerly Abutments.
Bars Prefixed AE are for Easterly Abutments.

ESTIMATED QUANTITIES

WESTERLY ABUTMENTS	9,779 LBS.
EASTERLY ABUTMENTS	9,443 LBS.

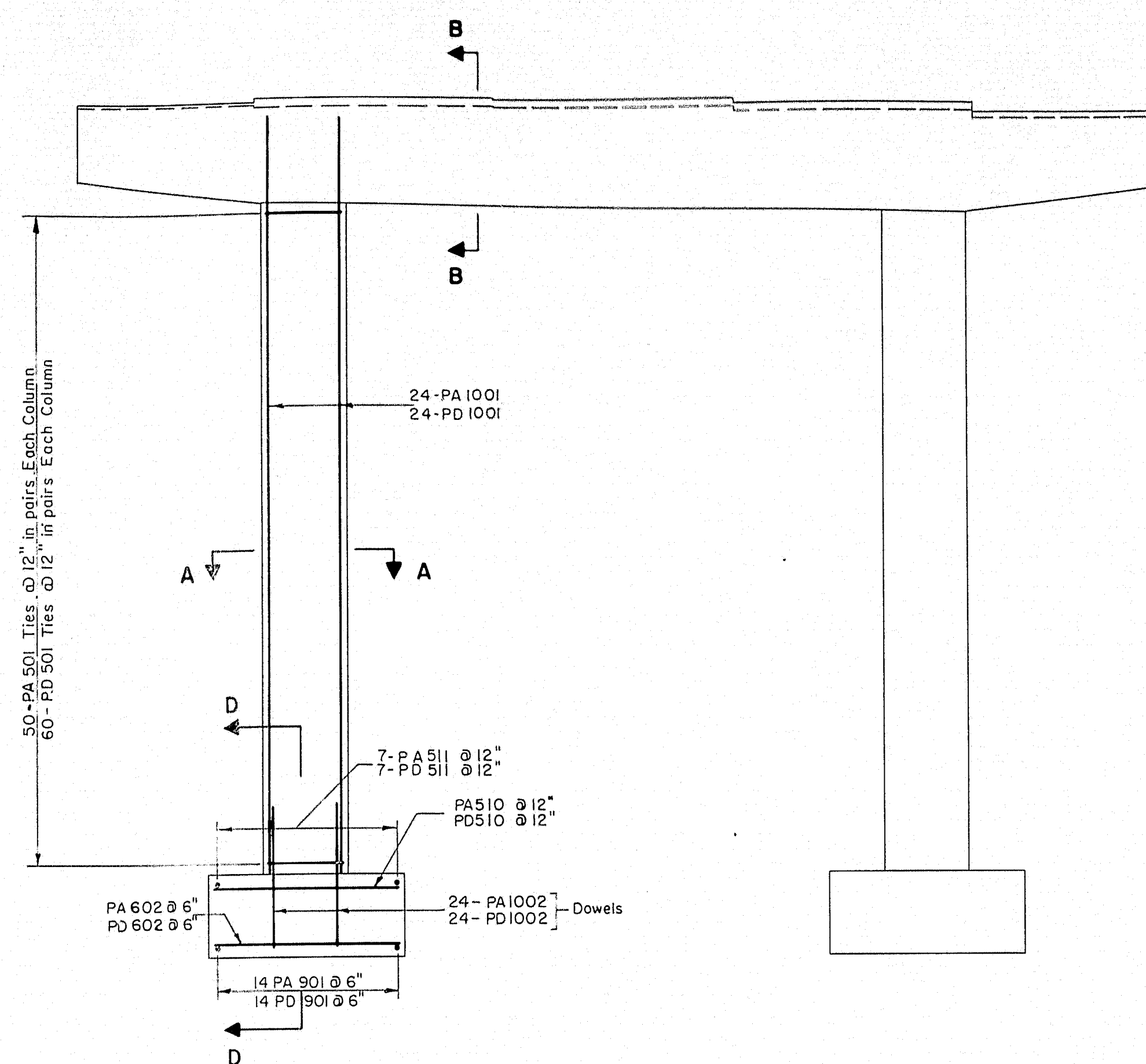


SECTION A-A
Scale $\frac{3}{8}'' = 1' - 0''$



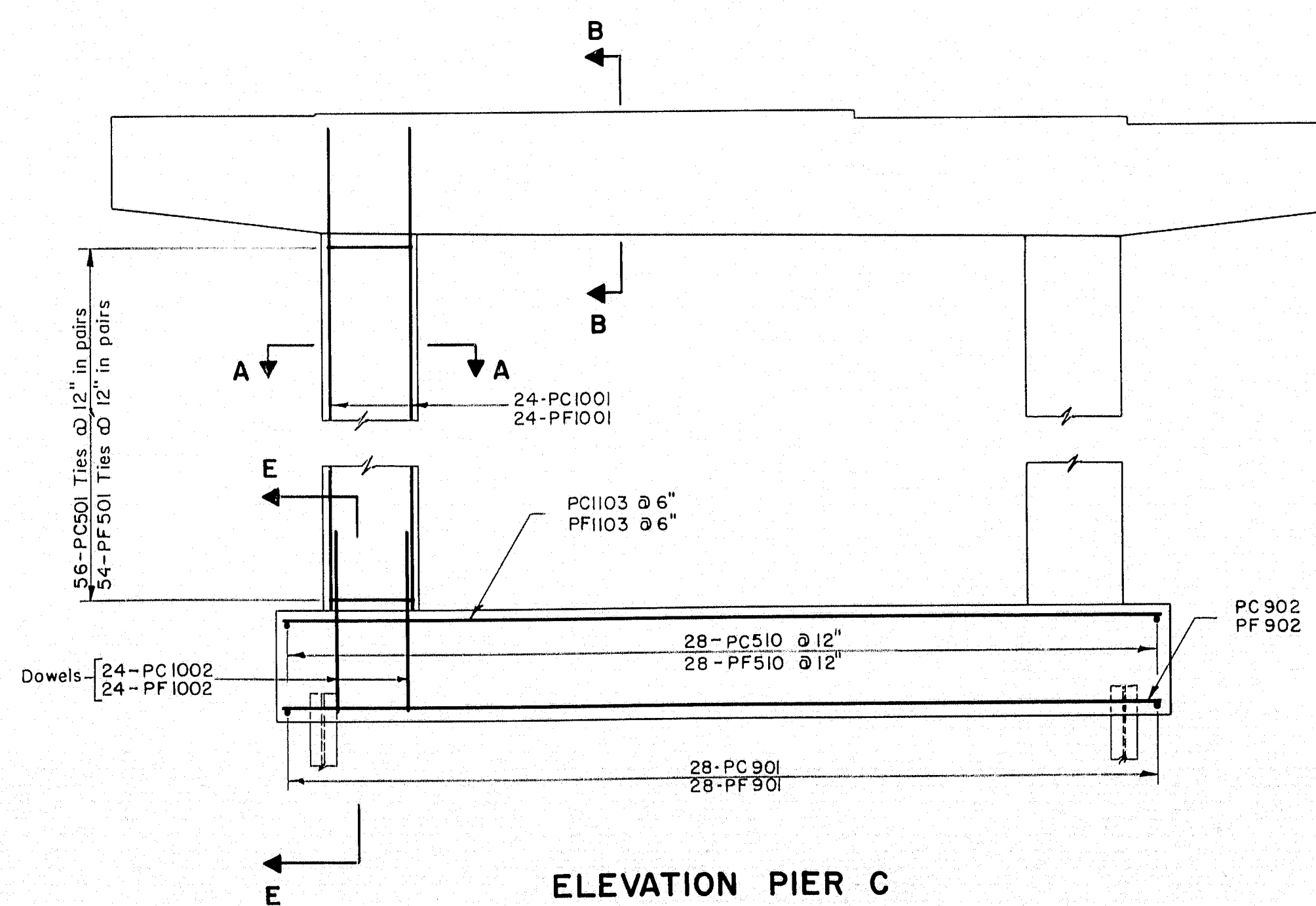
SECTION B-B
Scale $\frac{3}{8}'' = 1' - 0''$

THE CLARKESON ENGINEERING CO., INC.		
DESIGN G.B.	CHECK H.L.	BRIDGE NO.
DRAWN F.E.T.	APPROVED W/H - C.J.M.	SURVEY PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
OVER		
MAINE CENTRAL R.R.		
& COUNTY ROAD		
IN THE CITY OF		
WATERVILLE		
KENNEBEC COUNTY		
ABUTMENT REINFORCEMENT		
SHEET 1 OF 6 SHEETS		AUGUSTA, MAINE



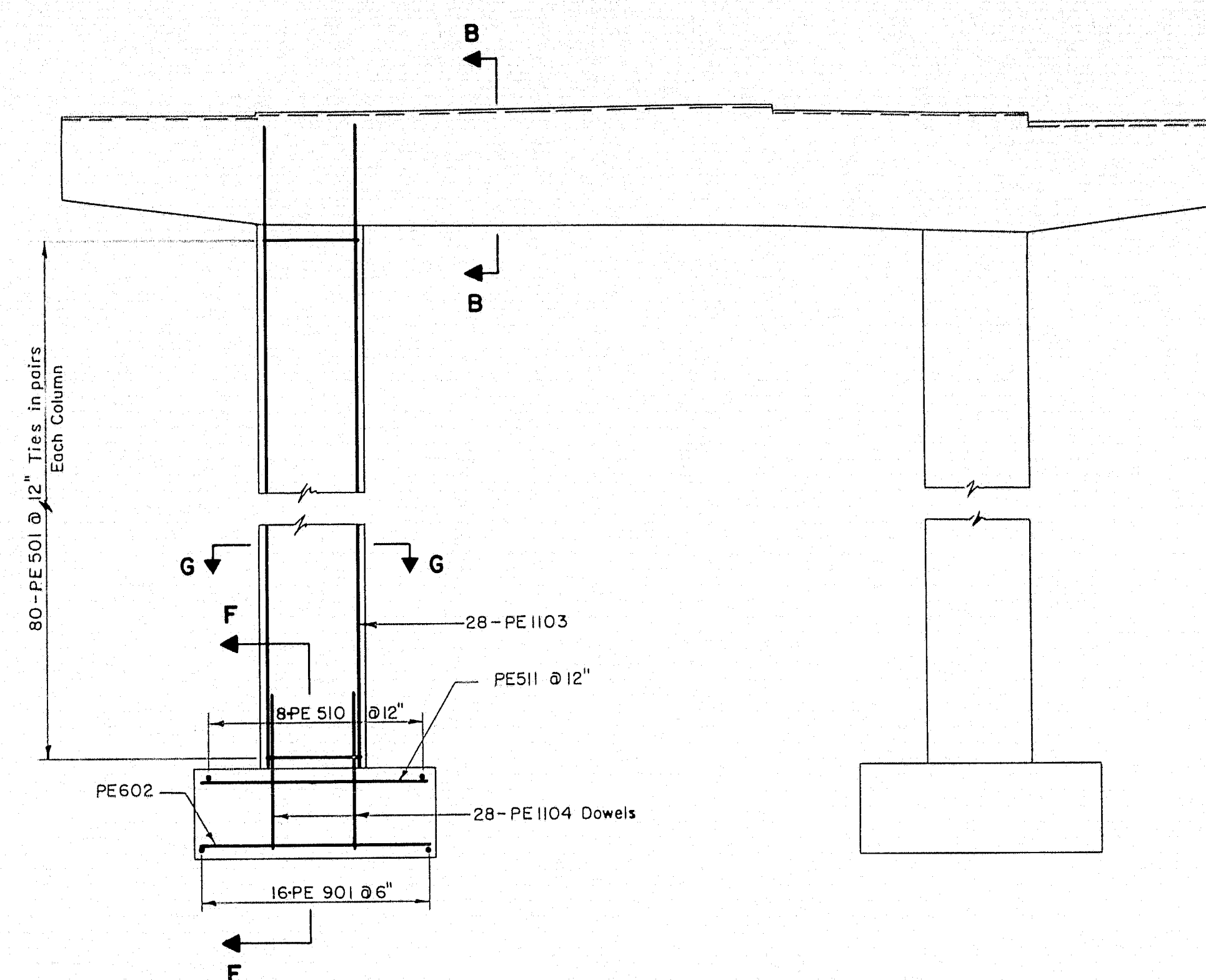
ELEVATION PIER A
ELEVATION PIER D SIMILAR

Scale: 1/4" = 1'-0"



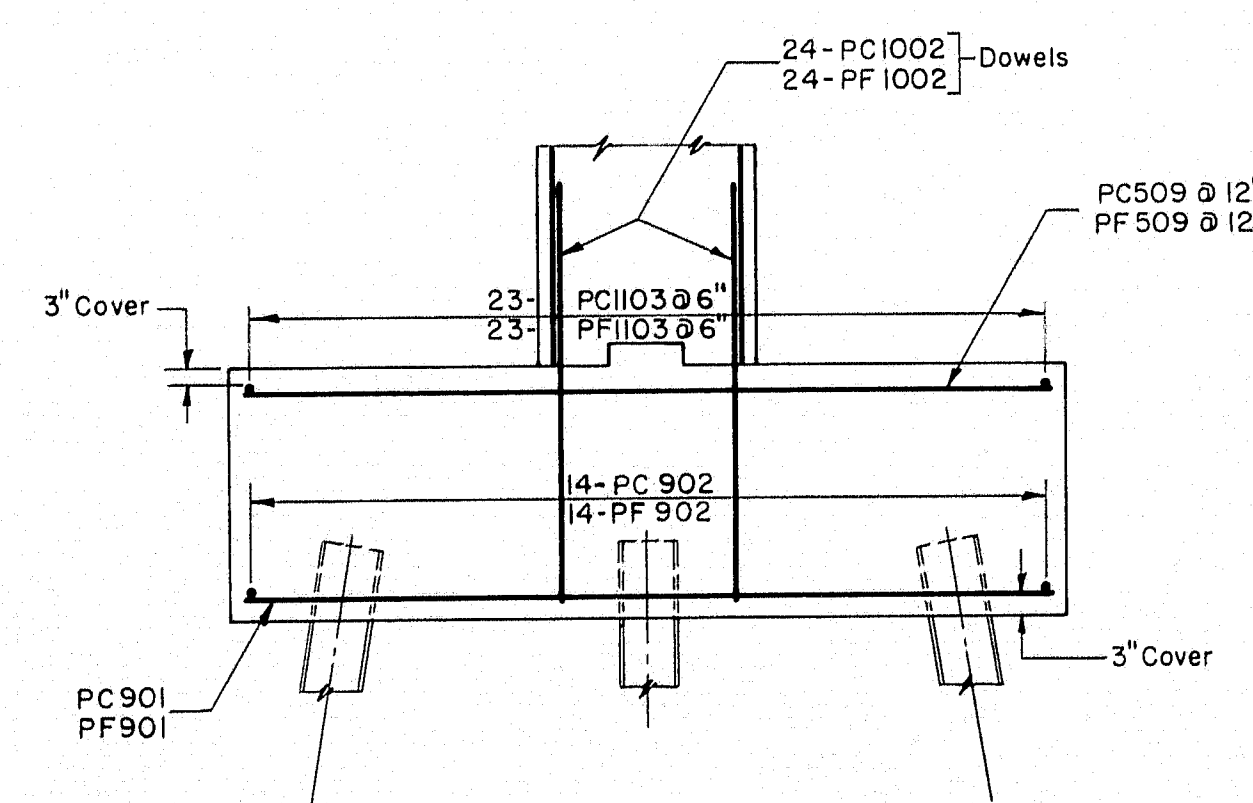
ELEVATION PIER C
ELEVATION PIER F SIMILAR

Scale: 1/4" = 1'-0"



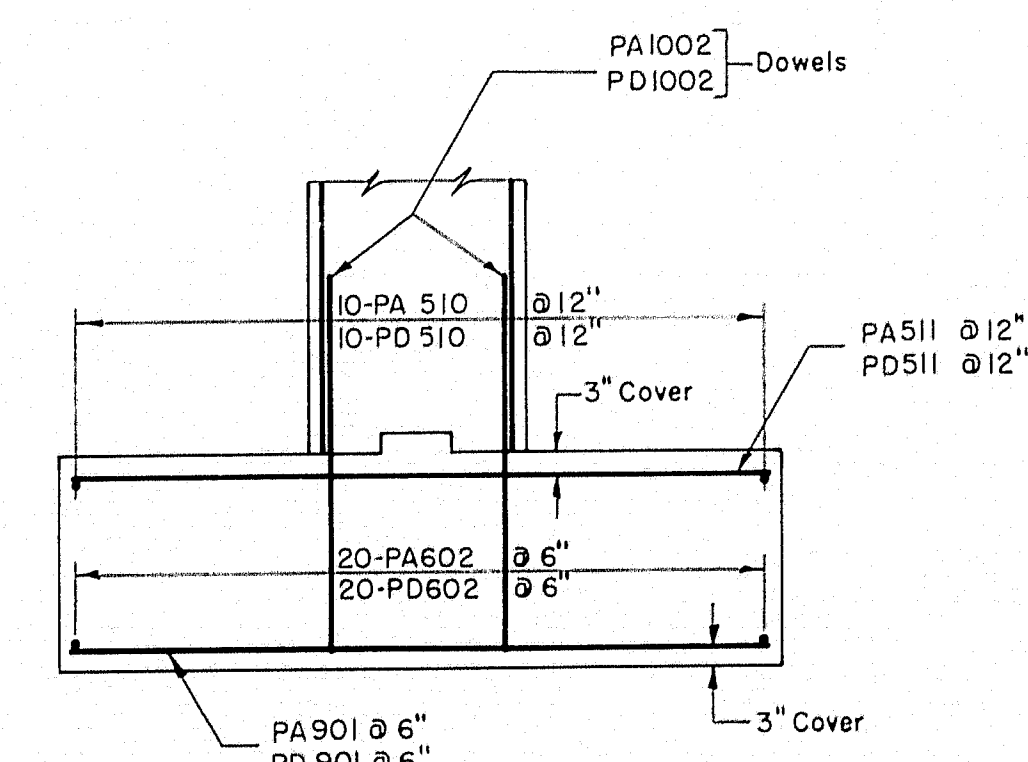
ELEVATION PIER E

Scale: 1/4" = 1'-0"



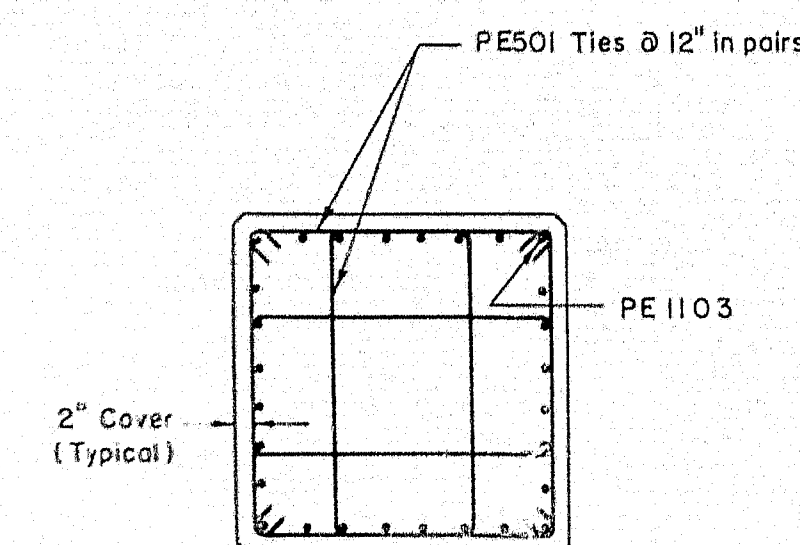
SECTION E-E

Scale: 3/8" = 1'-0"



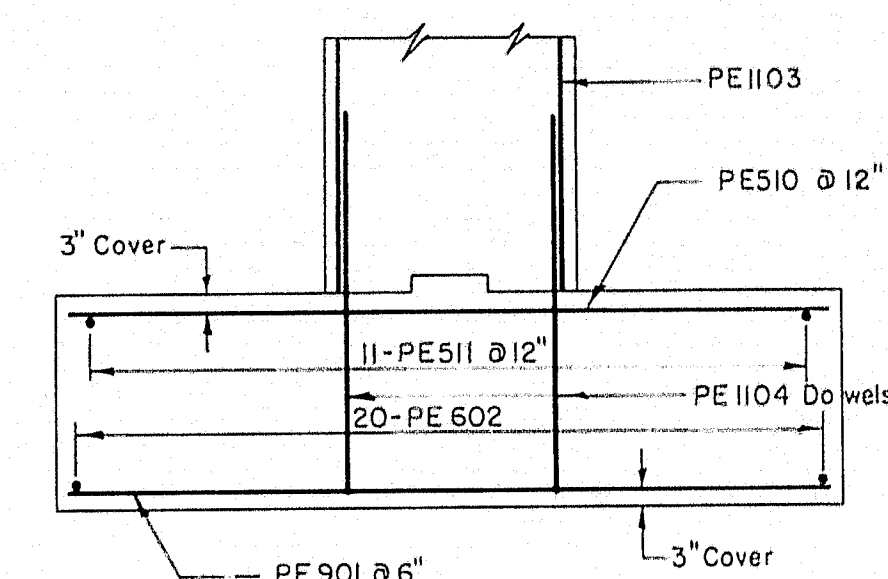
SECTION D-D

Scale: 3/8" = 1'-0"



SECTION G-G

Scale: 1/2" = 1'-0"



SECTION F-F

Scale: 3/8" = 1'-0"

Note: For Sections A-A & B-B See Sheet 3.
For reinforcement in caps See Sheet 3.
Bars prefixed PA, PB, PC, PD, PE, & PF are for Piers A, B, C, D, E & F respectively.

ESTIMATED QUANTITIES

PIER A	14,294	LBS.
PIER C	19,765	LBS.
PIER D	15,704	LBS.
PIER E	23,312	LBS.
PIER F	19,776	LBS.

THE CLARKESON ENGINEERING CO., INC.

DESIGN G.B.	CHECK H.L.	BRIDGE NO.
DRAWN E.K.	APPROVED W.A.H.-C.J.M.	SURVEY PLOT

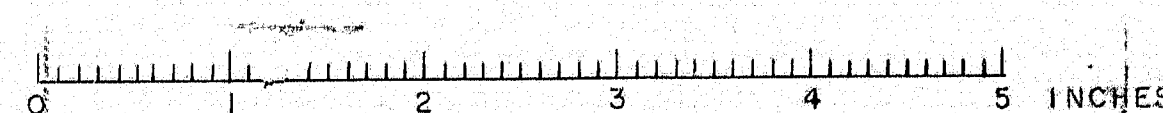
STATE HIGHWAY COMMISSION
INTERSTATE #95
OVER
MAINE CENTRAL R.R.
& COUNTY ROAD
IN THE CITY OF
WATERVILLE
KENNEBEC COUNTY

PIERS A,C,D,E&F REINFORCEMENT

SHEET 2 OF 6 SHEETS

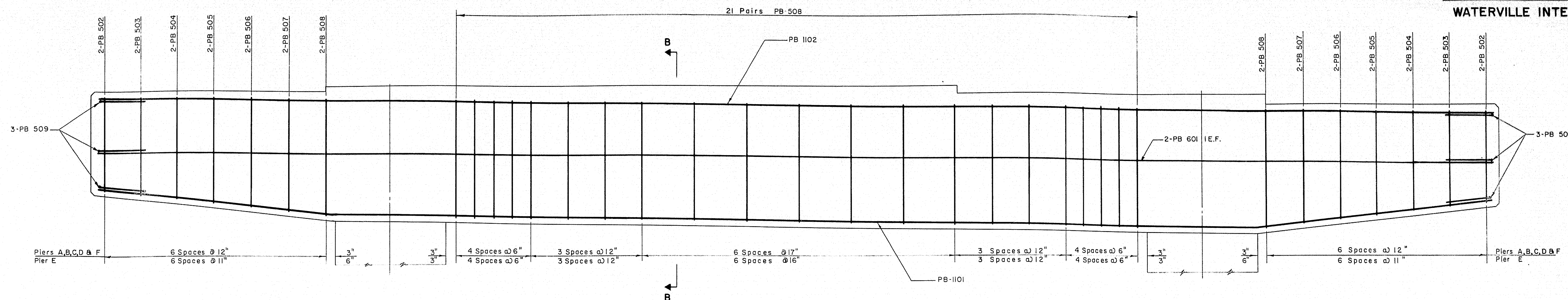
AUGUSTA, MAINE

75-121



B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-6(25)	21	24

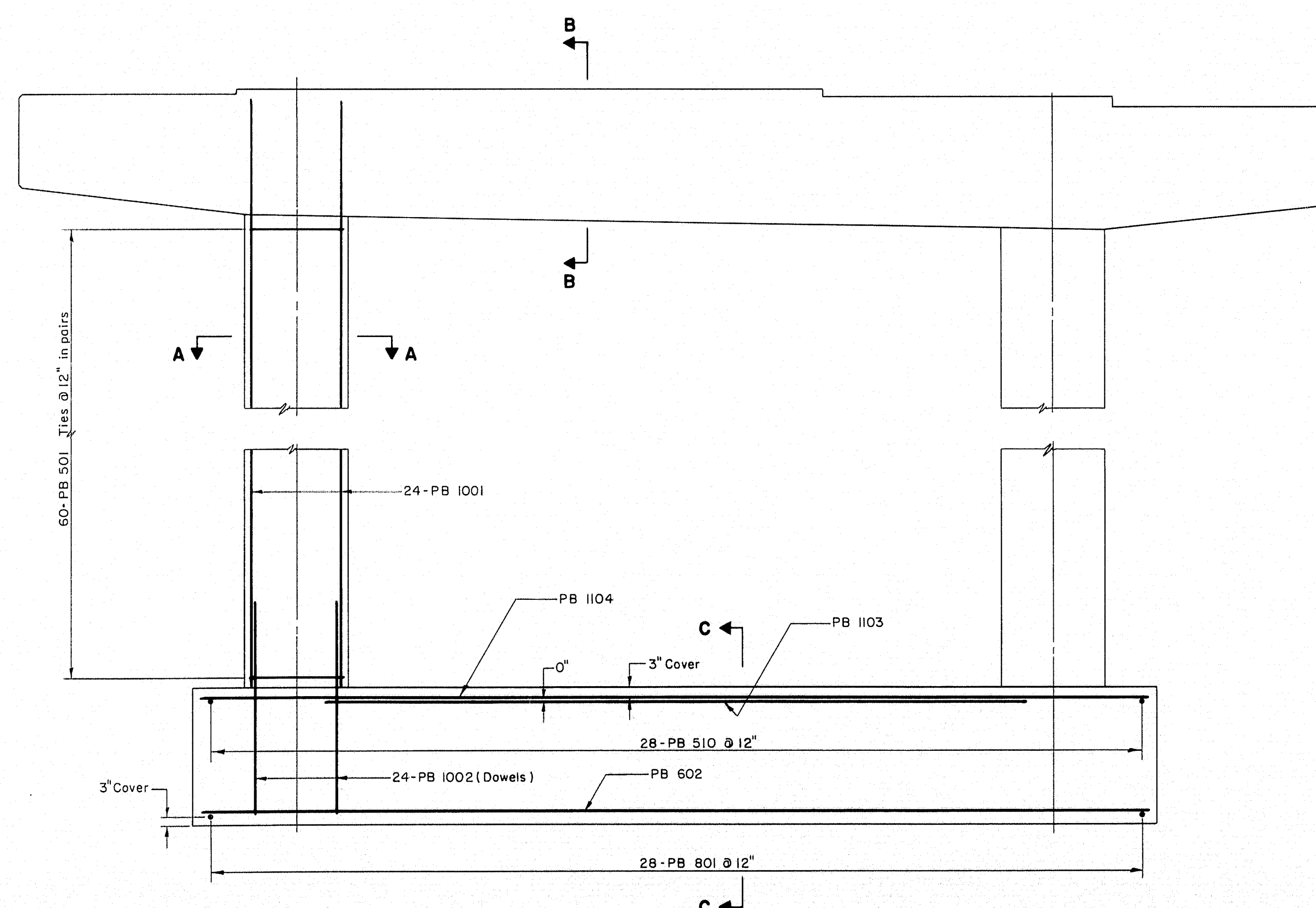
WATERVILLE INTERSTATE



PIER B CAP ELEVATION

Scale: $\frac{3}{4}$ " = 1'-0"

Note: Reinforcing in Pier Caps A, C, D, E & F similar except to be prefixed PA, PC, PD, PE, & PF respectively.

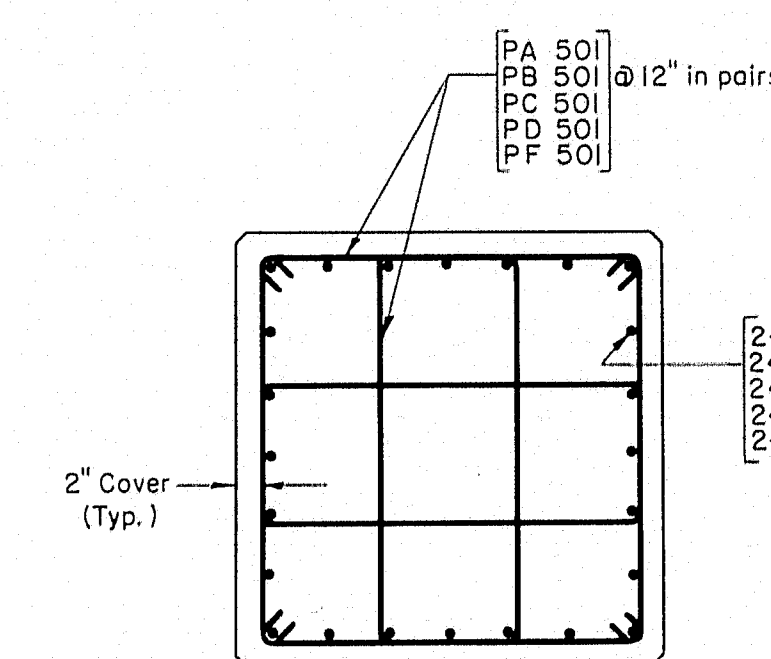


PIER B ELEVATION

Scale: $\frac{3}{8}$ " = 1'-0"

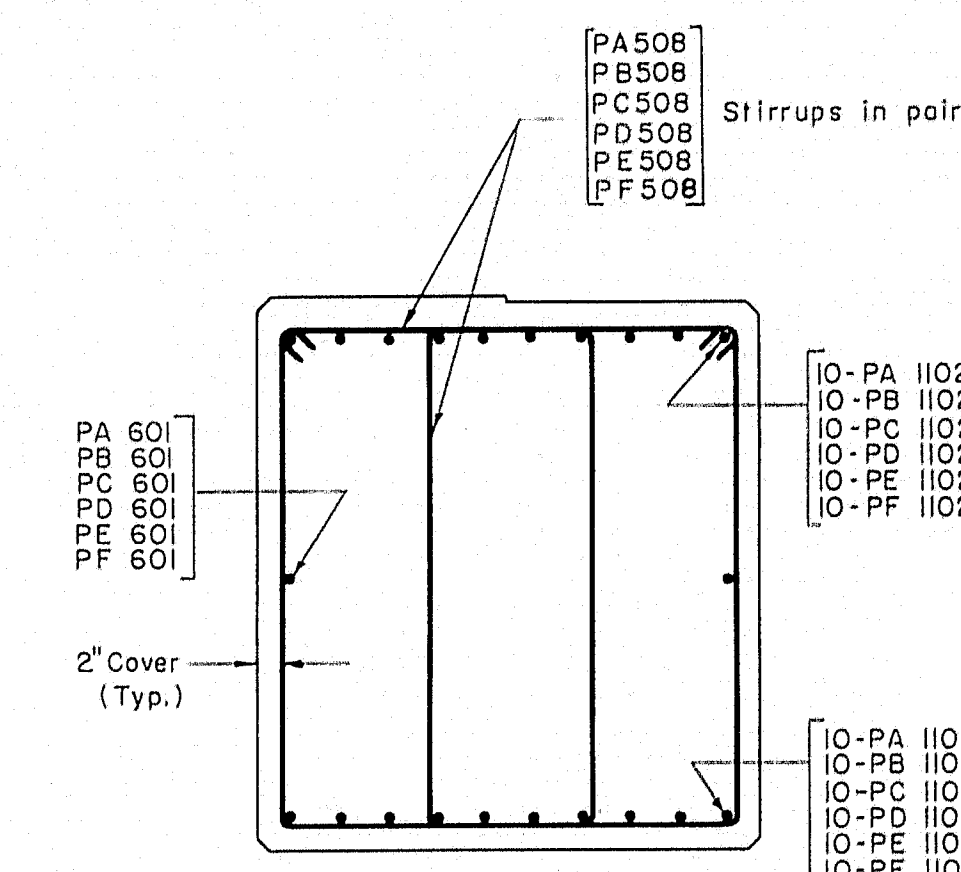
ESTIMATED QUANTITIES

PIER B 19,668 LBS.



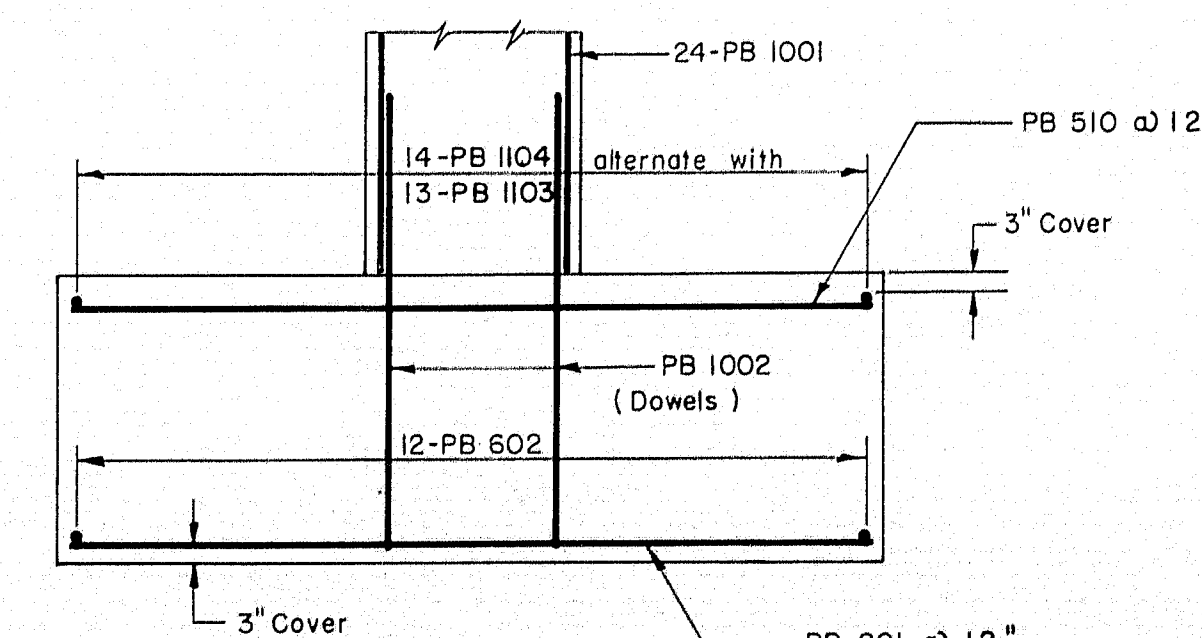
SECTION A-A

Scale: $\frac{3}{4}$ " = 1'-0"



SECTION B-B

Scale: $\frac{3}{4}$ " = 1'-0"



SECTION C-C

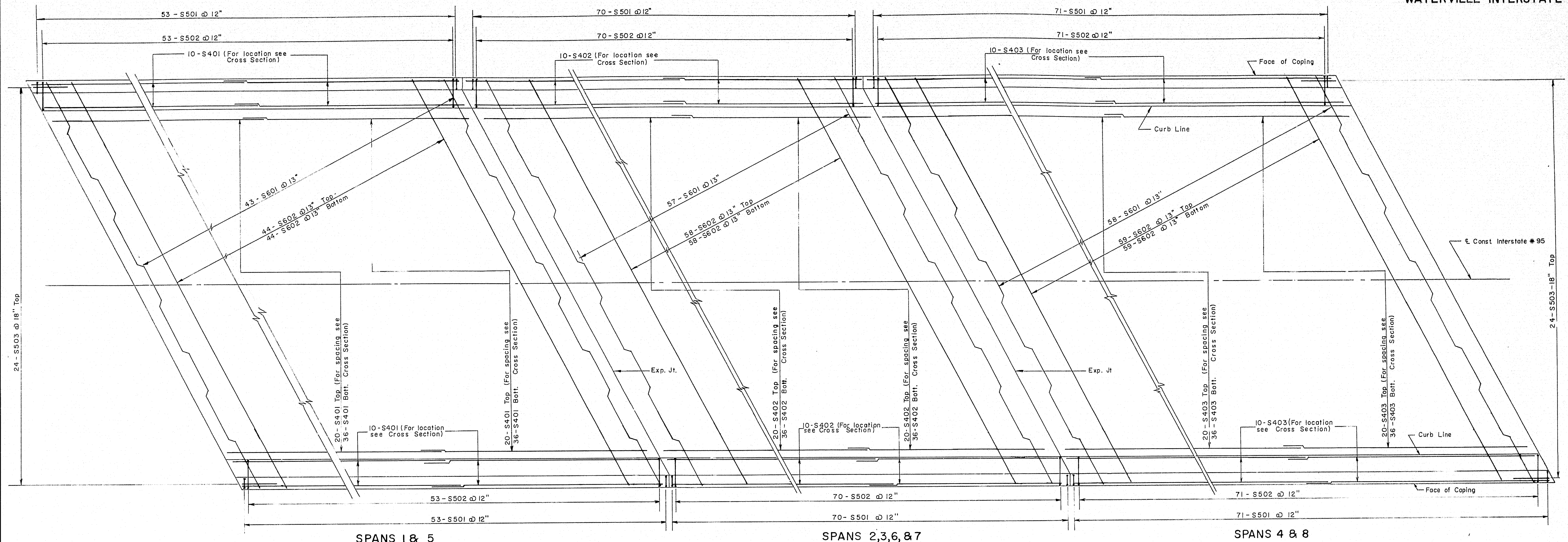
Scale: $\frac{3}{8}$ " = 1'-0"

THE CLARKSON ENGINEERING CO., INC.		
DESIGN G.B.	CHECK H.L.	BRIDGE NO. SURVEY
DRAWN E.K.	APPROVED W.A.H.C.M.	PLOT
STATE HIGHWAY COMMISSION		
INTERSTATE #95		
OVER		
MAINE CENTRAL R.R.		
8 COUNTY ROAD		
IN THE CITY OF		
WATERVILLE		
KENNEBEC COUNTY		
PIER B REINFORCEMENT		
SHEET 3 OF 6 SHEETS		AUGUSTA, MAINE

75-122

B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-6 (23)	22	24

WATERVILLE INTERSTATE



SPANS 1 & 5

SPANS 2, 3, 6, & 7

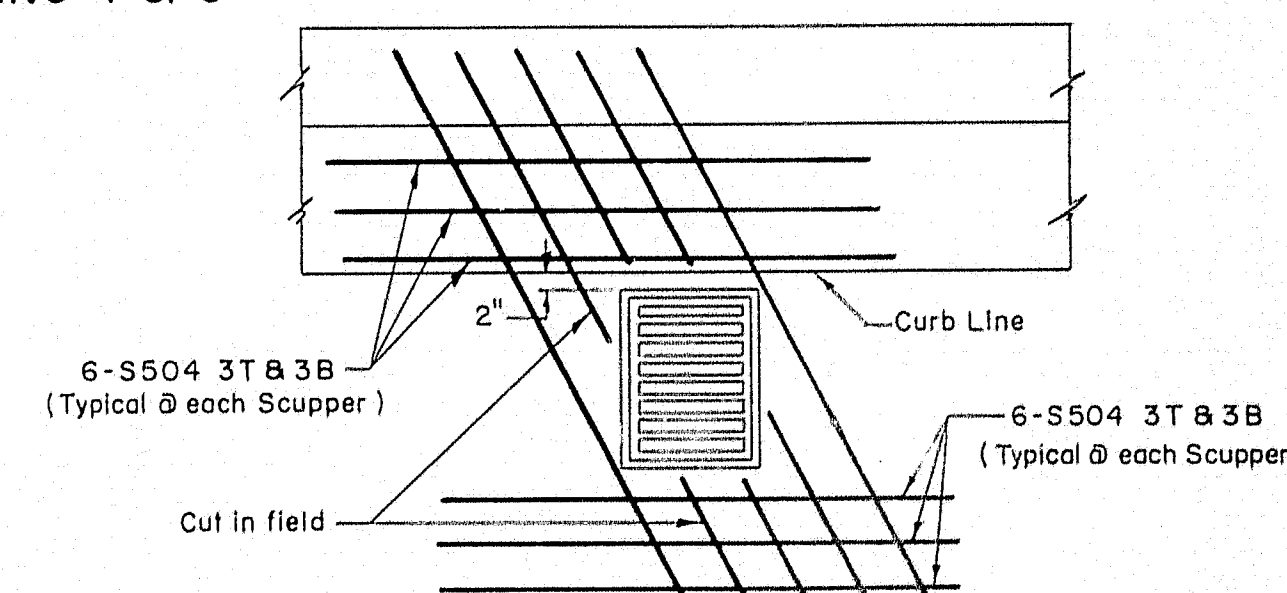
SPANS 4 & 8

DECK SLAB REINFORCEMENT

Scale $\frac{1}{4}'' = 1'-0''$

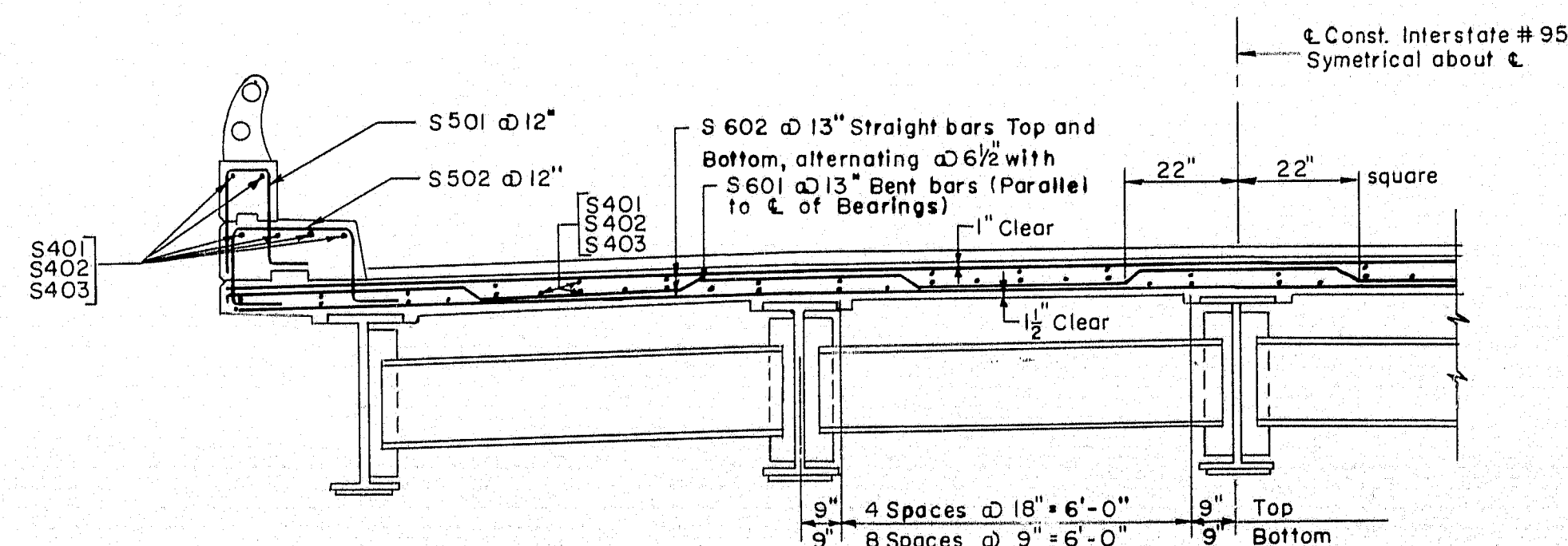
ESTIMATED QUANTITIES

DECK SLABS 117,701 LBS.
APPROACH SLABS 17,151 LBS.



SCUPPER REINFORCEMENT

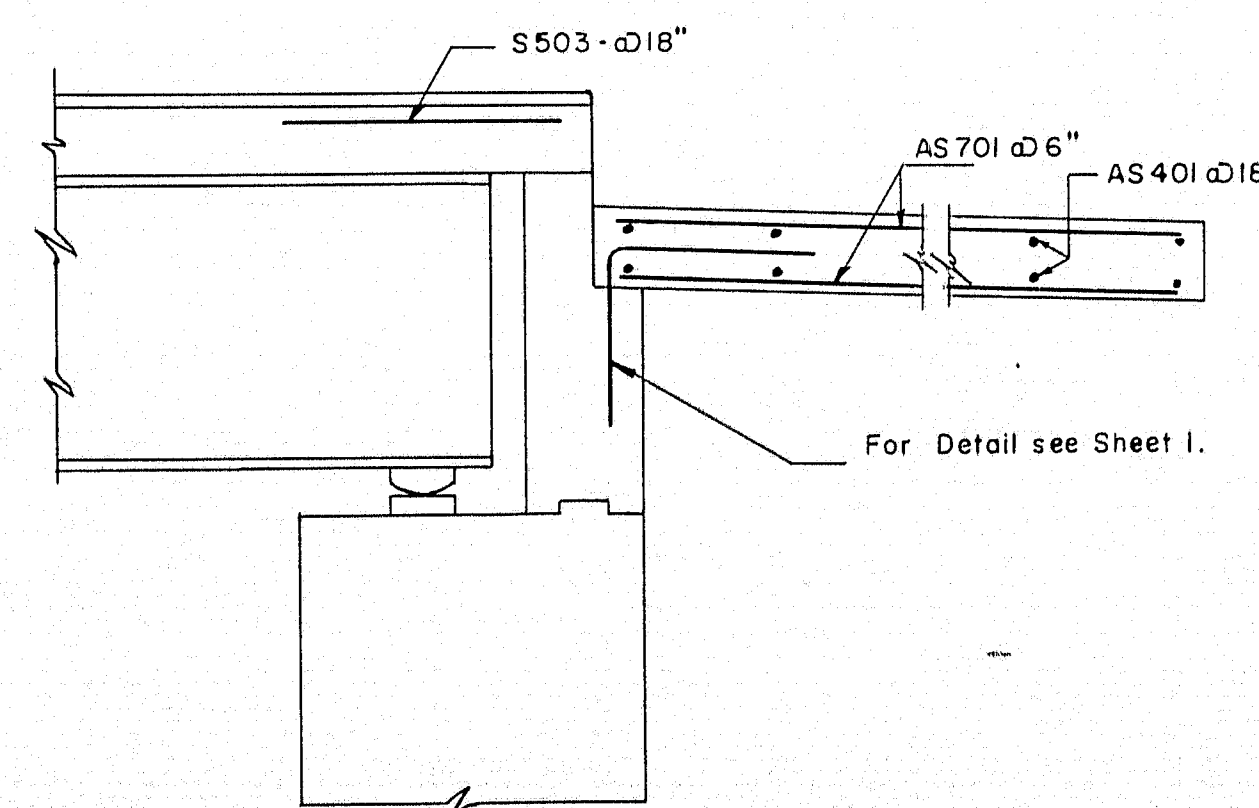
Scale: $\frac{1}{2}'' = 1'-0''$



TYPICAL CROSS SECTION

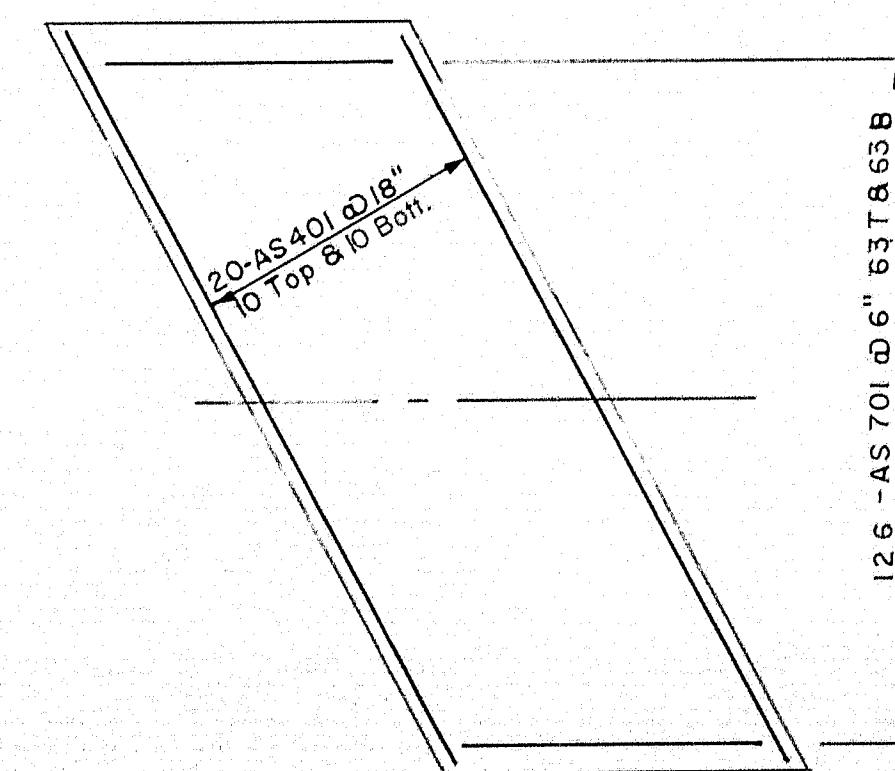
Scale $\frac{3}{8}'' = 1'-0''$

Note:
All longitudinal Reinforcing to be #4 Bars spaced as shown
(Parallel to ϵ Construction)



TYPICAL CROSS SECTION (APPROACH SLAB)

Scale $\frac{1}{2}'' = 1'-0''$



APPROACH SLAB REINFORCEMENT

Scale $\frac{1}{8}'' = 1'-0''$

THE CLARKSON ENGINEERING CO., INC.

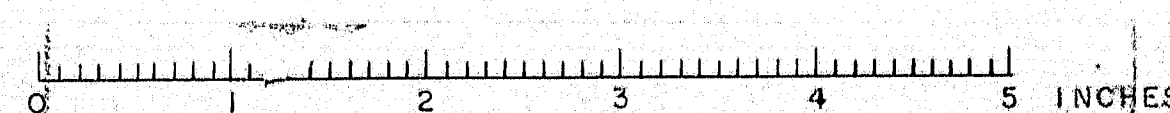
DESIGN G.B.-H.L. CHECK G.B.-H.L. BRIDGE NO. SURVEY
DRAWN F.E.T. APPROVED W.A.H.-C.J.M. PLOT

STATE HIGHWAY COMMISSION

INTERSTATE #95
OVER
MAINE CENTRAL R.R.
& COUNTY ROAD
IN THE CITY OF
WATERVILLE
KENNEBEC COUNTY
DECK REINFORCEMENT

SHEET 4 OF 6 SHEETS AUGUSTA, MAINE

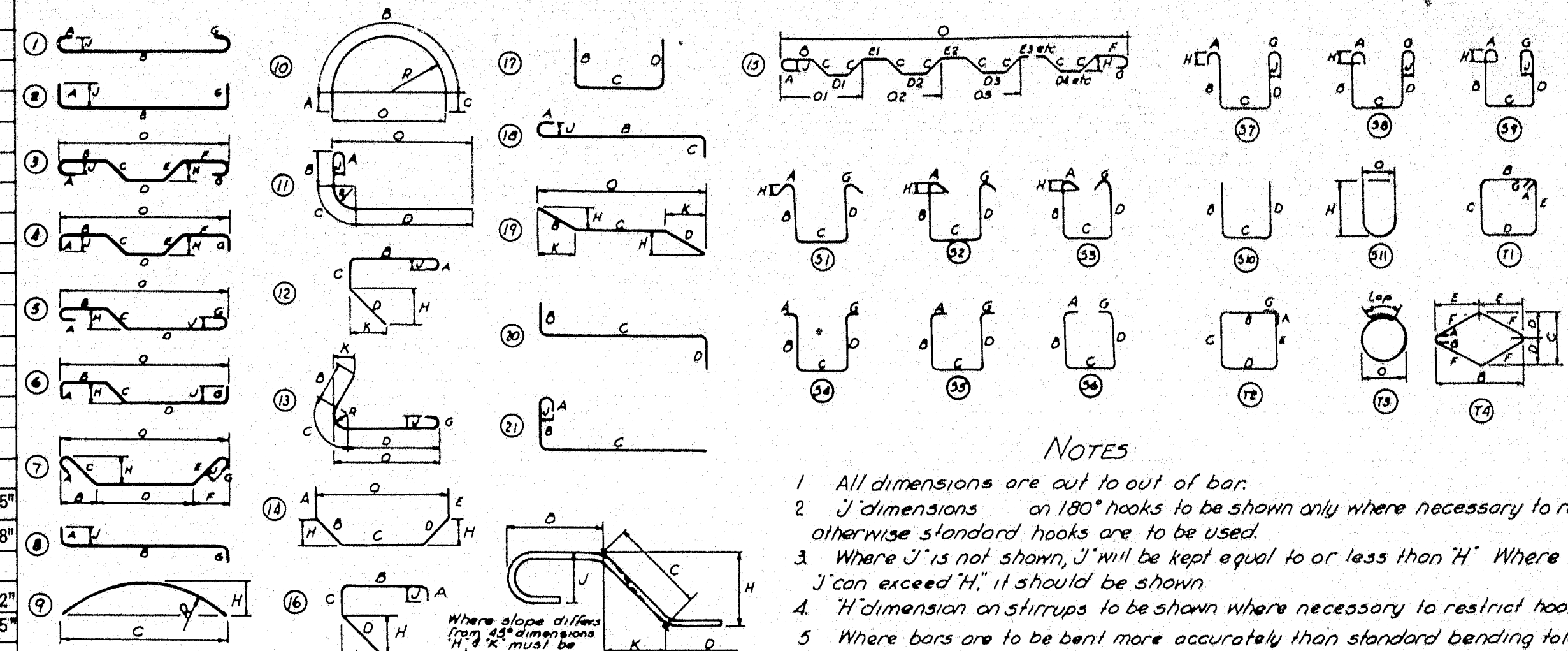
75-123

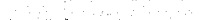


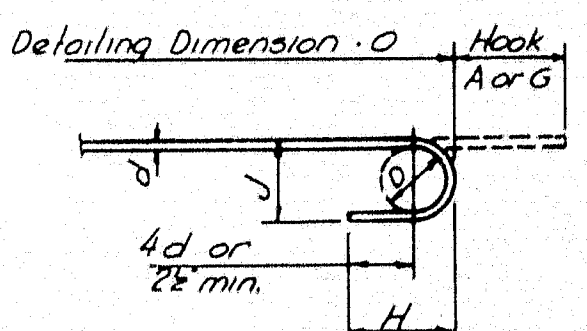
WATERVILLE INTERSTATE

WATERVILLE INTERSTATE

TYPICAL BAR BENDS



1. All dimensions are out to out of bar.
2. 'J' dimensions on 180° hooks to be shown only where necessary to restrict hook size, otherwise standard hooks are to be used.
3. Where 'J' is not shown, 'J' will be kept equal to or less than 'H'. Where  'J' can exceed 'H', it should be shown.
4. 'H' dimension on stirrups to be shown where necessary to restrict hooks.
5. Where bars are to be bent more accurately than standard bending tolerances, bending dimensions which require closer working should have limits indicated.
6. Figures in circles show types.
7. No allowance for bend curvature is to be made except for standard hook & radii in excess of some.



STANDARD HOOK DETAIL

BAR SIZES

<i>Equival Size</i>	<i>Present (Numbers)</i>
$\frac{1}{8}''$ _____	#2
$\frac{3}{8}''$ _____	#3
$\frac{1}{2}''$ _____	#4
$\frac{5}{8}''$ _____	#5
$\frac{3}{4}''$ _____	#6
$\frac{7}{8}''$ _____	#7
$1''$ _____	#8
$1\frac{1}{8}''$ _____	#9
$1\frac{3}{8}''$ _____	#10
$1\frac{1}{2}''$ _____	#11

8. Bar reinforcement metal shall conform to the requirements of the Standard Specifications for new Billet Steel Concrete Reinforcement Bars (Intermediate Grade) Serial Designation A.A.S.H.O. M31-48 or its latest revision.
All bars shall be deformed to conform with A.S.T.M. Specifications A305-49.

THE CLARKESON ENGINEERING CO. INC.		
DESIGN H.L.	CHECK G.B.	BRIDGE NO. SURVEY
DRAWN J.S.	APPROVED WAH.-C.J.M.	PLOT

STATE HIGHWAY COMMISSION

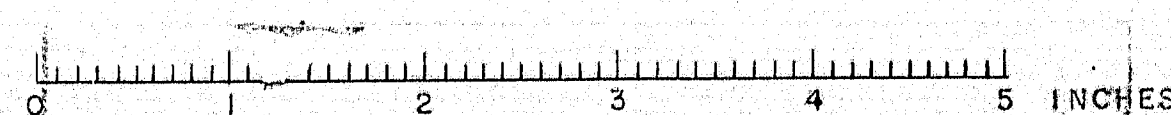
INTERSTATE # 95
OVER

MAINE CENTRAL R.R.
& COUNTY ROAD

IN THE CITY OF
WATERVILLE

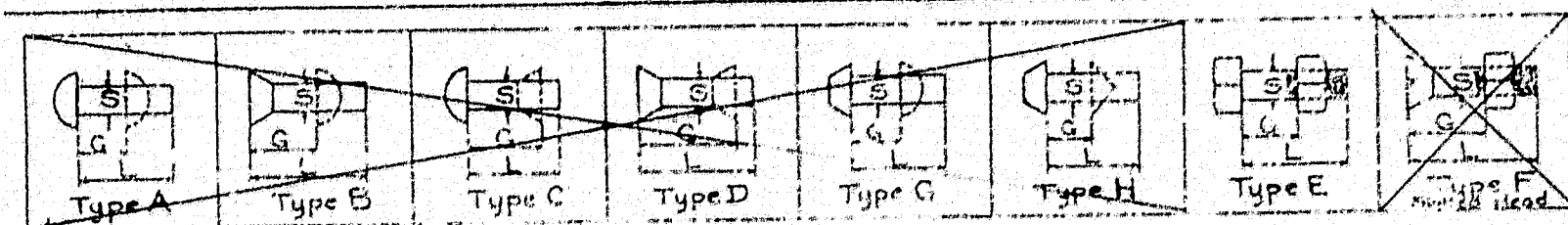
KENNEBEC COUNTY
REINFORCING SCHEDULE

SHEET 5. OF 6 SHEETS AUGUSTA, MAINE



Item	No. Pieces	Size	Length	Mark	Type	A	B	C	D	E	F	G	H	J	K	R	O
PIER A																	
1	100	5	9'-10"	PA 501	TI	0'-5"	1'-10"	2'-8"	1'-10"	2'-8"		0'-5"					
2	4	1	10'-2"	PA 502	TI		2'-3"	2'-5"	2'-3"	2'-5"							
3	1	1	10'-4"	PA 503	TI			2'-6"		2'-6"							
4	1	1	10'-8"	PA 504	TI			2'-8"		2'-8"							
5	1	1	10'-10"	PA 505	TI			2'-9"		2'-9"							
6	1	1	11'-2"	PA 506	TI			2'-11"		2'-11"							
7	4	1	11'-4"	PA 507	TI			3'-0"		3'-0"							
8	46	1	11'-8"	PA 508	TI	0'-5"	2'-3"	3'-2"	2'-3"	3'-2"		0'-5"					
9	6	1	5'-6 3/4"	PA 509	SIO		1'-3"	3'-0 3/4"	1'-3"								
10	20	1	6'-6"	PA 510	Str.												
11	14	5	9'-6"	PA 511	TI												
12	2	6	37'-11"	PA 601	TI												
13	40	6	6'-6"	PA 602	TI												
14	28	9	9'-6"	PA 901	TI												
15	48	10	27'-0"	PA 1001	TI												
16	48	10	6'-6"	PA 1002	Str.												
17	10	11	37'-11"	PA 1101	TI		6'-5 1/2"	25'-0"	6'-5 1/2"			0'-9"					37'-10"
18	10	11	37'-11"	PA 1102	Str.												
PIER B																	
24	120	5	9'-10"	PB 501	TI	0'-5"	1'-10"	2'-8"	1'-10"	2'-8"		0'-5"					
25	4	1	10'-2"	PB 502	TI		2'-3"	2'-5"	2'-3"	2'-5"							
26	1	1	10'-4"	PB 503	TI			2'-6"		2'-6"							
27	1	1	10'-8"	PB 504	TI			2'-8"		2'-8"							
28	1	1	10'-10"	PB 505	TI			2'-9"		2'-9"							
29	1	1	11'-2"	PB 506	TI			2'-11"		2'-11"							
30	4	1	11'-4"	PB 507	TI			3'-0"		3'-0"							
31	46	1	11'-8"	PB 508	TI	0'-5"	2'-3"	3'-2"	2'-3"	3'-2"		0'-5"					
32	6	1	5'-6 3/4"	PB 509	SIO		1'-3"	3'-0 3/4"	1'-3"								
33	28	5	11'-0"	PB 510	Str.												
34	2	6	37'-11"	PB 601	TI												
35	12	6	27'-6"	PB 602	TI												
36	28	8	11'-0"	PB 801	TI												
37	48	10	33'-11"	PB 1001	TI												
38	48	10	6'-6"	PB 1002	Str.												
39	10	11	37'-11"	PB 1101	TI		6'-5 1/2"	25'-0"	6'-5 1/2"			0'-9"					37'-10"
40	10	11	37'-11"	PB 1102	Str.												
41	13	11	20'-6"	PB 1103	Str.												
42	14	11	27'-6"	PB 1104	Str.												
PIER C																	
48	112	5	9'-10"	PC 501	TI	0'-5"	1'-10"	2'-8"	1'-10"	2'-8"		0'-5"					
49	4	1	10'-2"	PC 502	TI		2'-3"	2'-5"	2'-3"	2'-5"							
50	1	1	10'-4"	PC 503	TI			2'-6"		2'-6"							
51	1	1	10'-8"	PC 504	TI			2'-8"		2'-8"							
52	1	1	10'-10"	PC 505	TI			2'-9"		2'-9"							
53	1	1	11'-2"	PC 506	TI			2'-11"		2'-11"							
54	4	1	11'-4"	PC 507	TI			3'-0"		3'-0"							
55	46	1	11'-8"	PC 508	TI	0'-5"	2'-3"	3'-2"	2'-3"	3'-2"		0'-5"					
56	6	1	5'-6 3/4"	PC 509	SIO		1'-3"	3'-0 3/4"	1'-3"								
57	28	5	11'-0"	PC 510	Str.												
58	2	6	37'-11"	PC 601	TI												
59	28	9	11'-0"	PC 901	TI												
60	14	9	27'-6"	PC 902	TI												
61	48	10	30'-8"	PC 1001	TI												
62	48	10	6'-6"	PC 1002	Str.												
63	10	11	37'-11"	PC 1101	TI		6'-5 1/2"	25'-0"	6'-5 1/2"			0'-9"					37'-10"
64	10	11	37'-11"	PC 1102	Str.												
65	24	11	27'-6"	PC 1103	Str.												
PIER D																	
71	120	5	9'-10"	PD 501	TI	0'-5"	1'-10"	2'-8"	1'-10"	2'-8"		0'-5"					
72	4	1	10'-2"	PD 502	TI		2'-3"	2'-5"	2'-3"	2'-5"							
73	1	1	10'-4"	PD 503	TI			2'-6"		2'-6"							
74	1	1	10'-8"	PD 504	TI			2'-8"		2'-8"							
75	1	1	10'-10"	PD 505	TI			2'-9"		2'-9"							
76	1	1	11'-2"	PD 506	TI			2'-11"		2'-11"							
77	4	1	11'-4"	PD 507	TI			3'-0"		3'-0"							
78	46	1	11'-8"	PD 508	TI	0'-5"	2'-3"	3'-2"	2'-3"	3'-2"		0'-5"					
79	6	1	5'-6 3/4"	PD 509	SIO		1'-3"	3'-0 3/4"	1'-3"								
80	20	1	6'-6"	PD 510	Str.												
81	14	5	9'-6"	PD 511	TI												
PIER E																	
94	160	5	11'-10"	PE 501	TI	0'-5"	2'-4"	3'-2"	2'-4"	3'-2"		0'-5"					
95	4	1	10'-2"	PE 502	TI		2'-3"	2'-5"	2'-3"	2'-5"							
96	1	1	10'-4"	PE 503	TI			2'-6"		2'-6"							
97	1	1	10'-8"	PE 504	TI			2'-8"		2'-8"							
98	1	1	10'-10"	PE 505	TI			2'-9"		2'-9"							
99	1	1	11'-2"	PE 506	TI			2'-11"		2'-11"							
100	4	1	11'-4"	PE 507	TI			3'-0"		3'-0"							
101	46	1	11'-8"	PE 508	TI	0'-5"	2'-3"	3'-2"	2'-3"	3'-2"		0'-5"					
102	6	1	5'-6 3/4"	PE 509	SIO		1'-3"	3'-0 3/4"	1'-3"								
103	16	1	10'-4"	PE 510	Str.												
104	22	5	7'-6"	PE 511	TI												
105	2	6	37'-11"	PE 601	TI												
106	40	6	7'-6"	PE 602	TI												
107	32	9	10'-6"	PE 901	Str.												
108	10	11	37'-11"	PE 1101	TI		6'-5 1/2"	25'-0"	6'-5 1/2"			0'-9"					37'-10"
109	10	11	37'-11"	PE 1102	Str.												
110	56	11	43'-0"	PE 1103	Str.												
111	56	11	6'-3"	PE 1104	Str.												
112																	
113																	
114																	
115																	
116																	
117																	
118	108	5	9'-10"	PF 501	TI	0'-5"	1'-10"	2'-8"	1'-10"	2'-8"		0'-5"					
119	4	1	10'-2"	PF 502	TI		2'-3"	2'-5"	2'-3"	2'-5"							
120	1	1	10'-4"	PF 503	TI			2'-6"		2'-6"							
121	1	1	10'-8"	PF 504	TI			2'-8"		2'-8"							
122	1	1	10'-10"	PF 505	TI			2'-9"		2'-9"							
123	1	1	11'-2"	PF 506	TI			2'-11"		2'-11"							
124	4	1	11'-4"	PF 507	TI			3'-0"		3'-0"							
125	46	1	11'-8"	PF 508	TI	0'-5"	2'-3"	3'-2"	2'-3"	3'-2"		0'-5"					
126	6	1	5'-6 3/4"	PF 509	SIO		1'-3"	3'-0 3/4"	1'-3"								
127	28	5	11'-0"	PF 510	Str.												
128	2	6	37'-11"	PF 601	TI												
129	28	9	11'-0"	PF 901	TI												
130	14	9	27'-6"	PF 902	TI												
131	48	10	30'-11"	PF 1001	TI												
132	48	10	6'-6"	PF 1002	Str.												
133	10	11	37'-11"	PF 1101	TI		6'-5 1/2"	25'-0"	6'-5 1/2"			0'-9"					3

BETHLEHEM STEEL COMPANY
FABRICATED STEEL CONSTRUCTION
BETH DRAFTING ROOM BETH WORKS
SUMMARY OF FIELD RIVETS AND BOLTS



Line	No.	Description	Mark of Item No.	Weight	Remarks	Order No.
1	775	736 DIAPHR. T. STR. L.				
2		396-ESS-570				
3						
4						
5						
6						
7						
8						
9						
10						
11						
12						
13						
14						
15						
16						
17						
18						
19						
20						
21						
22						
23						
24						
25						
26						
27						
28						
29						
30						

SUMMARY OF RIVETS AND BOLTS: TOTAL WEIGHT THIS PAGE: 801.4829

ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE SPECIFICATIONS OF THE BETHLEHEM STEEL COMPANY, PITTSBURGH, PA. AND THE AISC SPECIFICATIONS FOR STRUCTURAL STEEL CONSTRUCTION, THIRD EDITION, 1941.

IN CHARGE OF (Hoover) Nathan Cross
MADE BY: JF
DATE: 12/4/58
CHECKED BY: R
DATE: 12-8-58
SHIP NO. 1

CONTRACT: CP4434
PAGE NO. 1
ITEM NO. 1

CUSTOMER: CALLAHAN BROS. INC.
STRUCTURE: INTERSTATE #95
LOCATION: WATERVILLE, MAINE

BETHLEHEM STEEL COMPANY
FABRICATED STEEL CONSTRUCTION
BETH DRAFTING ROOM BETH WORKS
BILL OF MISCELLANEOUS MATERIAL

Line	No.	Description	Mark of Item No.	Weight	Remarks	Order No.
1	240	3" x 24" HARDENED CUT STEEL WASHERS				
2						
3						
4						
5						
6						
7						
8						
9						
10						
11						
12						
13						
14						
15						
16						
17						
18						
19						
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21						
22						
23						
24						
25						
26						
27						
28						
29						
30						

Material: ASTM A325
Specifications: NONE
Machining: NONE
Paint: SHIP WITH A325 BOLTS ON SHEET # 58H1
Shipment: FIELD WASHERS
Remarks:

IN CHARGE OF (Hoover) Nathan Cross
MADE BY: JF
DATE: 12/4/58
CHECKED BY: R
DATE: 12-8-58
SHIP NO. 1

CONTRACT: CP4434
PAGE NO. 1
ITEM NO. 1

CUSTOMER: CALLAHAN BROS. INC.
STRUCTURE: INTERSTATE #95
LOCATION: WATERVILLE, MAINE

BETHLEHEM STEEL COMPANY
FABRICATED STEEL CONSTRUCTION
BETH DRAFTING ROOM BETH WORKS
BILL OF MISCELLANEOUS MATERIAL

Line	No.	Description	Mark of Item No.	Weight	Remarks	Order No.
1	165	STD WASHER 3" x 4" THK - 1/2" HOLE				
2		(USED UNDER NUTS OF ANCH. BOLTS)				
3						
4						
5						
6						
7						
8						
9						
10						
11						
12						
13						
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25						
26						
27						
28						

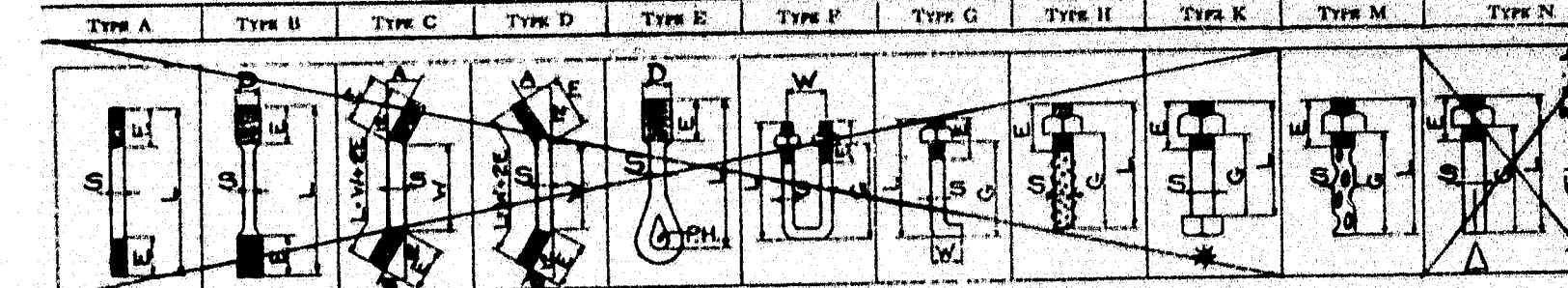
Material: ASTM A7
Specifications: MAINE STATE HIGHWAY COMMISSION
Machining: ONE SHOP COAT AS PER SPEC
Paint: SHIP WITH 1ST CARLOAD OF STEEL
Shipment: FIELD WASHERS
Remarks:

IN CHARGE OF (Hoover) Nathan Cross
MADE BY: JF
DATE: 12/4/58
CHECKED BY: R
DATE: 12-8-58
SHIP NO. 1

CONTRACT: CP4434
PAGE NO. 1
ITEM NO. 1

CUSTOMER: CALLAHAN BROS. INC.
STRUCTURE: INTERSTATE #95
LOCATION: WATERVILLE, MAINE

BETHLEHEM STEEL COMPANY
FABRICATED STEEL CONSTRUCTION
BETH DRAFTING ROOM BETH WORKS
BILL OF ROPS



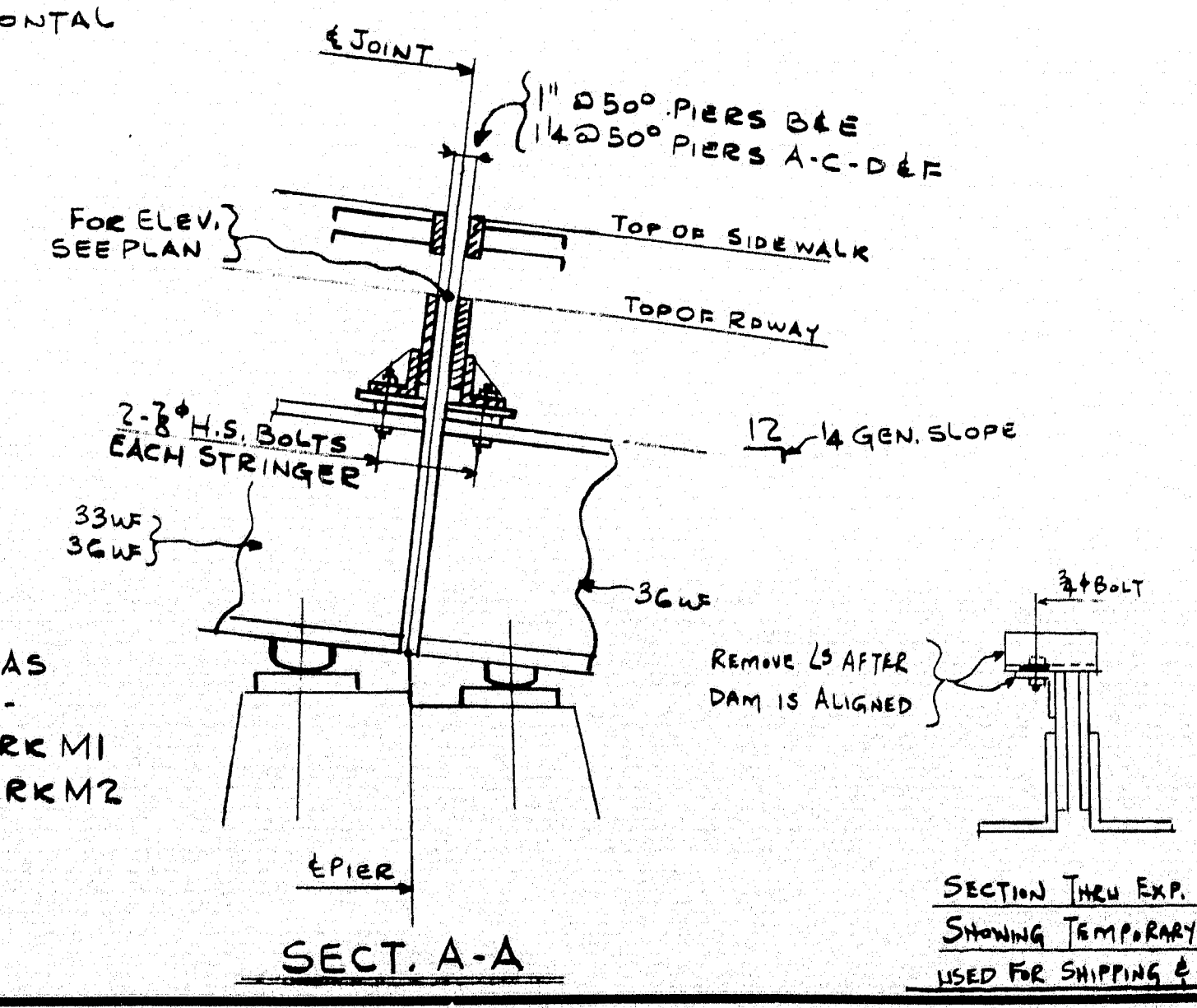
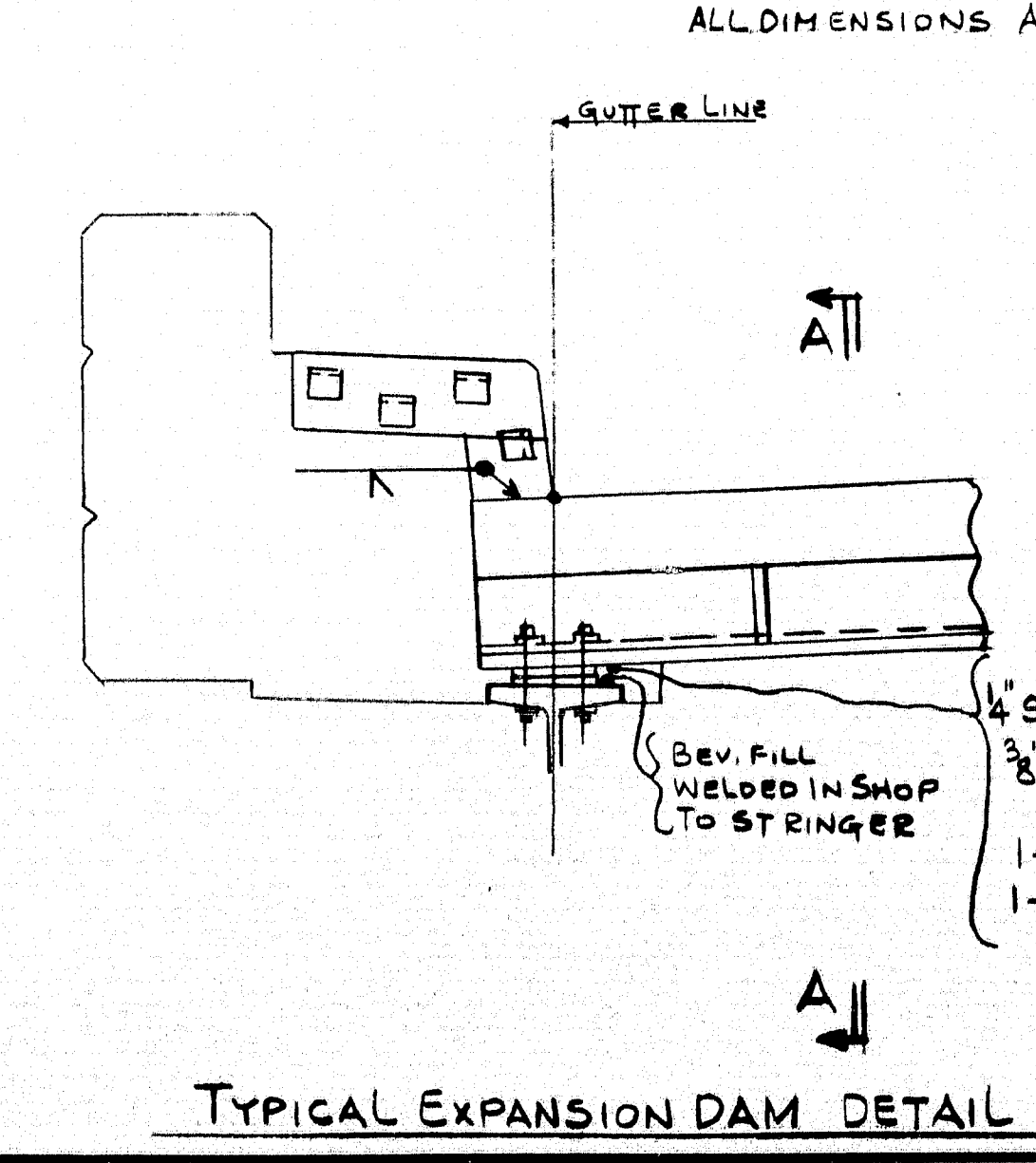
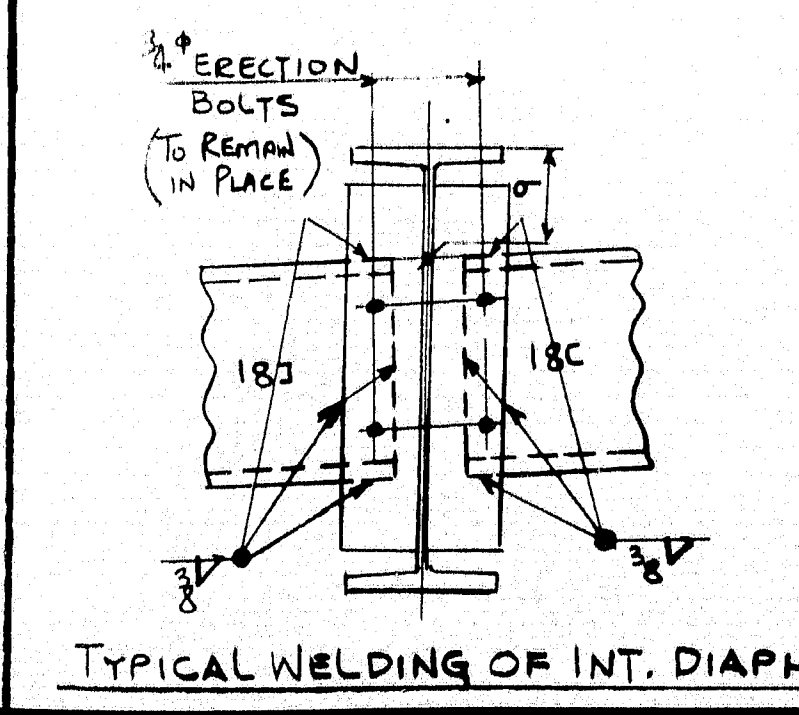
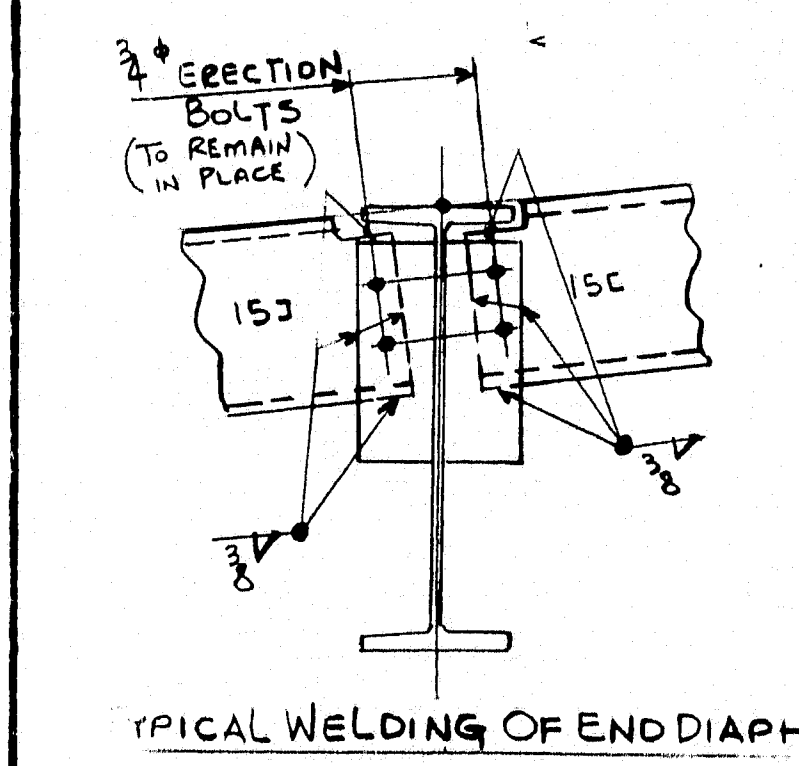
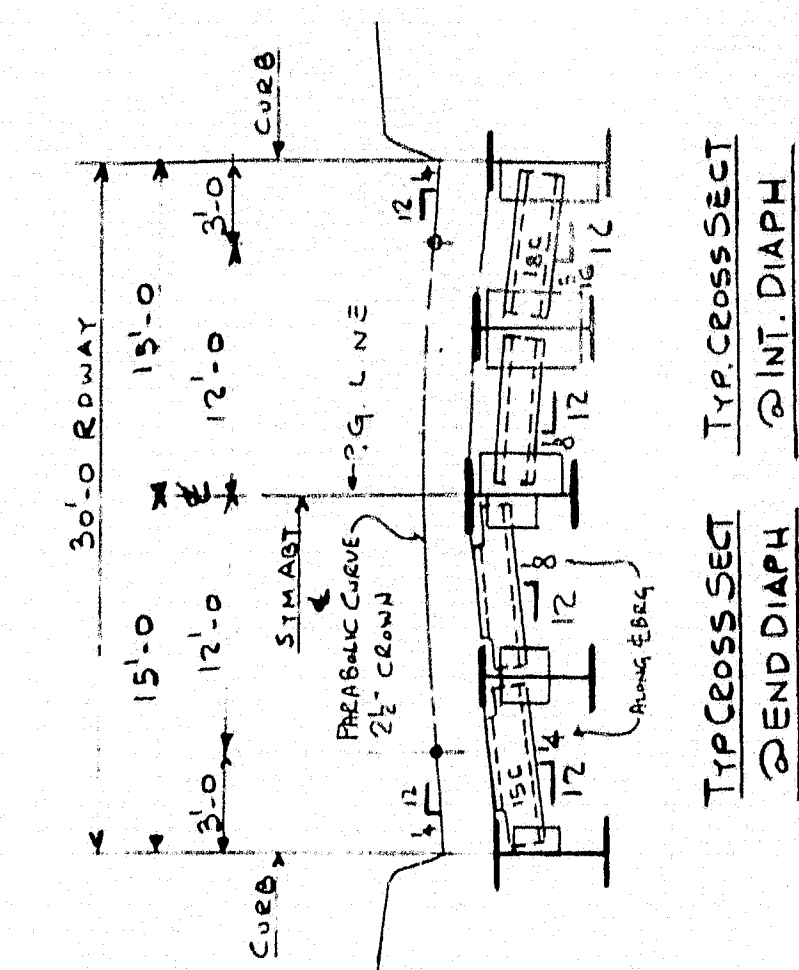
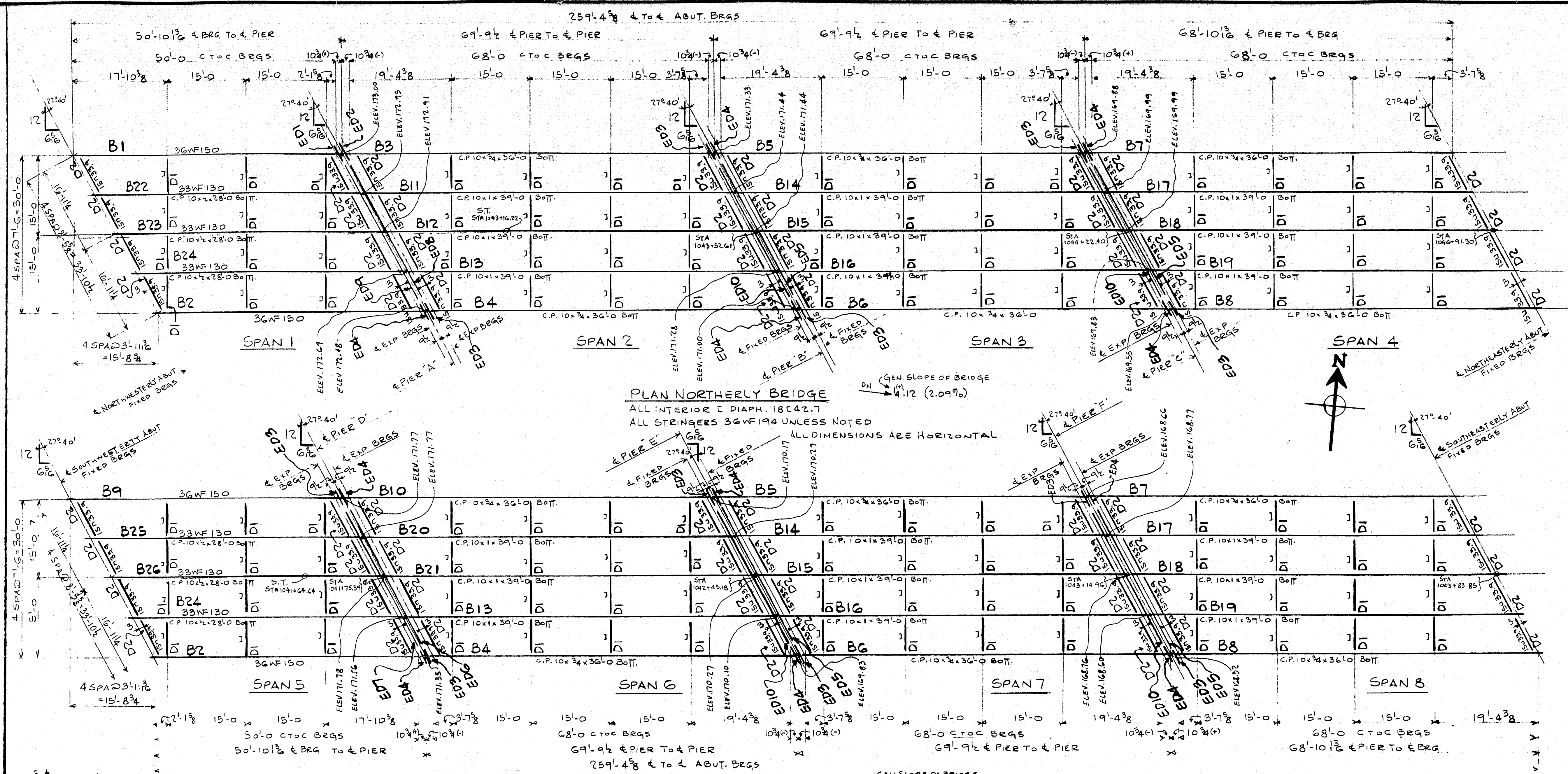
Line	No.	Description	Mark of Item No.	Weight	Remarks	Order No.
1	160	AB1 M 14 20 - 4 - 24				
2						
3						
4						
5						
6						
7						
8						
9						
10						
11						
12						
13						
14						
15						
16						
17						
18						
19						
20						
21						
22						
23						
24						
25						
26						
27						
28						

Material: 160 HEX
Specifications: AMER. STD. HEAVY
Machining: SPEC: ASTM A7
Paint: ADVANCE SHIPMENT
Shipment: NO PAINT OIL THDS
Remarks:

IN CHARGE OF (Hoover) Nathan Cross
MADE BY: JF
DATE: 12/4/58
CHECKED BY: R
DATE: 12-8-58
SHIP NO. 1

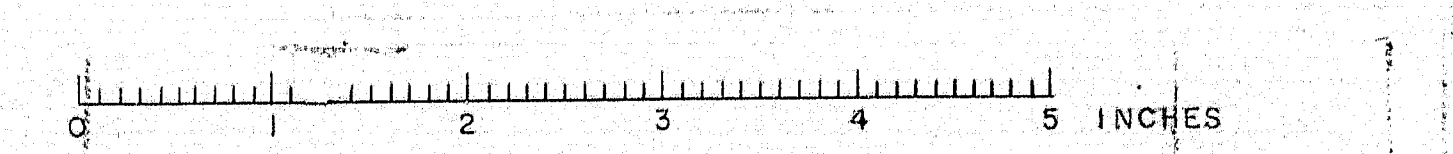
CONTRACT: CP4434
PAGE NO. 1
ITEM NO. 1

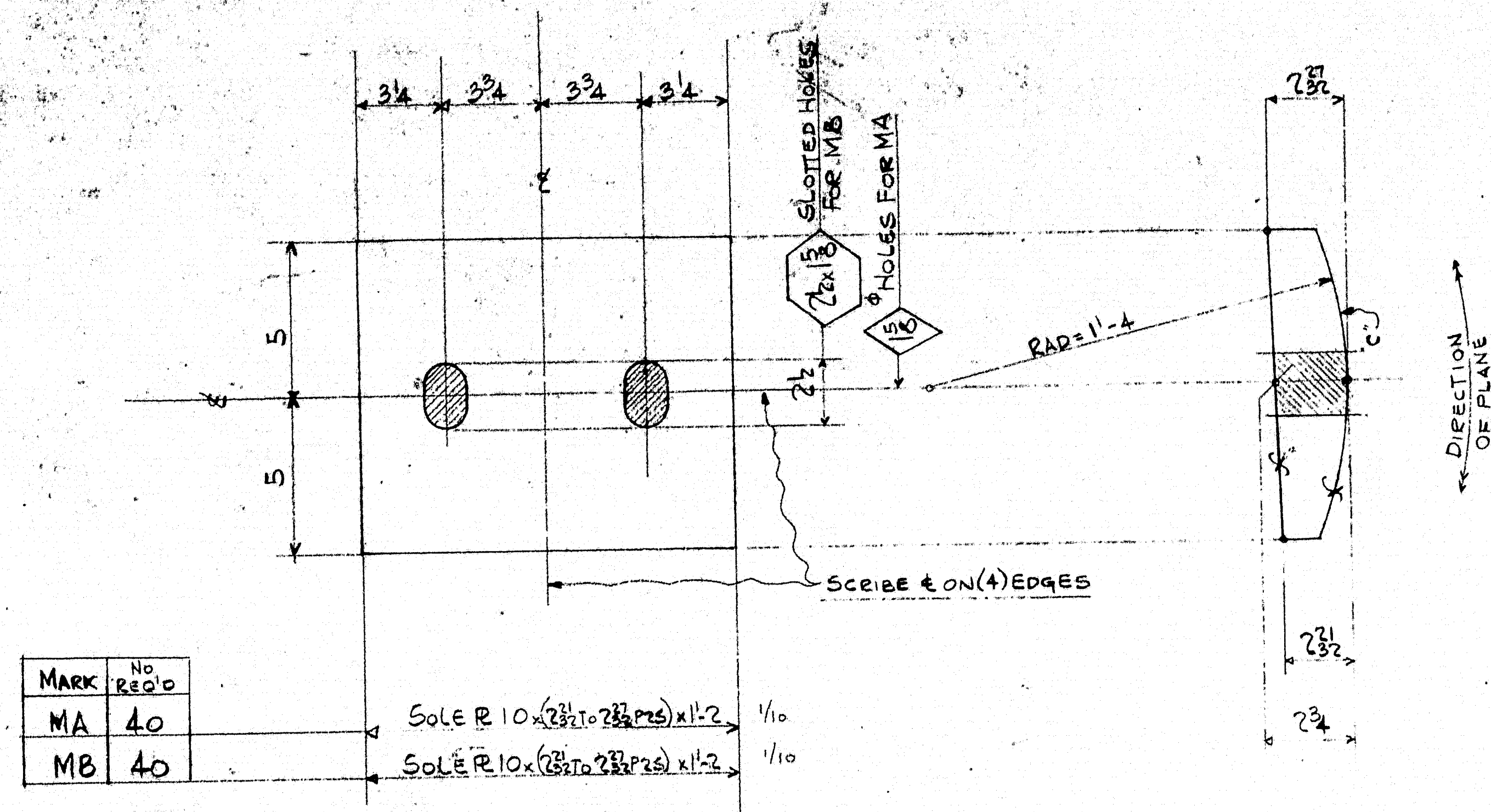
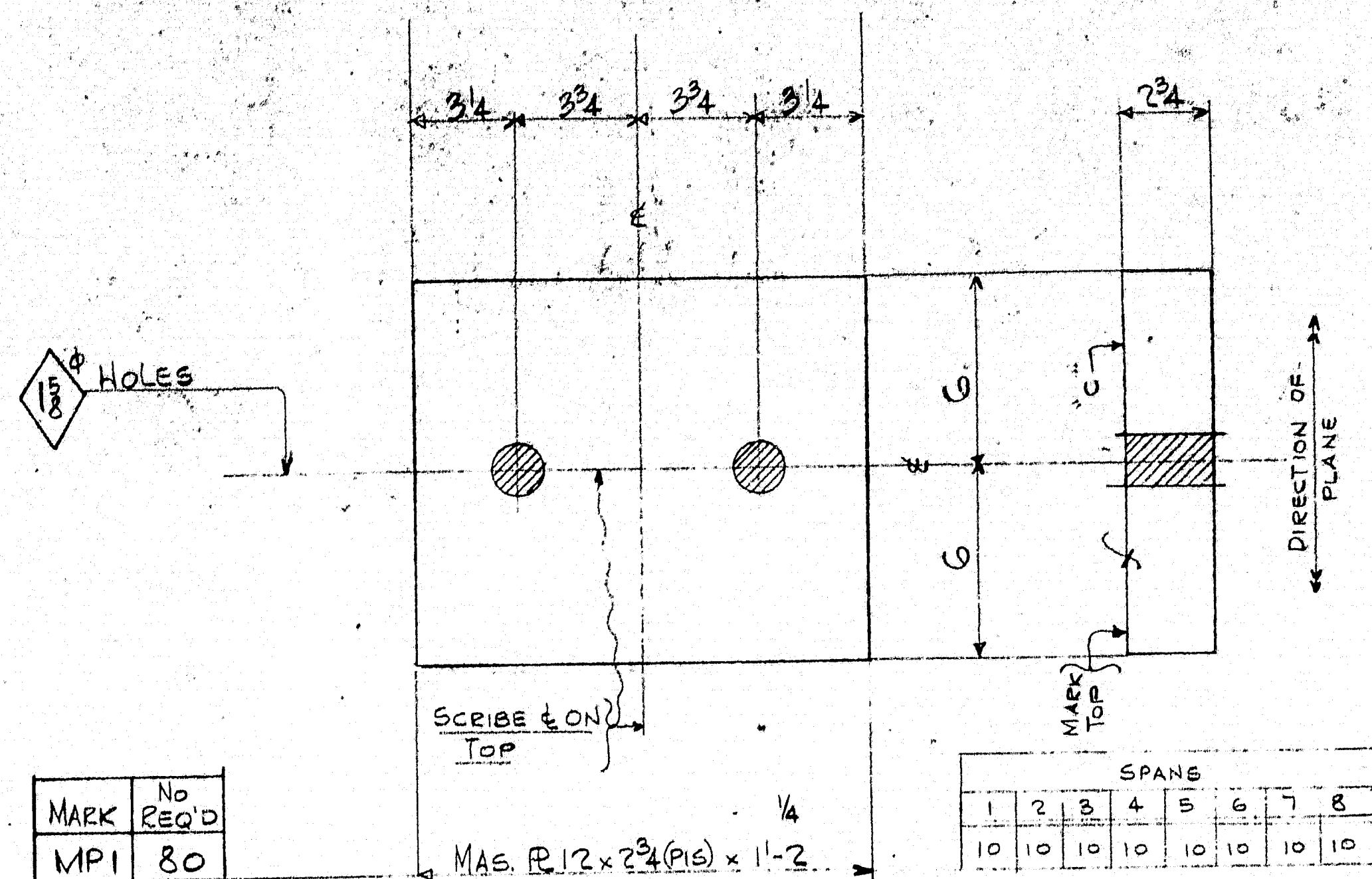
CUSTOMER: CALLAHAN BROS. INC.
STRUCTURE: INTERSTATE #95
LOCATION: WATERVILLE, MAINE



- GENERAL NOTES:**
1. ERECTION - BY OTHERS
 2. MAT'L SPECS: A ST M-A7 UNLESS NOTED
 3. WORKMANSHIP SPECS: STATE OF MAINE STATE HIGHWAY COMMISSION (JAN. 1956)
 4. ALL HOLES, CUTS, BLOCKS, ETC. FOR OTHER TRADES MUST BE SHOWN ON THIS PLAN AT TIME OF APPROVAL, OTHERWISE WORK MUST BE DONE IN THE FIELD AT NO EXPENSE TO B.S. CO.
 5. ALL WELDING SHALL CONFORM TO THE CURRENT SPECS. FOR WELDED HIGHWAY & RAILWAY BRIDGES OF THE AMERICAN WELDING SOCIETY.
 6. SHOP PAINT: ONE COAT AS PER SPECS
 7. FIELD PAINT: FURNISHED & APPLIED BY OTHERS
 8. FIELD CONNECTIONS TO BE WELDED OR A325 BOLTED AS SHOWN
 9. ALL ERECTION BOLTS TO REMAIN IN PLACE
 10. RAILING, RAILING ANCHOR BOLTS, FIELD WELD WIRE, DRIFT PINS, FITTING UP BOLTS, BEARING PREPARATION MATERIAL, ALL DRAINAGE MATERIAL, SPIRALS, STUDS, AND ERECTION SHIMS ARE BY OTHERS.

TEMP. LAYOUT		INTERSTATE #95	
SHOP INSP.		OVER MAINE CENTRAL RR & COUNTY ROAD PROJ 6(23)/23	
F. WIL.		WATERVILLE, MAINE	
B. SMITH		BETHLEHEM STEEL COMPANY	
MILLING		FABRICATED STEEL CONSTRUCTION	
PAINT		BETHLEHEM DRAFTING ROOM BETH. WORKS	
WELDING		IN CHARGE NATHAN SRODAS MADE BY J. 12/5/57	
SHOP CONTACT SURFACES		CHECKED BY E. 12-22-58 TRACED BY	
		REVISED LAST ASSEM. MK.	
SHOP NO.		CONTRACT CP4434	
		SHEET E2	





- SHOP NOTES:-
1. PLATES TO BE PRESS STRAIGHTENED WHERE NECESSARY TO INSURE FLATNESS.
 2. SURFACES MARKED "C" TO BE COATED WITH A HOT MIXTURE OF WHITE LEAD & TALLOW
 3. SOLE PLATES ARE NOT TO BE PAINTED UNTIL AFTER ASSEMBLY TO STRINGERS
 4. MAT'L SPECS ASTM A7
 5. WORKMANSHIP SPECS. MAINE STATE HIGHWAY COMMISSION

NOTES NONE

OPEN HOLES AS NOTED

BEAM NO

ROLLING AS NOTED

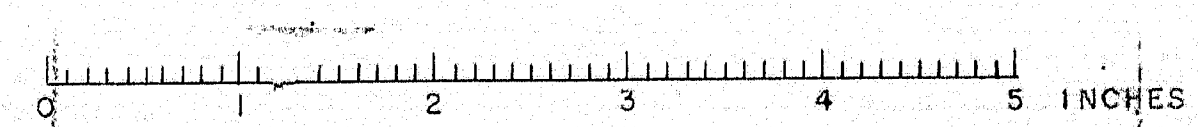
PAINT (ONE COAT AS PER SPECS EXCEPT "C" SURFACES AS NOTED)

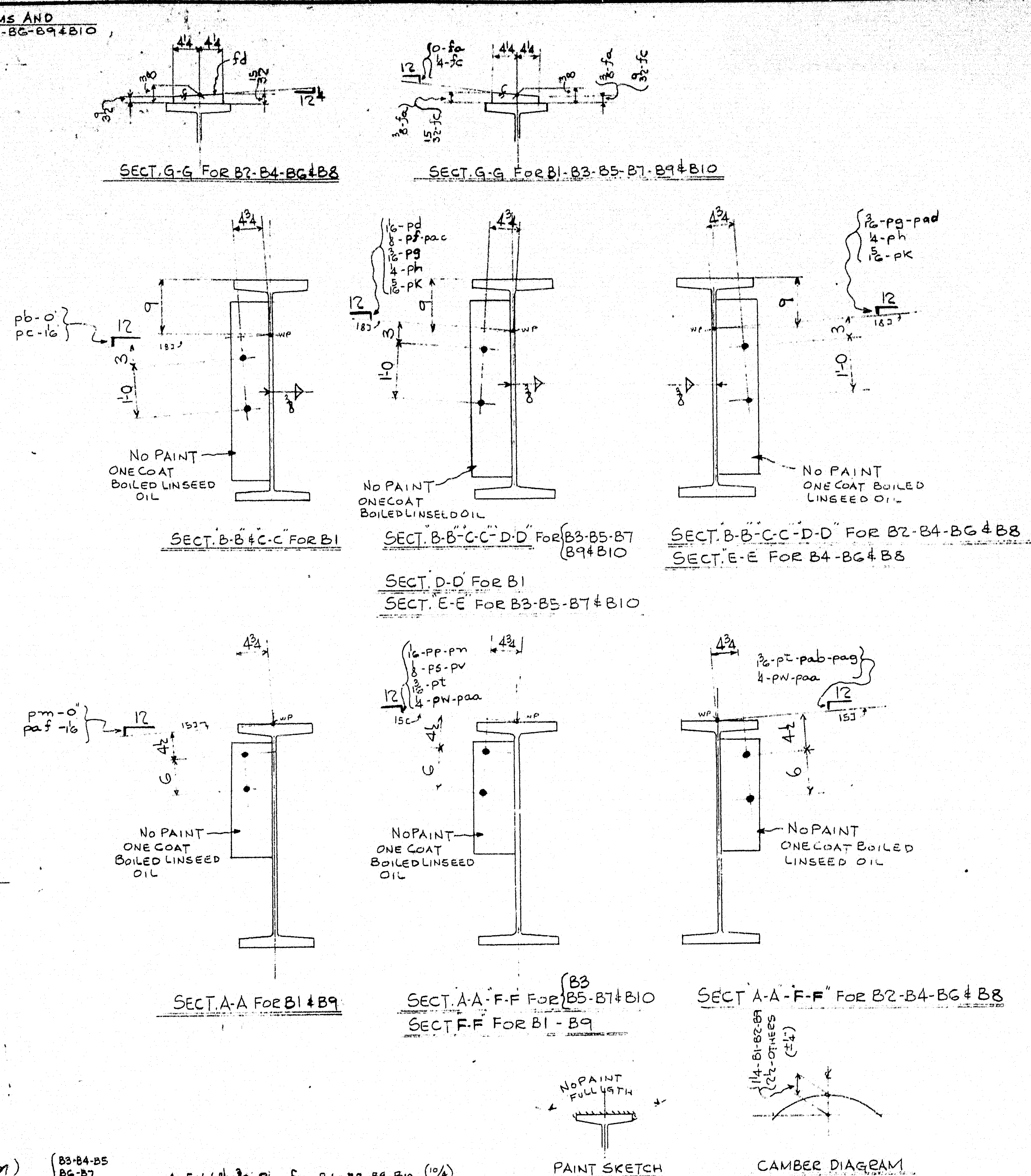
WELDING No

SHOP CONTACT SURFACES No

BEARING DETAILS	
TEMP.	INTERSTATE #95
BLVD.	OVER MAINE CENTRAL R.R. & COUNTY ROAD PENT (12/15)
CITY	WATERVILLE MAINE; CUST: CALLAHAN BROTHERS INC.
STATE	BETHLEHEM STEEL COMPANY
DESIGN	FABRICATED STEEL CONSTRUCTION
CONSTR.	BETHLEHEM DRAFTING ROOM BETH. WORKS
Q. NO.	IN CHARGE: NATHANIELS MADE BY: JH 12/15/56
WEIGHTS	CHECKED BY: R 12/15/58 TRACED BY: LAST ASSEM. MK.
REVISION	REVISION
SHOP NO.	CONTRACT CP4424
SHEET	1

75-125 C





ETCH CAMBER DIAGRAM

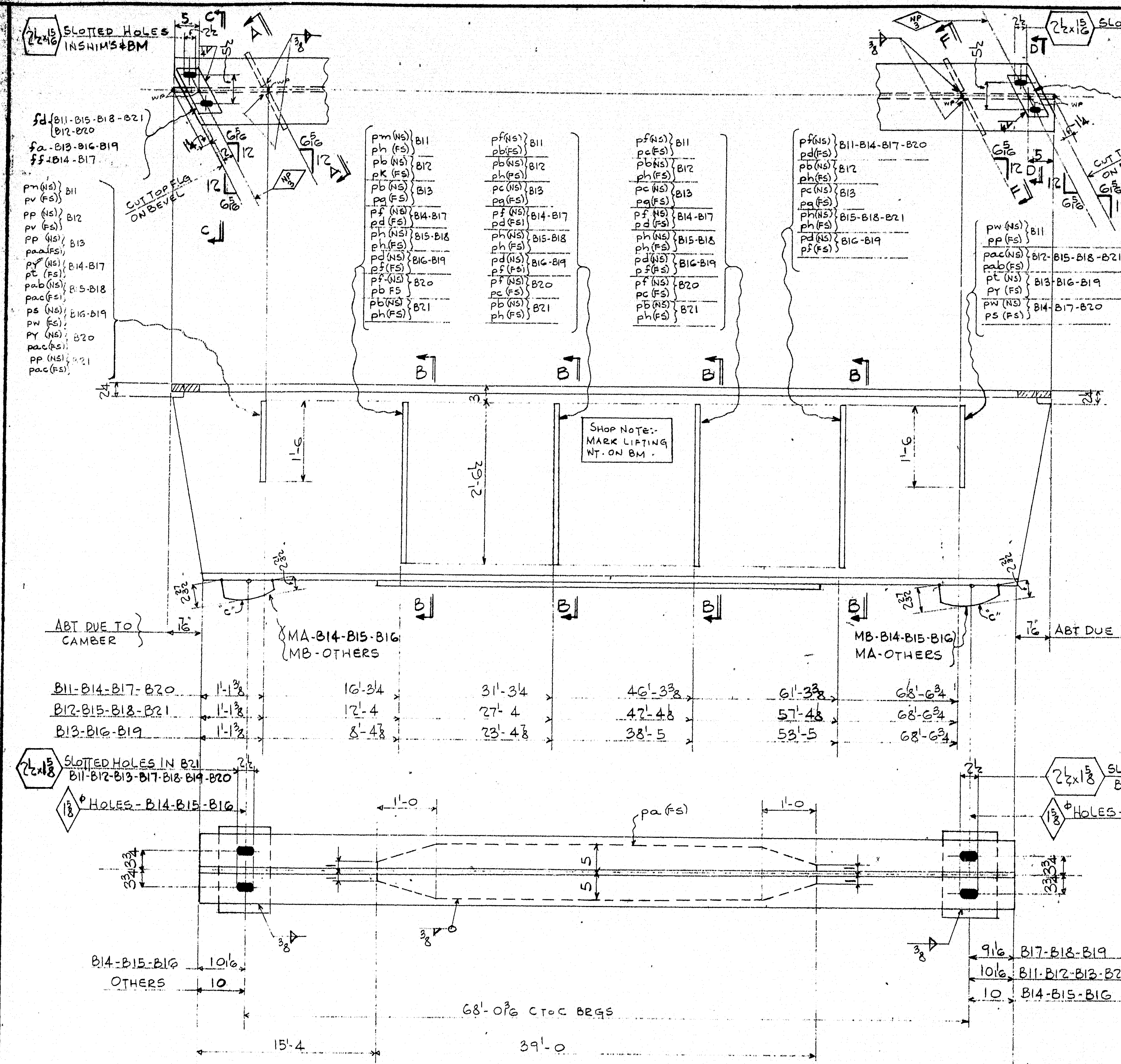
NOTES:--

1- FOR GEN NOTES & WELDING SPECS, SEE SHT E2

2- MAT'L ASTM-A7 UNLESS NOTED

STRINGERS B1 TO B10

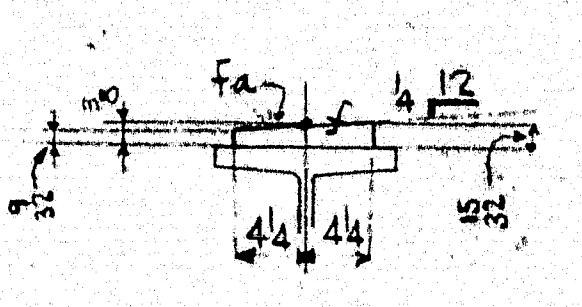
TEMP.	INTERSTATE #95
WELD	FROM
LAYOUT	OVER MAINE CENTRAL R.R. & COUNTY ROAD 6 (25) 1133
FITTER	WATERVILLE, MAINE
SHOP INSP.	BETHLEHEM STEEL COMPANY
P.G. WK.	FABRICATED STEEL CONSTRUCTION
S. SMITH	BETHLEHEM DRAFTING ROOM
MCH.	CHECKED BY R. 12-15-58
ERECT.	TRACED BY
O. INSP.	MADE BY H. 12/11/58
WEIGHTS	IN CHARGE NATIONAL ROSS
	REVISED LAST ASSEM. MK.



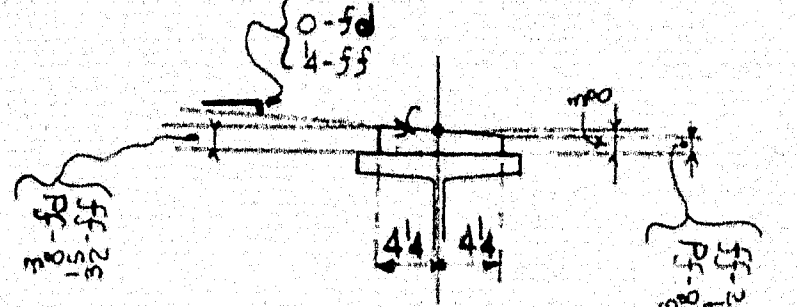
MARK	NO	SPAN
B11	ONE	?
B12	ONE	?
B13	2	246
B14	2	347
B15	2	347
B16	2	347
B17	2	448
B18	2	448
B19	2	448
B20	ONE	6
B21	ONE	6

MARK	NO	SPAN
B11	ONE	?
B12	ONE	?
B13	2	246
B14	2	347
B15	2	347
B16	2	347
B17	2	448
B18	2	448
B19	2	448
B20	ONE	6
B21	ONE	6

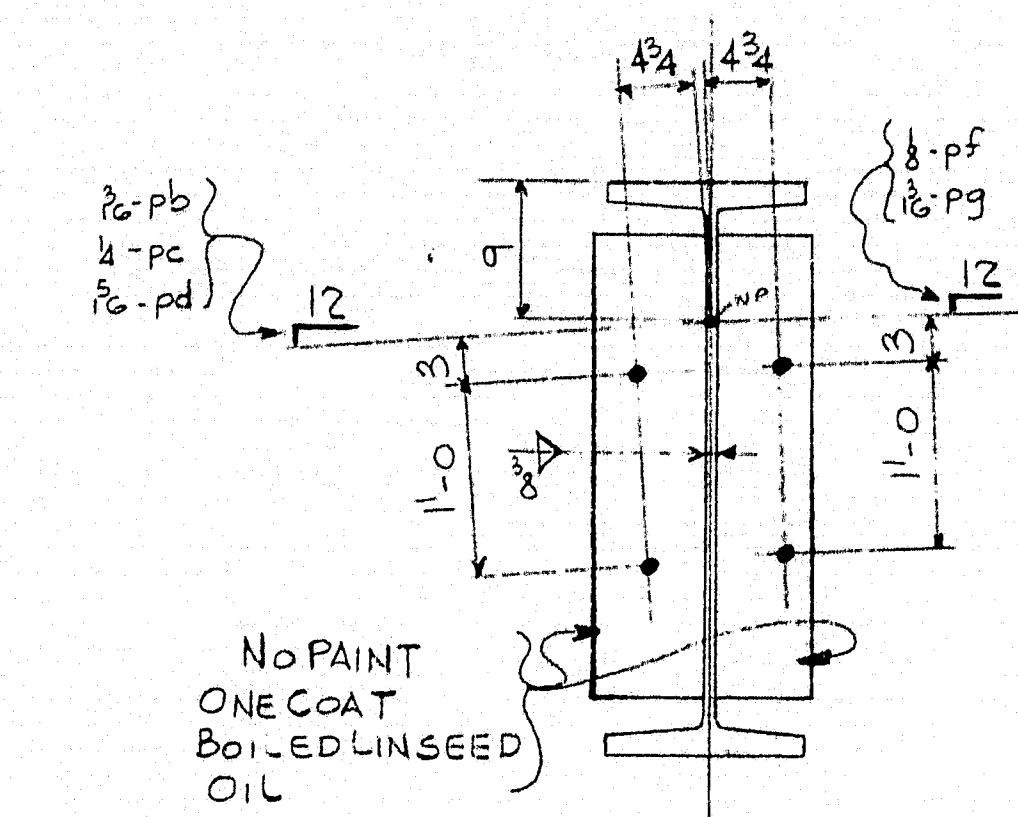
- 1/8" 18-Cov. R10x139'-0" - pa ASTM A373 (UM)
- 11 - R G x 2 x 2'-0" - pb 1-B11, 4-B12, 1-B13, 1-B20, 3-B21
 - 9 - R G x 2 x 2'-0" - pc 1-B11, 8-B13, 2-B20
 - 34 - R G x 2 x 2'-0" - pd 1-B11, 4-B14, 8-B16, 1-B17, 1-B19, 1-B20
 - 39 - R G x 2 x 2'-0" - pf 3-B11, 4-B14, 8-B16, 1-B17, 1-B19, 1-B20
 - 8 - R G x 2 x 2'-0" - pg B13
 - 41 - R G x 2 x 2'-0" - ph 1-B11, 3-B12, 8-B15, 1-B18, 5-B21
 - 1 - R G x 2 x 2'-0" - pk B12
 - 1 - R G x 2 x 2'-0" - pm B11
 - 5 - R G x 2 x 2'-0" - pp B11-B12, 8-B21
 - 9 - R G x 2 x 2'-0" - ps B14-B16, 1-B17, 1-B19, 1-B20
 - 10 - R G x 2 x 2'-0" - pt B13-B14, 8-B16, 1-B17, 1-B19
 - 2 - R G x 2 x 2'-0" - pv B11-B12
 - 10 - R G x 2 x 2'-0" - pw B14-B16, 1-B17, 1-B19, 1-B20
 - 11 - R G x 2 x 2'-0" - py B13-B14, 8-B16, 1-B17, 1-B19, 1-B20
 - 2 - R G x 2 x 2'-0" - pa B13
 - 10 - R G x 2 x 2'-0" - pb 2-B15, 1-B18, 1-B19, 1-B20
 - 12 - R G x 2 x 2'-0" - pc 2-B15, 1-B18, 1-B19, 1-B20



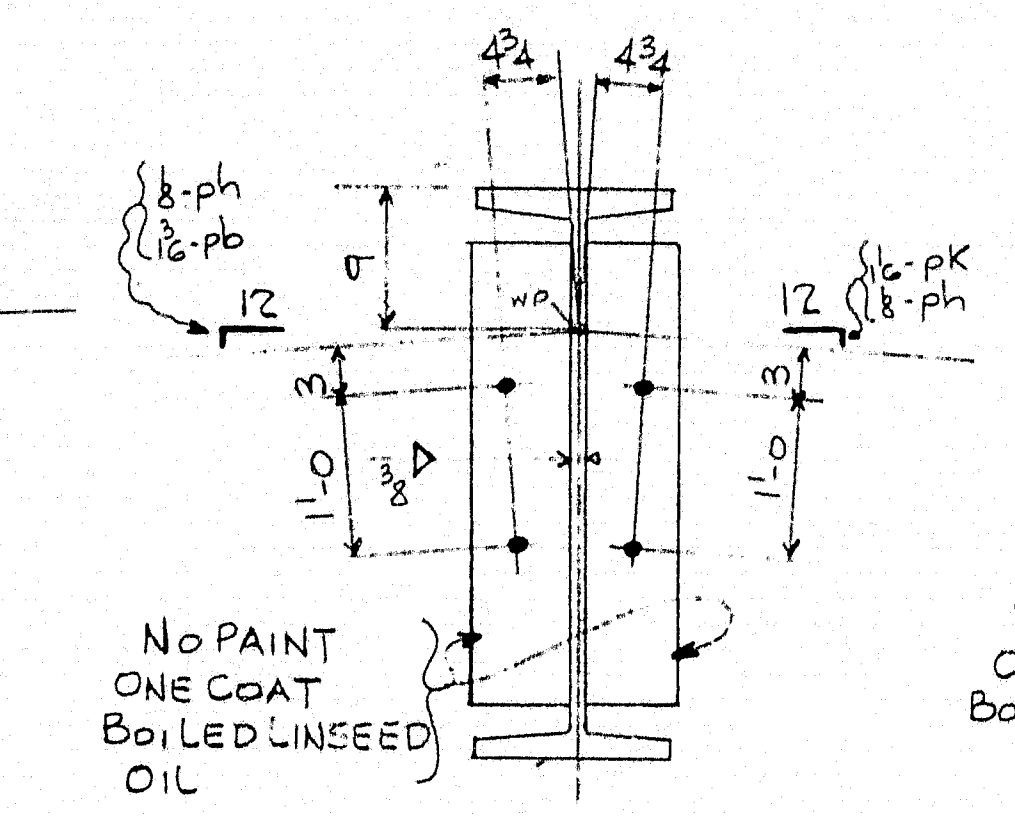
SECT. C-C FOR B19
SECT. C-C & D-D FOR B13 & B16



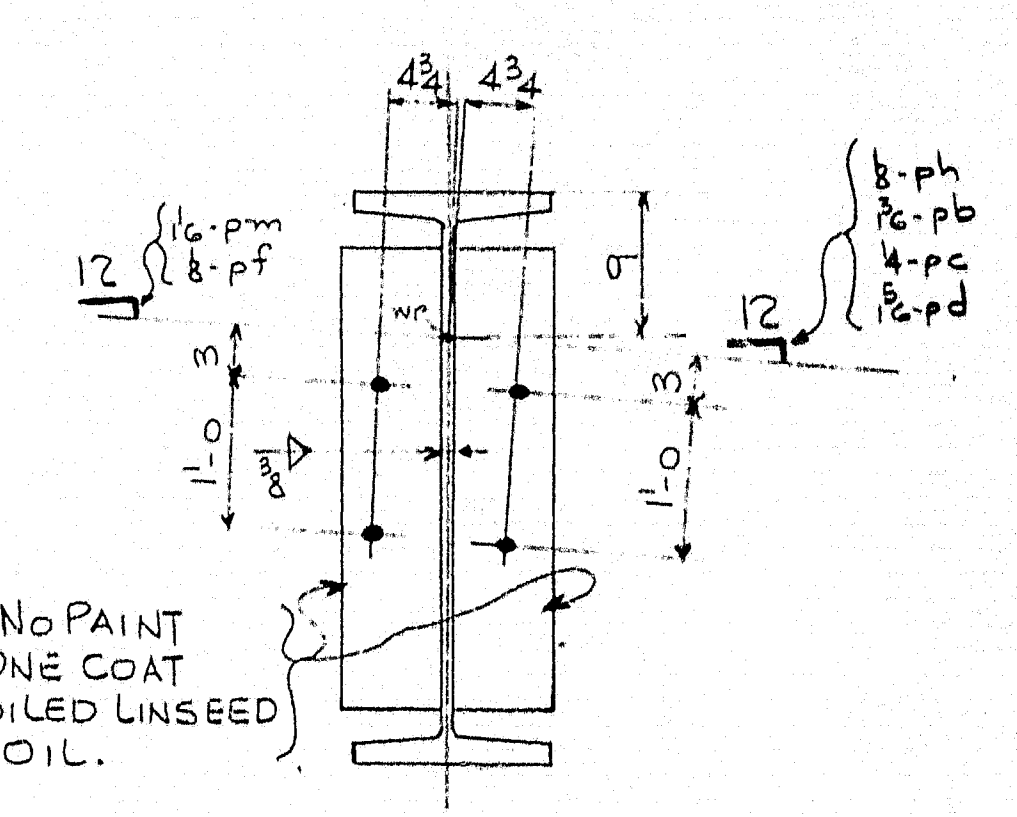
SECT. C-C FOR B17 & B18
SECT. C-C & D-D FOR B11-B14-B15-B20-B21 & B12



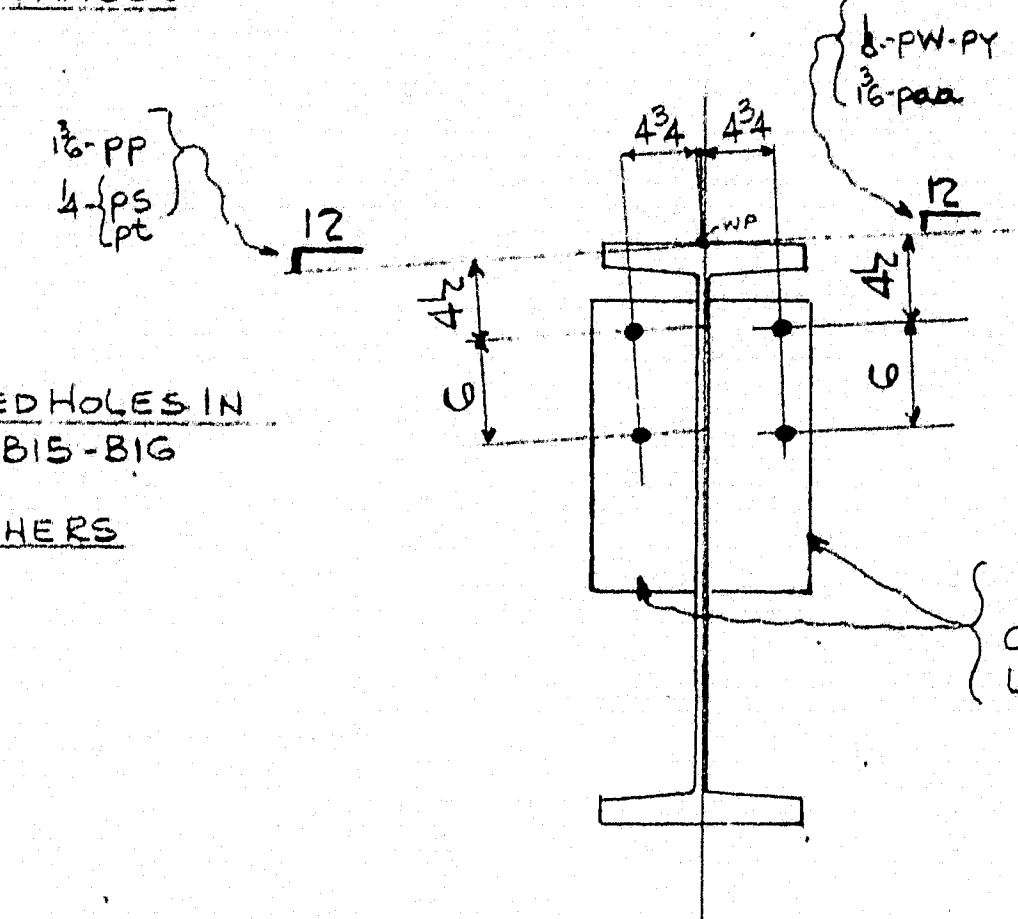
SECT. B-B FOR B13-B16 & B19



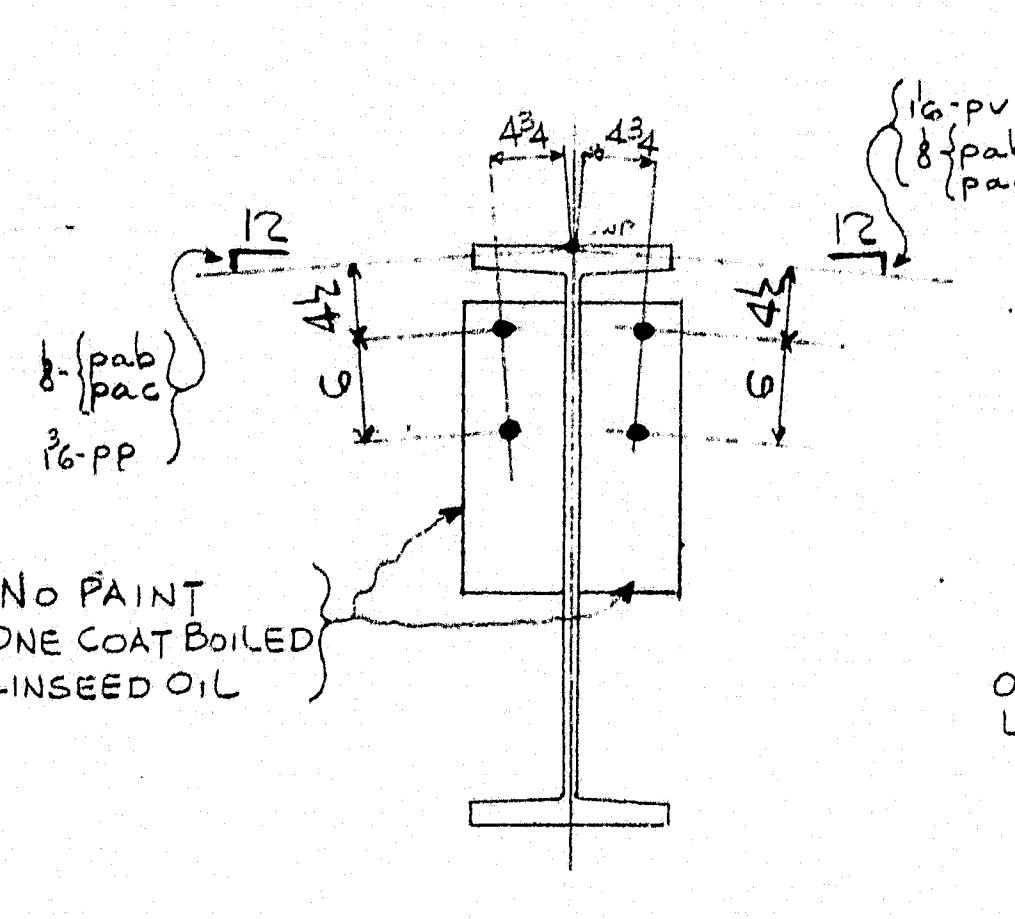
SECT. B-B FOR B17-B18 & B21



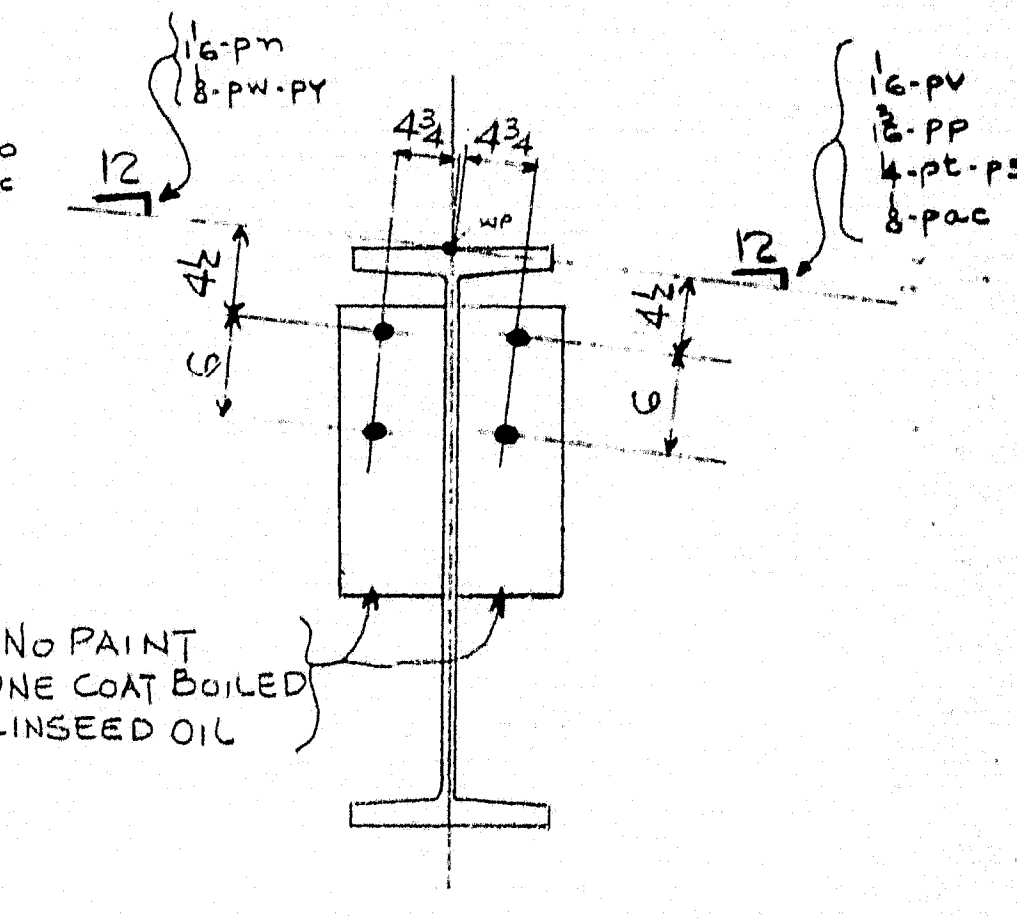
SECT. B-B FOR B11-B14-B17 & B20



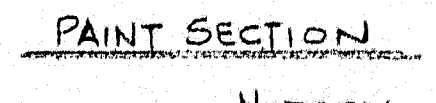
SECT. A-A & F-F FOR B13-B16 & B19



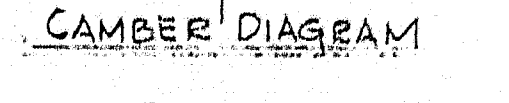
SECT. A-A & F-F FOR B15-B18 & B21



SECT. A-A & F-F FOR B14-B17 & B20



PAINT SECTION

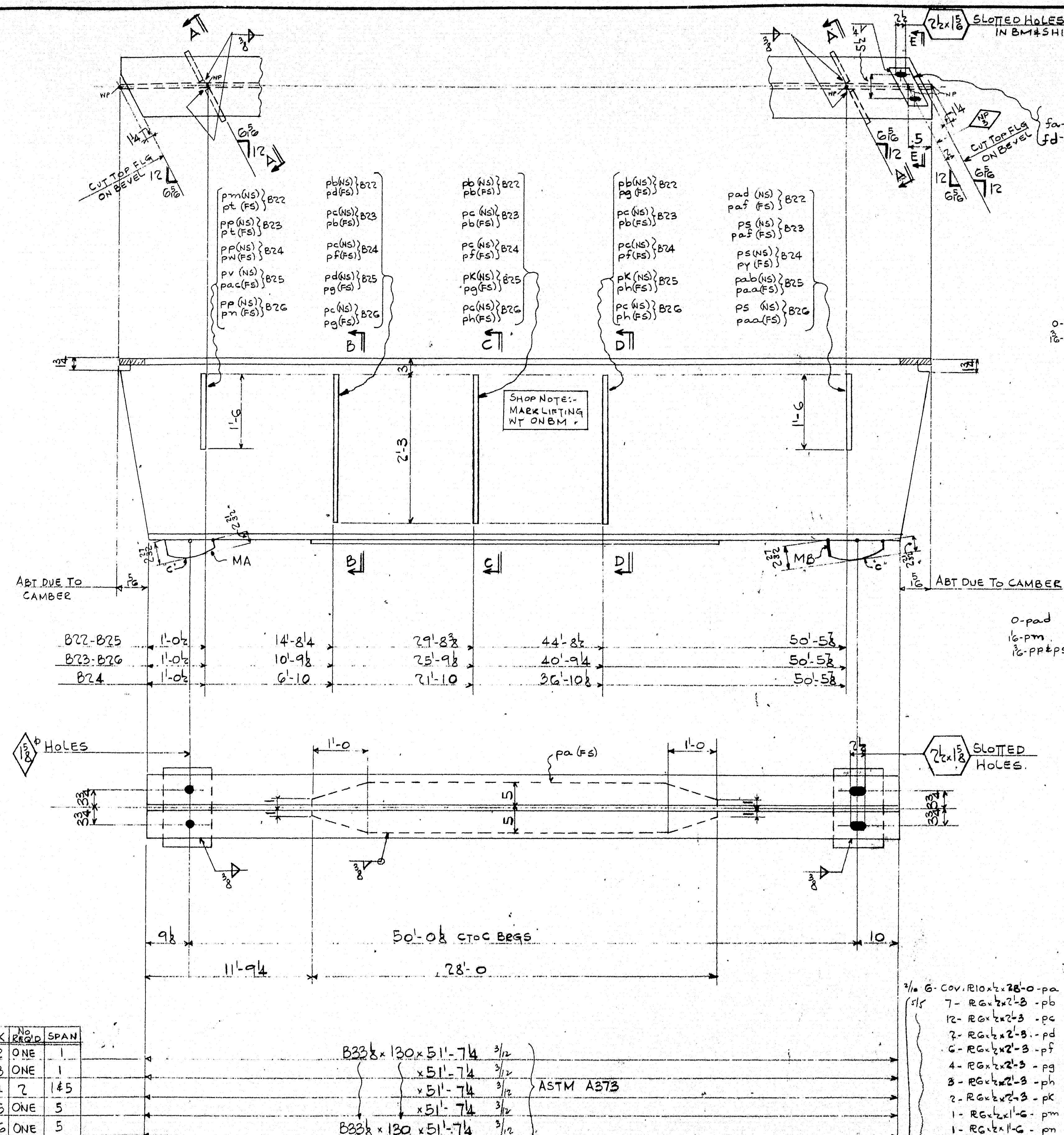


CAMBER DIAGRAM

NOTES:
1. FOR GEN. NOTES & WELDING SPEC. SEE SHT E2
2. MAT'L ASTM A7 UNLESS NOTED

STRINGERS B11 TO B21

TEMP.	INTERSTATE #95
SUP.	OVER MAINE CENTRAL R.R. (COUNTY ROAD 613) 132
LAYOUT	WATERVILLE, MAINE
FITTER	BETHLEHEM STEEL COMPANY
SHOP INSP.	FABRICATED STEEL CONSTRUCTION
PE. WK.	BETHLEHEM DRAFTING ROOM
B. SMITH	MADE BY
MCH.	IN CHARGE
ERECT.	CHECKED BY
O. INSP.	REVISOR
WEIGHTS	LAST ASSEM. MK.
	SHOP NO. CONTRACT CP4434
	SHEET 3



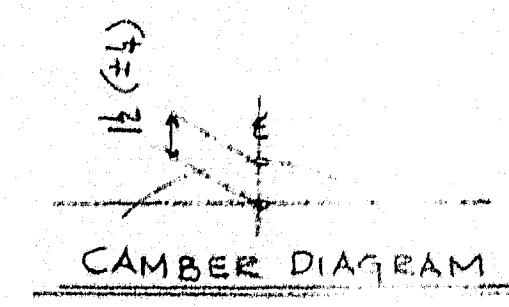
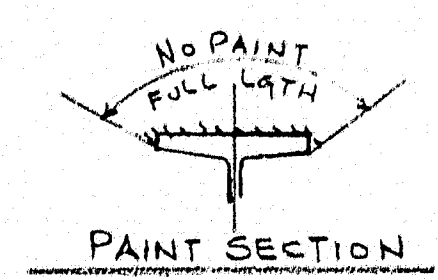
MARK	NO.	SPAN
B22	ONE	1
B23	ONE	1
B24	2	145
B25	ONE	5
B26	ONE	5

B22 x 130 x 51'-7 1/2
 x 51'-7 1/2
 x 51'-7 1/2
 x 51'-7 1/2
 B22 x 130 x 51'-7 1/2

SPECIAL QUALITY

- 1/2 G. Cov. R10 x 2 x 28'-0" - pa
- 7 - R6 x 2 x 2'-3" - pb
- 12 - R6 x 2 x 2'-3" - pc
- 2 - R6 x 2 x 2'-3" - pd
- 6 - R6 x 2 x 2'-3" - pf
- 4 - R6 x 2 x 2'-3" - pg
- 3 - R6 x 2 x 2'-3" - ph
- 2 - R6 x 2 x 2'-3" - pk
- 1 - R6 x 2 x 1'-6" - pm
- 1 - R6 x 2 x 1'-6" - pn
- 4 - R6 x 2 x 1'-6" - pp
- 4 - R6 x 2 x 1'-6" - ps
- 2 - R6 x 2 x 1'-6" - pt
- 1 - R6 x 2 x 1'-6" - pv
- 2 - R6 x 2 x 1'-6" - pw
- 2 - R6 x 2 x 1'-6" - py
- 2 - R6 x 2 x 1'-6" - pz

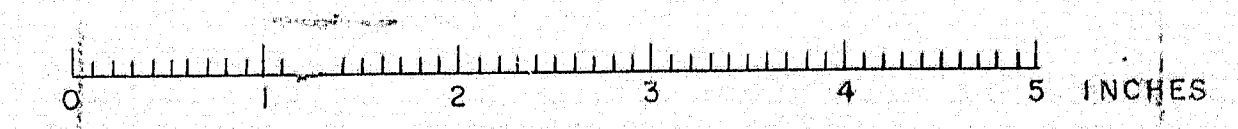
- 1 - R6 x 2 x 1'-6" - pa
- 1 - R6 x 2 x 1'-6" - pb
- 1 - R6 x 2 x 1'-6" - pc
- 1 - R6 x 2 x 1'-6" - pd
- 1 - R6 x 2 x 1'-6" - pf
- 1 - R6 x 2 x 1'-6" - pg
- 1 - R6 x 2 x 1'-6" - ph
- 1 - R6 x 2 x 1'-6" - pk
- 1 - R6 x 2 x 1'-6" - pm
- 1 - R6 x 2 x 1'-6" - pn
- 1 - R6 x 2 x 1'-6" - pp
- 1 - R6 x 2 x 1'-6" - ps
- 1 - R6 x 2 x 1'-6" - pt
- 1 - R6 x 2 x 1'-6" - pv
- 1 - R6 x 2 x 1'-6" - pw
- 1 - R6 x 2 x 1'-6" - py
- 1 - R6 x 2 x 1'-6" - pz



NOTES
 1. FOR GEN. NOTES & WELDING SPECS SEE SH E2
 2. MAT'L ASTM A7 UNLESS NOTED

STRINGERS B22 TO B26

TEMP.	INTERSTATE #95
SUPT.	OVER MAINE CENTRAL R.R. COUNTY ROAD 4 (2) 133
LAYOUT	WATERVILLE MAINE
FITTER	BETHLEHEM STEEL COMPANY
SHOP INSP.	FABRICATED STEEL CONSTRUCTION
P.O. WK.	BETHLEHEM STEEL COMPANY
MECH.	MADE BY
ERECT.	CHECKED BY
WEIGHTS	TRACED BY
	REVISOR
	LAST ASSEM. MK.
SHOP NO.	CONTRACT CP4434
	SHEET 4





STATE OF MAINE STATE HIGHWAY COMMISSION

PLANS

WATERVILLE

KENNEBEC COUNTY

MAINE FEDERAL AID INTERSTATE #95

PROJECT NO. I-95-6(24)124

TOTAL LENGTH 0.095 MILES

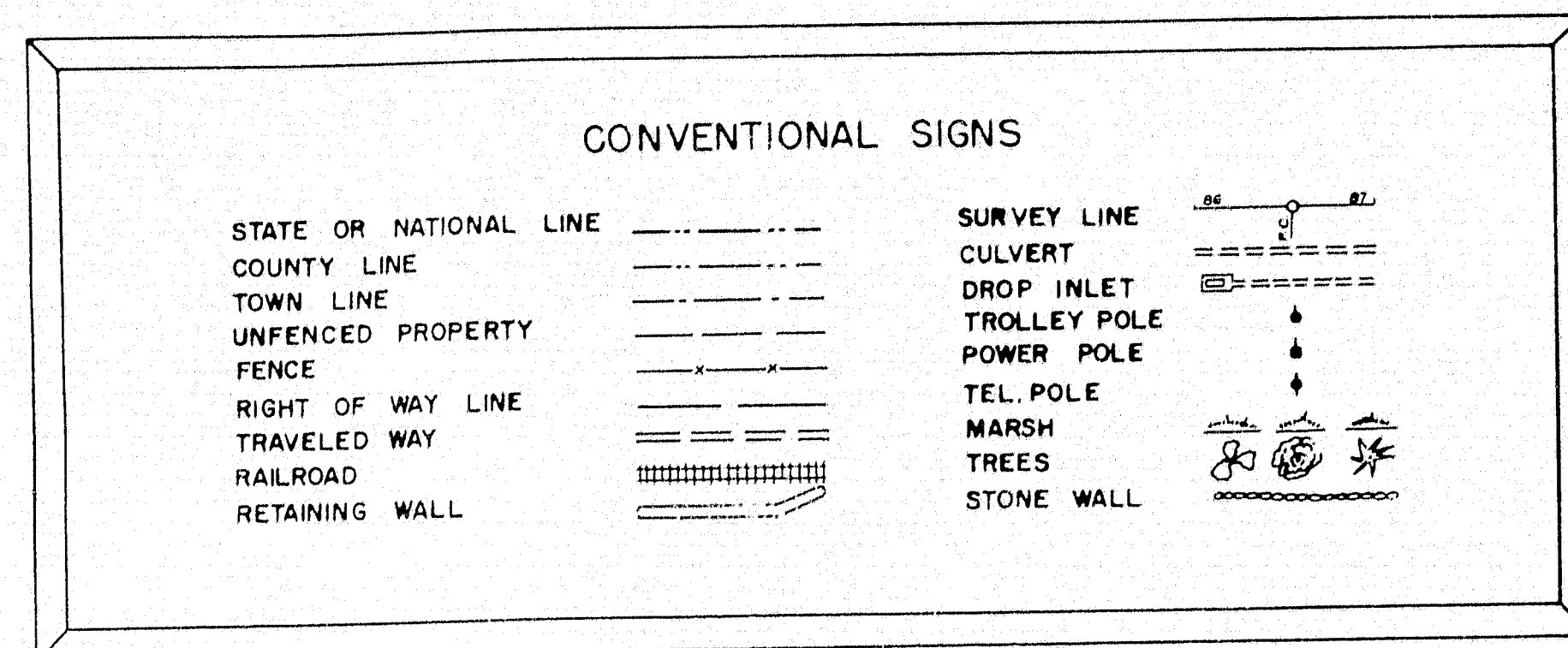
SCALES { PLAN 1 IN. = 50 FT.
PROFILE { HOR. 1 IN. = 50 FT.
VER. 1 IN. = 5 FT.
CROSS SECTIONS 1 IN. = 10 FT.

INTERSTATE #95 BRIDGE OVER MAIN STREET

INDEX OF SHEETS

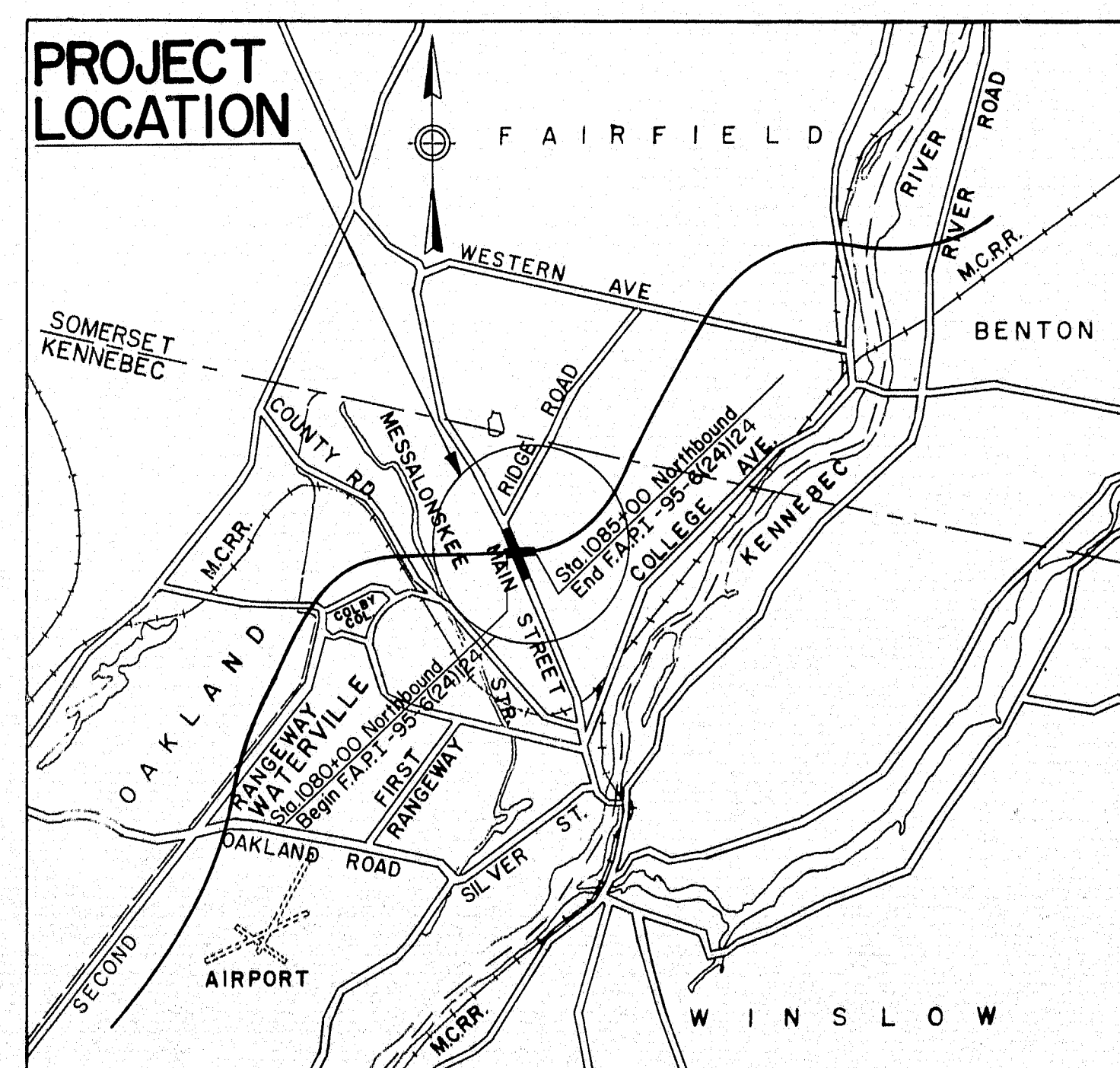
SHEET NO.

1	Title Sheet
2-4	Typical Sections
5	Quantities
6-10	Standard Details
11-12	Special Details
13	Subsurface Data
14	General Plan and Profile—Main Street
15	Profile—Sta. 1075+00 to Sta. 1089+00
16	Profile—Ramps "E", "F", "G" & "H"
17	Grading, Pavement & Curb Tie Plan—Main St. Interchange
18-24	Cross Sections
25-34	Bridge Plans



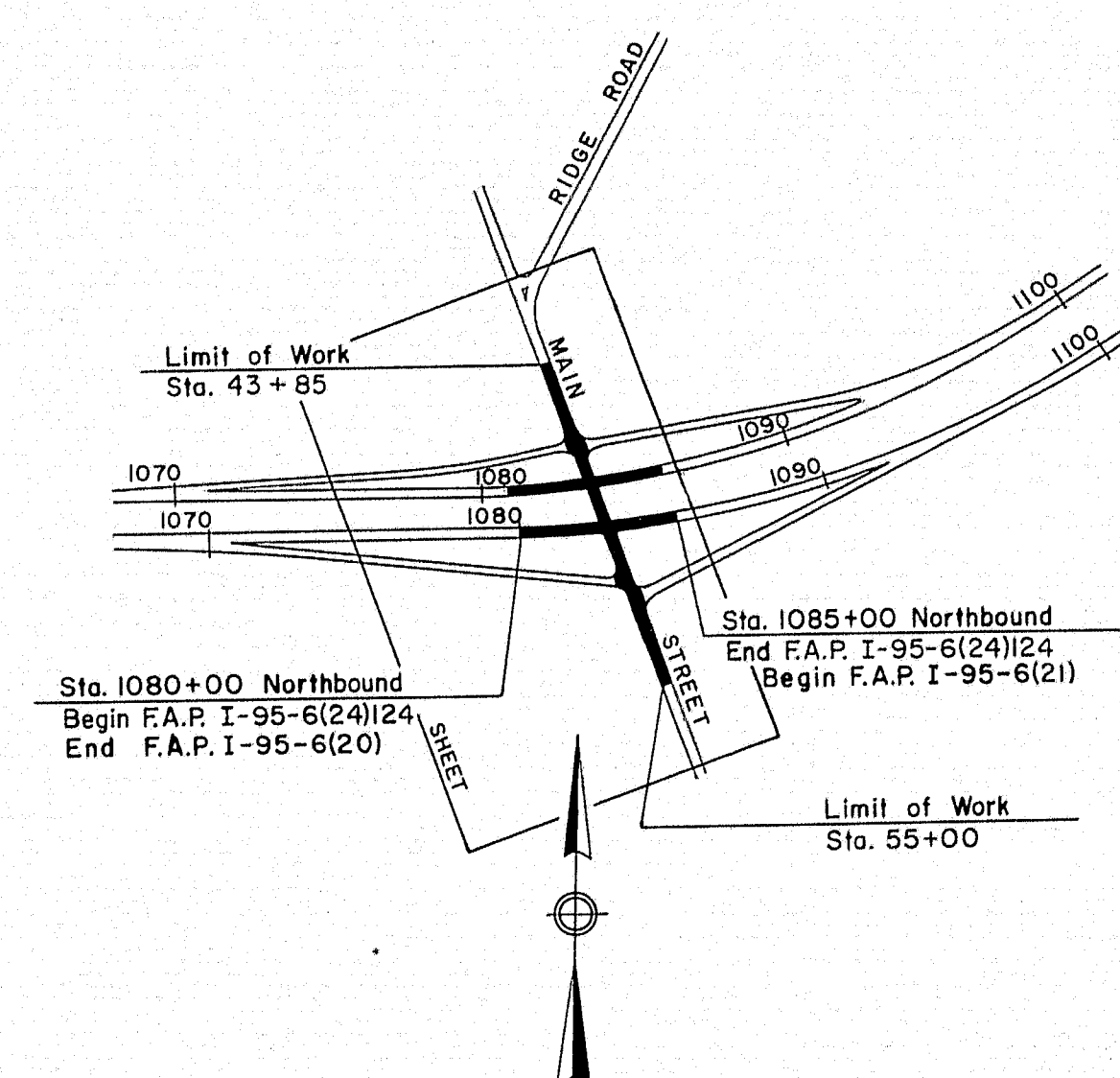
Interstate #95 and Main Street Interchange Ramps shall be Constructed to Top of Lower Course Crushed Gravel Base only, within the Limits Indicated on the Plans. Construction of Main Street is to include Pavement. Construction of Main Street Bridge is included in this Contract

All work contemplated under this Contract to be Governed by and in conformity with the Standard Specifications, Highways and Bridges, Revision Of Jan. 1956, except as modified on these Plans And by the Special Provisions.



A PORTION OF KENNEBEC AND SOMERSET COUNTIES

APPROX. SCALE 1 IN. = 1 MILE



LAYOUT PLAN

SCALE: 1" = 600'

INTERSTATE #95

A.D.T.(1960)	= 4790
A.D.T.(1980)	= 7620
D.H.V.(1980)	= 1143
D.(1980)	= 65 %
T.(1980)	= 15 %
V.(M.P.H.)	= 60

MAIN STREET

A.D.T.(1960)	= 6250
A.D.T.(1980)	= 7940
D.H.V.(1980)	= 1190
D.(1980)	= 65 %
T.(1980)	= 15 %
V.(M.P.H.)	= 45

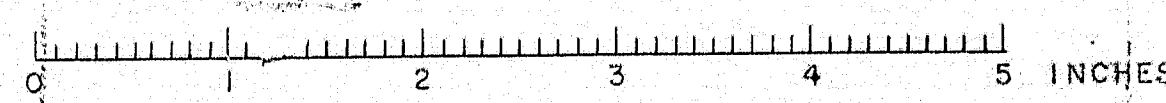
APPROVED:
MAINE STATE HIGHWAY COMMISSION

David H. Sturges
CHAIRMAN
Robert L. Williams
CHIEF ENGINEER

CONSULTING ENGINEERS
THE CLARKESON ENGINEERING COMPANY, INC.
BOSTON MASS.
John Clarkson
CONSULTING ENGINEER
9/20/58
DATE

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
REGION 1
APPROVED:
DIVISION ENGINEER DATE

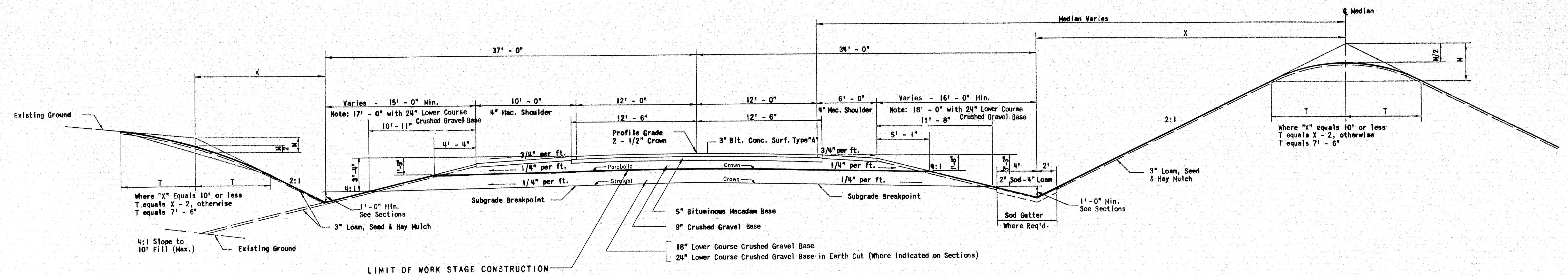
75-126



GRADING CONTRACT — STAGE CONSTRUCTION
INTERSTATE #95 TO BE CONSTRUCTED TO TOP OF LOWER COURSE
CRUSHED GRAVEL BASE ONLY UNLESS OTHERWISE INDICATED

U. S. R. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(24)	2	34

WATERVILLE INTERSTATE

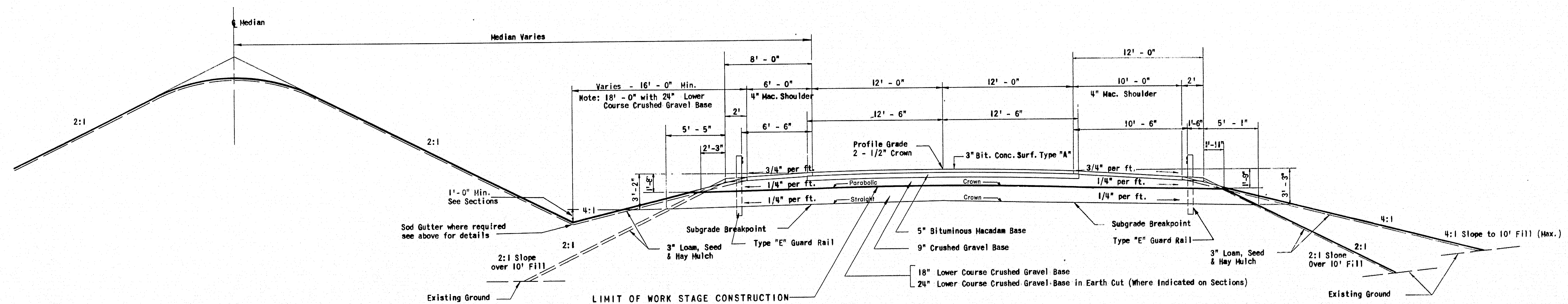


10' PAVED SHOULDER	CY/100 L.F.
4" Bituminous Macadam Surface	12.19
Crushed Gravel Base (to 4:1 Slope)	36.33
18" Lower Course Crushed Gravel Base	91.92
24" Lower Course Crushed Gravel Base	130.64

TYPICAL SECTION - NORMAL
 Scale: 1" = 5'
SOUTHBOUND INTERSTATE

24' PAVEMENT	CY/100 L.F.
3" Bituminous Concrete Surface	22.22
5" Bituminous Macadam Base (25' Wide)	38.58
9" Crushed Gravel Base	66.67
18" Lower Course Crushed Gravel Base	136.42
24" Lower Course Crushed Gravel Base	180.87

6' PAVED SHOULDER	CY/100 L.F.
4" Bituminous Macadam Surface	7.25
Crushed Gravel Base (to 4:1 Slope)	27.46
18" Lower Course Crushed Gravel Base	73.74
24" Lower Course Crushed Gravel Base	106.40



8' PAVED SHOULDER	CY/100 L.F.
4" Bituminous Macadam Surface	9.72
Crushed Gravel Base (to 2:1 Slope)	29.23
18" Lower Course Crushed Gravel Base	62.80

TYPICAL SECTION - NORMAL
 Scale: 1" = 5'
NORTHBOUND INTERSTATE

12' PAVED SHOULDER	CY/100 L.F.
4" Bituminous Macadam Surface	14.66
Crushed Gravel Base (to 2:1 Slope)	38.14
18" Lower Course Crushed Gravel Base	83.09

MAINE STATE HIGHWAY COMMISSION
 AUGUSTA, MAINE

TYPICAL SECTIONS

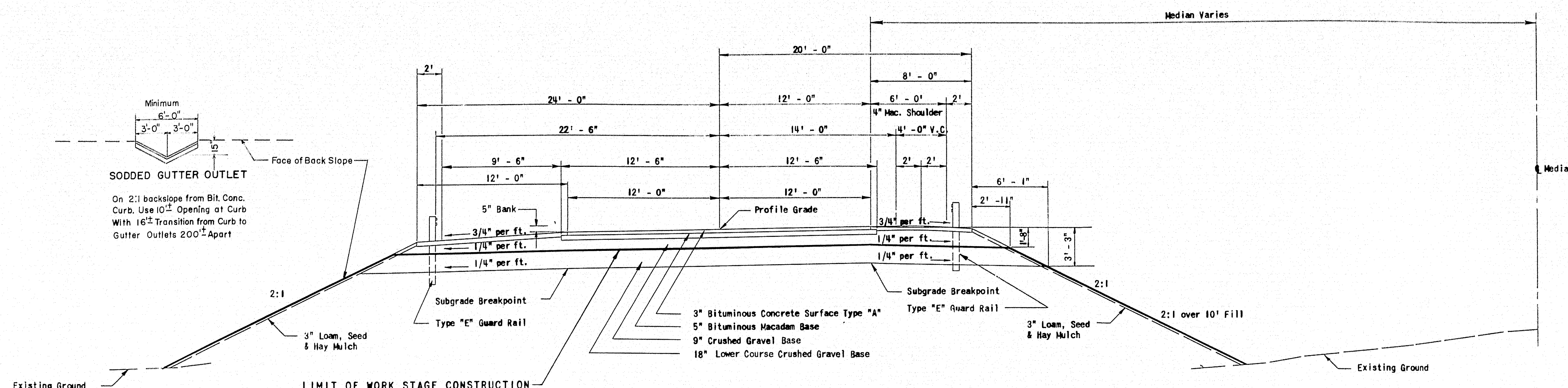
THE CLARKESON ENGINEERING CO., INC.
 CONSULTING ENGINEERS
 BOSTON MASSACHUSETTS

GRADING CONTRACT — STAGE CONSTRUCTION

INTERSTATE #95 TO BE CONSTRUCTED TO TOP OF LOWER COURSE
CRUSHED GRAVEL BASE ONLY UNLESS OTHERWISE INDICATED

S. P. R. REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(24)	3	34

WATERVILLE INTERSTATE

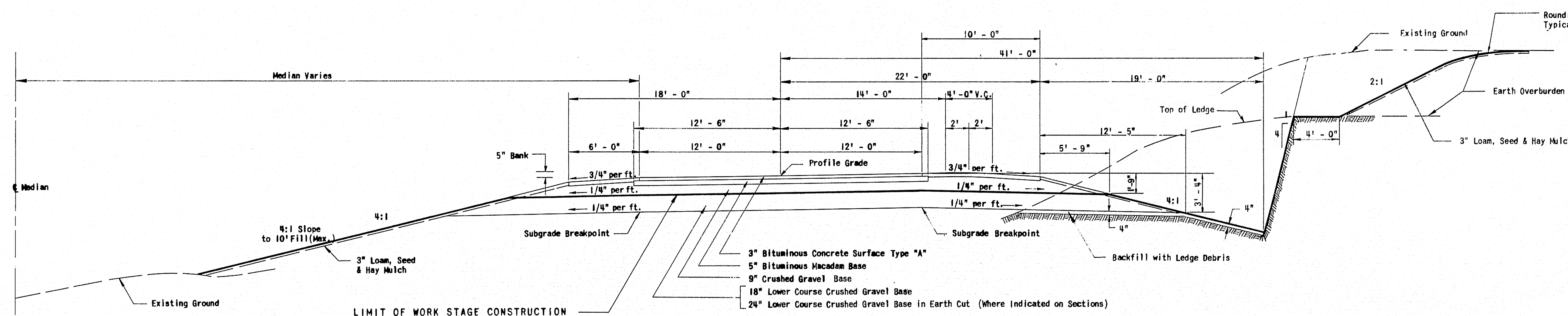


12' PAVED SHOULDER
For Quantities see Typical
Section - Normal

TYPICAL SECTION - SUPERELEVATED
Showing 8 = 5" Superelevation for D = 0° - 30°
Scale: 1" = 5'

24" PAVEMENT	CY/100 L.F.
3" Bituminous Concrete Surface	22.22
5" Bituminous Macadam Base (25' Wide)	38.58
9" Crushed Gravel Base	66.67
18" Lower Course Crushed Gravel Base	133.33
24" Lower Course Crushed Gravel Base	177.78

8' PAVED SHOULDER	CY/100 L.F.
4" Bituminous Macadam Surface	9.72
Crushed Gravel Base (to 2:1 Slope)	38.37
18" Lower Course Crushed Gravel Base (to 2:1 Slope)	66.15



6' PAVED SHOULDER
For Quantities see Typical
Section - Normal

TYPICAL SECTION - SUPERELEVATED
Showing 8 = 5" Superelevation for D = 0° - 30°
Scale: 1" = 5'

10' PAVED SHOULDER	CY/100 L.F.
4" Bituminous Macadam Surface	12.19
Crushed Gravel Base (to 4:1 Slope)	49.67
18" Lower Course Crushed Gravel Base	99.67
24" Lower Course Crushed Gravel Base	141.00

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

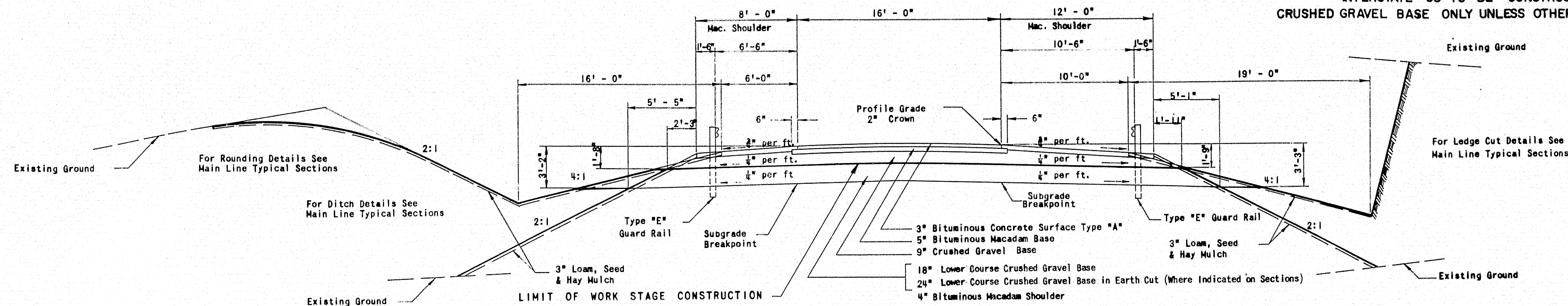
TYPICAL SECTIONS

THE CLARKESON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS

GRADING CONTRACT — STAGE CONSTRUCTION

B. P. R.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-8(24)	2	24

INTERSTATE #95 TO BE CONSTRUCTED TO TOP OF LOWER COURSE
CRUSHED GRAVEL BASE ONLY UNLESS OTHERWISE INDICATED

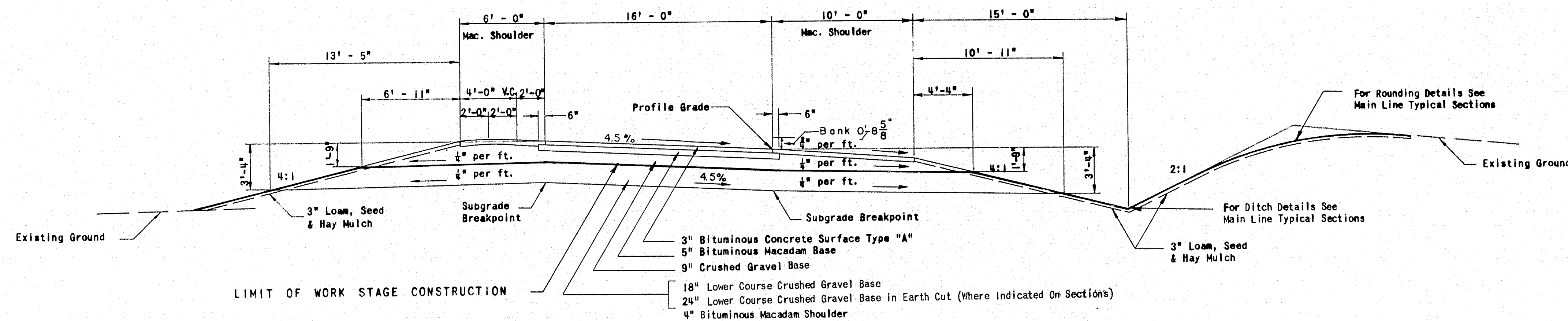


TYPICAL SECTION - ONE WAY RAMP - NORMAL
Scale: 1" = 5'

0' SHOULDER	C.Y./100 L.F.
4" Bituminous Macadam	9.72
Crushed Gravel Base (to 2:1)	29.23
18" Lower Course Crushed Gravel Base	62.80

16' PAVEMENT	C.Y./100 L.F.
3" Bituminous Concrete	14.81
5" Bituminous Macadam (25')	38.58
9" Crushed Gravel Base	44.44
18" Lower Course Crushed Gravel Base	88.89

12' SHOULDER	C.Y./100 L.F.
4" Bituminous Macadam	14.86
Crushed Gravel Base (to 2:1)	38.14
18" Lower Course Crushed Gravel Base	83.09

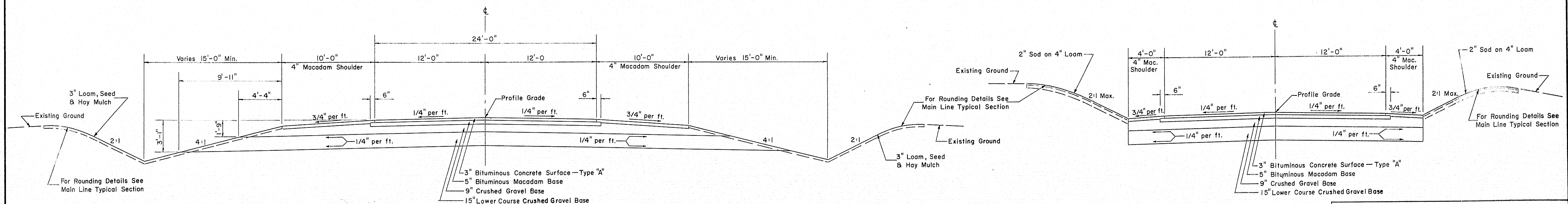


TYPICAL SECTION - ONE WAY RAMP - SUPERELEVATED
Scale: 1" = 5'

0' SHOULDER	C.Y./100 L.F.
4" Bituminous Macadam	7.26
Crushed Gravel Base (to 4:1)	38.14
18" Lower Course Crushed Gravel Base	83.84

16' PAVEMENT	C.Y./100 L.F.
3" Bituminous Concrete	14.81
5" Bituminous Macadam (25')	38.58
9" Crushed Gravel Base	44.44
18" Lower Course Crushed Gravel Base	88.89

10' SHOULDER	C.Y./100 L.F.
4" Bituminous Macadam	12.19
Crushed Gravel Base (to 4:1)	36.33
18" Lower Course Crushed Gravel Base	91.92



MAIN STREET — NORMAL
Not to Scale
Sta. 47+00 to 55+00

10' SHOULDER	C.Y./100 L.F.
4" Bituminous Macadam	12.19
Crushed Gravel Base	36.33
15" Lower Course Crushed Gravel Base	72.79

24' PAVEMENT	C.Y./100 L.F.
3" Bituminous Concrete	22.22
5" Bituminous Macadam (25')	38.58
9" Crushed Gravel Base	66.67
15" Lower Course Crushed Gravel Base	111.11

4' SHOULDER	C.Y./100 L.F.
4" Bituminous Macadam	4.79
Crushed Gravel Base	14.20
15" Lower Course Crushed Gravel Base	18.52

MAIN STREET — NORMAL
Not to Scale
Sta. 44+00 to 46+00

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

TYPICAL SECTIONS

THE CLARKESON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS

ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	QUANTITY	UNIT
201-5	Clearing	0.25	Acres
202-5	Removing Trees (3" - 24")	10	Each
202-6	Removing Trees (Over 24")	7	Each
203-9	Earth Excavation	0.011,000	C.Y.
204-10	Structural Earth Excavation - Drainage	450	C.Y.
204-11	Structural Rock Excavation - Drainage	104	C.Y.
204-14	Structural Earth Excavation - Piers	377	C.Y.
204-15	Structural Rock Excavation - Piers	47	C.Y.
205-8	Common Borrow	16,571,600	C.Y.
205-9	Granular Borrow	4,000	C.Y.
202-7	Gravel Base Course - In Place Measurement	440	C.Y.
202-9	Crushed Gravel Base Course - In Place Measurement	4,550	C.Y.
202-13	Lower Course Crushed Gravel Base - In Place Measurement	4,600	C.Y.
203-6	Bituminous Macadam Base Course	860	Ton
206-5	Overhaul - In Place Measurement	4,600	Yd.Mi.
206-6	Overhaul - Pit Measurement	3,600	Yd.Mi.
209-5	Stripping Pits	1,100	C.Y.
210-6	Sprinkling	40	Units
211-6	Calcium Chloride	40	Ton
211-11	Gravel Surface Course	40	C.Y.
212-13	Bituminous Macadam Surface Course	226	Ton
212-28	Bituminous Concrete Surface Course - Type "A"	6,414	Ton
212-7	Asphalt Cement	2,472	Gal.
212-8	Emulsified Asphalt (Quick Setting Grade)	6,900	Gal.
212-11	15" Asphalt Coated Corrugated Metal Pipe	20	L.F.
212-14	24" Asphalt Coated Corrugated Metal Pipe	40	L.F.
212-15	30" Asphalt Coated Corrugated Metal Pipe	40	L.F.
213-11	15" Reinforced Concrete Pipe	40	L.F.
213-13	24" Reinforced Concrete Pipe	40	L.F.
213-14	30" Reinforced Concrete Pipe	40	L.F.
215-24	Catch Basins - Type "F"	3	Each
215-26	Catch Basins - Type "H"	1	Each
216-9	Underdrain - Type "A"	100	L.F.
216-10	Underdrain - Type "B"	100	L.F.
216-13	18" Underdrain - Type "C"	280	L.F.
216-16	Underdrain Outlets	50	L.F.
217-8	Metal Endwall for 24" Pipe	1	Each
217-9	Metal Endwall for 30" Pipe	1	Each
217-33	Portland Cement Concrete, Abutments & Retaining Walls	2,305	C.Y.
217-35	Portland Cement Concrete, Piers	2,270	C.Y.
217-40	Portland Cement Concrete, Roadway & Sidewalk Slabs on Steel Bridges	4,430	C.Y.
217-42	Portland Cement	6,262	Bbls.
217-107	Structural Steel, Fabricated and Delivered	358,700	Lbs.
217-108	Structural Steel, Erection	358,700	Lbs.
217-13	Reinforcing Steel, Delivered	435,400	Lbs.
217-14	Reinforcing Steel, Placing	135,400	Lbs.
217-16	Steel H-Beam Piles - 42 lbs./foot	1,130	L.F.
217-5	Bridge Drainage	1	L.S.
217-5	Shoe Connectors, Delivered & Placed	1	L.S.
217-6	French Drains	9	C.Y.
217-15	Aluminum Railing	6	L.F.
217-14	Granite Edging	440	L.F.
217-15	Granite Edging - Circular	50	L.F.
217-35	Guard Posts - Type "A"	3	Each
217-10	Hand Laid Riprap	15	C.Y.
217-8	Loam Excavation	1,700	C.Y.
217-9	Sodding	2,300	C.Y.
217-10	Seeding - Parkway Mixture	94	Units
217-6	Hay Mulch	1	Ton
217-6	Project Markers	1	Each
217-6	Underdrain Outlet Markers	2	Each
217-6	Traffic Officers	1	Man Hrs.
217-7	Removal or Razing Building No. 1	1	Each
217-8	Removal or Razing Building No. 2	1	Each
217-10	Portable Barricade	10	Each
217-13	Portable Barricade with Flashing Lights	2	Each
217-5	Slope Paving for Bridges	9,506	S.Y.
	Gravel for Foundations	40	C.Y.

CLEARING		
STATION TO STATION	SIDE	REMARKS
1081 + 00 to 1083 + 52 S.B.	Rt.	
1081 + 14 to 1082 + 15 N.B.	Lt.	
Main Street		
49 + 85 to 52 + 40	Rt.	
52 + 40 to 53 + 00	Rt.	
53 + 00 to 54 + 75	Rt.	

TREES REMOVED			
STATION	SIDE	OFFSET	DESCRIPTION
Main Street			
46 + 88	Lt.	102'	11" Apple
49 + 75	Lt.	28'	23" Elm
52 + 30	Lt.	31'	24" Elm
52 + 38	Lt.	44'	24" Elm
52 + 50	Lt.	30'	19" Elm
52 + 72	Lt.	84'	22" Elm
53 + 03	Lt.	30'	20" Elm
53 + 20	Lt.	30'	18" Elm
53 + 33	Lt.	30'	19" Elm
54 + 12	Lt.	29'	17" Elm
47 + 10	Lt.	31'	29" Poplar
48 + 23	Lt.	32'	29" Maple
48 + 57	Lt.	31'	32" Maple
50 + 76	Lt.	27'	26" Elm
51 + 43	Lt.	29'	27" Elm
52 + 45	Lt.	53'	25" Elm
53 + 89	Lt.	30'	20" Elm

LOWER COURSE CRUSHED GRAVEL BASE	
STATION TO STATION	DESCRIPTION
Northbound	
1080 + 50 to 1081 + 50	18" Gravel Base
1083 + 18 to 1084 + 50	18" Gravel Base
Southbound	
1081 + 50 to 1082 + 78	18" Gravel Base
1084 + 44 to 1085 + 50	18" Gravel Base
Main Street	
44 + 00 to 55 + 00	15" Gravel Base
Ramp "E"	24" Gravel Base
Ramp "F"	18" Gravel Base
Ramp "G"	24" Gravel Base
Ramp "H"	18" Gravel Base

DRIVEWAYS		
STATION	SIDE	DESCRIPTION
Main Street		
44 + 40	Rt.	1" Gravel Surface Course
45 + 15	Lt.	1" Gravel Surface Course
45 + 20	Rt.	1" Gravel Surface Course
45 + 80	Lt.	1" Gravel Surface Course

ROADWAY CULVERTS						
STATION	SIZE	A.C.C.M.P. LEFT	R.C.P.	A.C.C.M.P. RIGHT	PIPE CONN.	METAL E.W.
Main Street						
44 + 00	15"		32'			
52 + 80	15"		40'			
Ramp "E"						
82 + 50	24"	16'	32'	20'	1	1
Ramp "G"						
82 + 00	30"	12'	32'	20'	1	1
Ramp "H"						
82 + 93	15"	16'	44'			

GRANITE EDGING		
STATION TO STATION	LENGTH	KIND
Main Street		
46 + 67 to 46 + 81 Rt.	14	Straight
47 + 00 to 47 + 13 Lt.	13	"
51 + 06 to 51 + 22 Rt.	16	"
52 + 24 to 52 + 83 Lt.	59	"
47 + 00 Lt.	5.2	Circular R=2'
47 + 13 Lt.	5.2	"
52 + 24 Lt.	5.2	"
52 + 83 Lt.	5.2	"
46 + 67 Rt.	5.2	"
46 + 81 Rt.	5.2	"
51 + 06 Rt.	5.2	"
51 + 22 Rt.	5.2	"

GUARD POSTS - TYPE "A"		
STATION	OFFSET	DESCRIPTION
3 To Be Used For Mail Boxes At Undetermined Locations		

LOAM EXCAVATION			
STATION TO STATION	DEPTH	AVERAGE WIDTH	LENGTH
1081 + 00 to 1083 + 52 S.B.	0.5'	105' 100'	225' 100'
1084 + 00 to 1087 + 00 N.B.	0.5'	110'	100'
Ramp "F"	0.8'	90'	130'
83 + 70 to 85 + 00	0.8'	90'	130'
Ramp "G"	0.5'	125'	110'
81 + 50 to 82 + 60	0.5'	125'	110'
Main Street			
46 + 00 to 55 + 00 Rt.	0.5'	20'	900'
Ramp "E"	0.5'	11'	90'

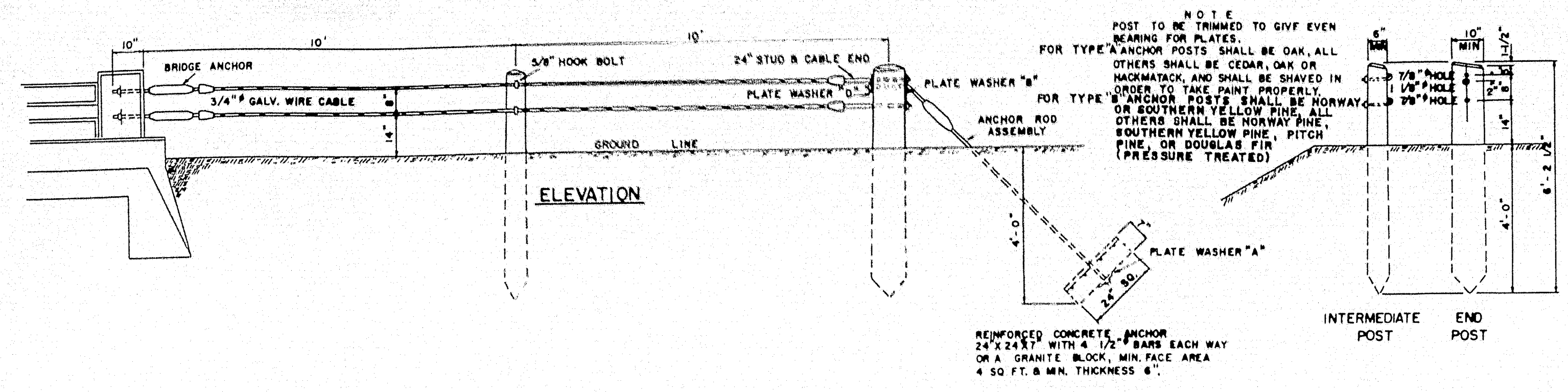
UNDERDRAIN				
STATION TO STATION	SIDE	SIZE & TYPE	LENGTH	
Main Street				
44 + 00 to 46 + 30	Lt.	18" - "G"	250'	
Undetermined		"A"	50'	
Undetermined		"B"	100'	
Undetermined		"G"	50'	
Undetermined		Underdrain Outlets	50'	

SODDING		
STATION TO STATION	SIDE	REMARKS
Main Street		
46 + 00 to 85 + 00 Lt. Ramp "F"	Lt.	
47 + 50 to 51 + 80	Lt.	
51 + 80 to 84 + 50 Lt. Ramp "H"	Lt.	
46 + 00 to 82 + 00 Lt. Ramp "E"	Rt.	
49 + 10	Rt.	
82 + 00 Rt. "E" to 81 + 50 Lt. "G"	Rt.	
81 + 50 Rt. "G" to 55 + 00	Rt.	
44 + 00 to 46 + 00	Lt. & Rt.	Side Slopes
Northbound		
1080 + 00 to 1081 + 75	Lt.	
1083 + 50 to 1085 + 00	Lt.	
1080 + 00 to 81 + 50 Lt. Ramp "G"	Lt.	
1083 + 30	Lt.	
Southbound		
1081 + 00 to 82 + 00 Rt. Ramp "E"	Lt.	
1082 + 47	Lt.	
1084 + 65	Lt.	

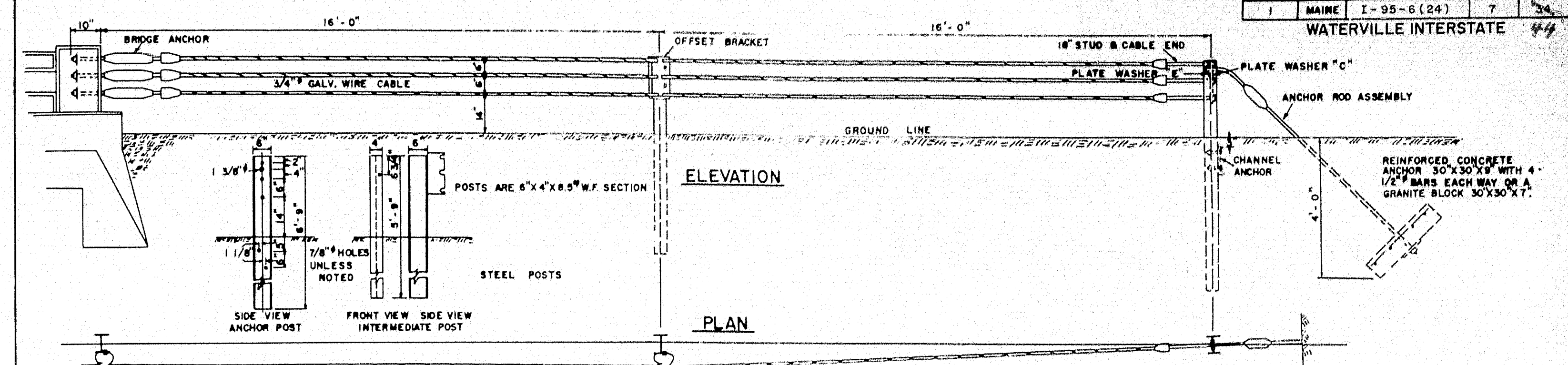
PORTABLE BARRICADES		
STATION	NO.	REMARKS
Main Street		
46 + 50± Rt.	2	With Flashing Lights
46 + 85± Lt.	2	
46 + 95± Rt.	1	
47 + 25± Lt.	1	
50 + 95± Rt.	1	
51 + 40± Rt.	2	
52 + 05 Lt.	1	
52 + 80 Lt.	2	With Flashing Lights

CATCH BASINS & MANHOLES (DRAIN.)			
STATION	SIDE	CATCH BASIN	H.I.
Main Street			
44 + 00	Rt.	F	
44 + 00	Lt.	F	
82 + 93	Rt.	F	
52 + 80	Lt.	H	

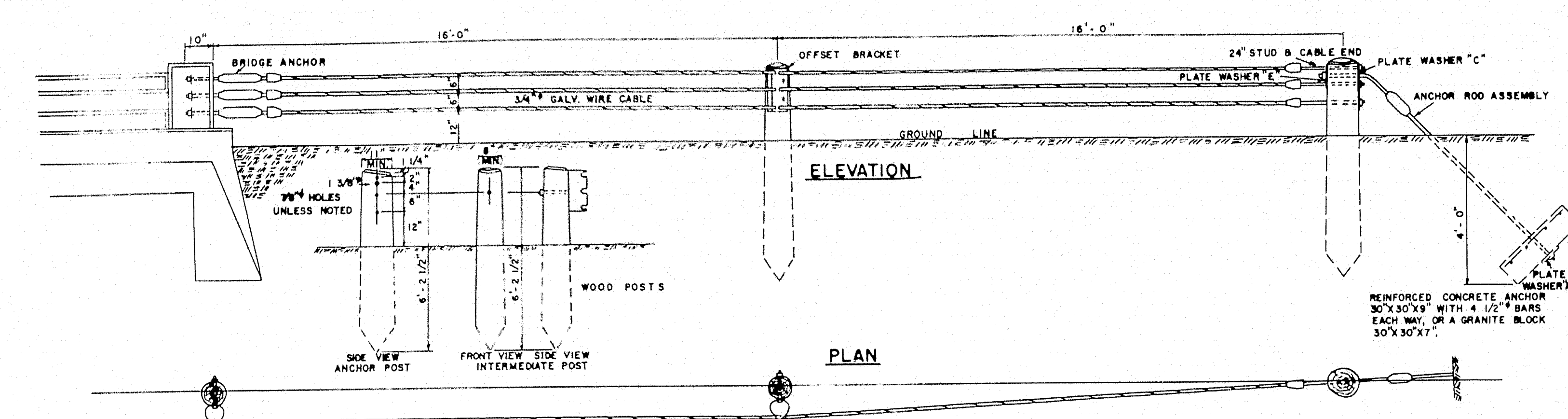
SUMMARY OF CLASSIFIED EXCAVATION AND BORROW	
Total Excavation from Cross Sections	11,502 C.Y.
Less Loam Excavation in Cuts	751
Sub-Total	10,751 C.Y.
Estimated Earth Shrinkage Factor (-15%)	1,613
Available Fill from Cross Sections	9,138 C.Y.
Total Earth Embankment from Cross Sections	21,976 C.Y.
Plus Loam Excavation in Fills	959
Total Fill Required	22,935 C.Y.
20% Granular Borrow	4,587
Embankment Sub-Total	18,348 C.Y.
Available Fill from Cross Sections	9,138
Common Borrow	9,210 C.Y.
Total Earth Excavation	10,751 + 219* = 11,000 C.Y.
Total Common Borrow	9,210 + 1,390* = 10,600 C.Y.
Total Granular Borrow	4,587 + 13* = 4,600 C.Y.



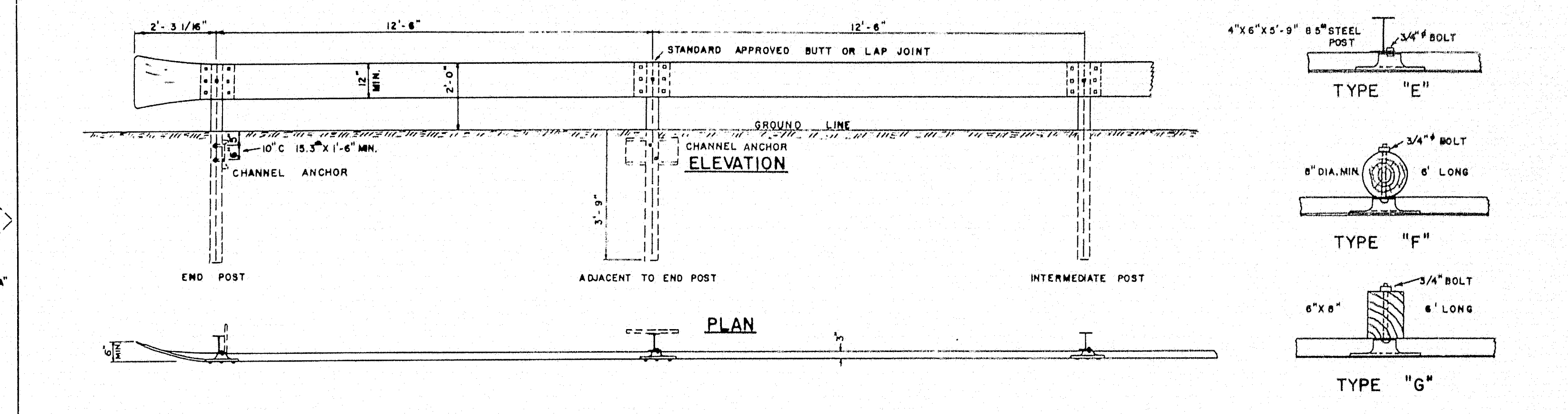
GUARD RAIL TYPE "A" & "B"



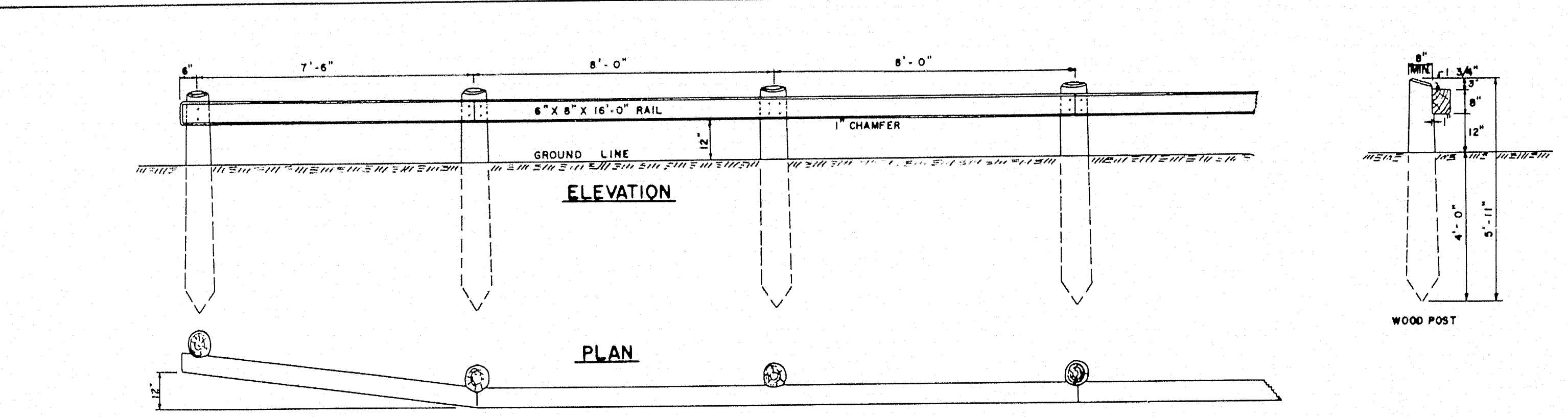
GUARD RAIL TYPE "C"



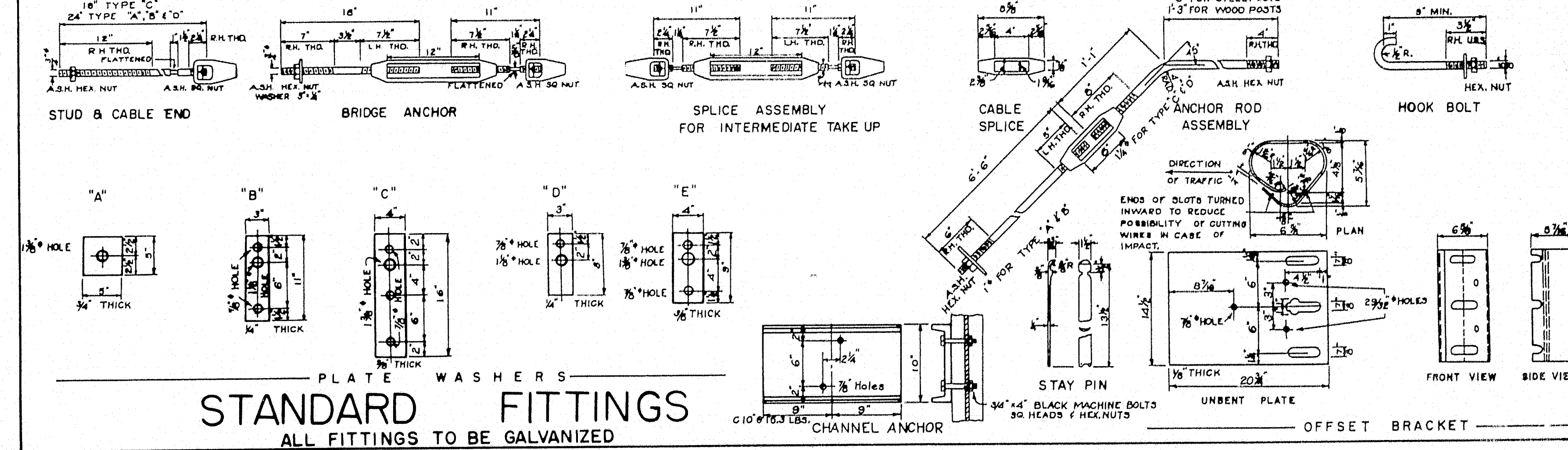
GUARD RAIL TYPE "D"



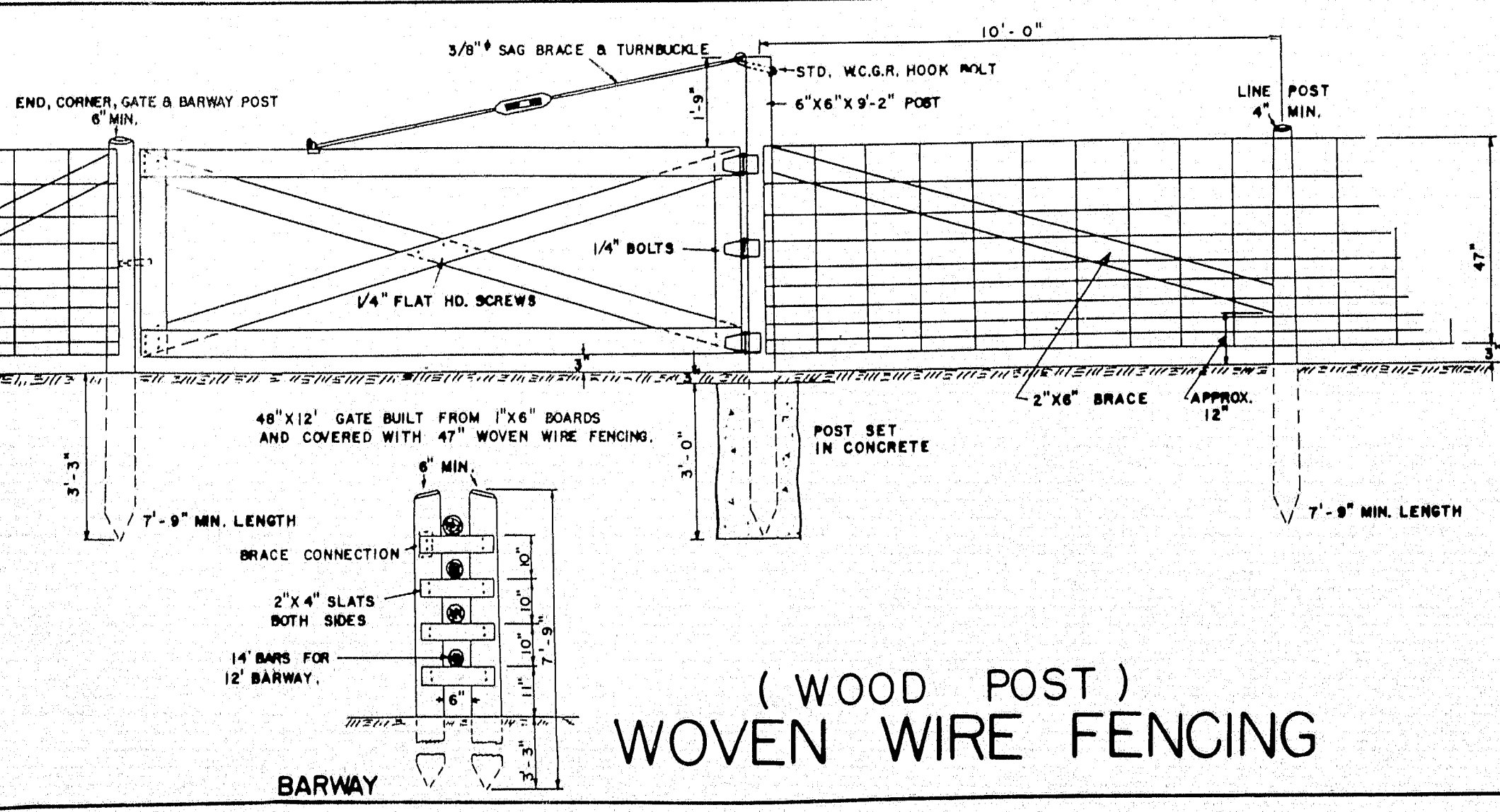
GUARD RAIL TYPE "E", "F" & "G"



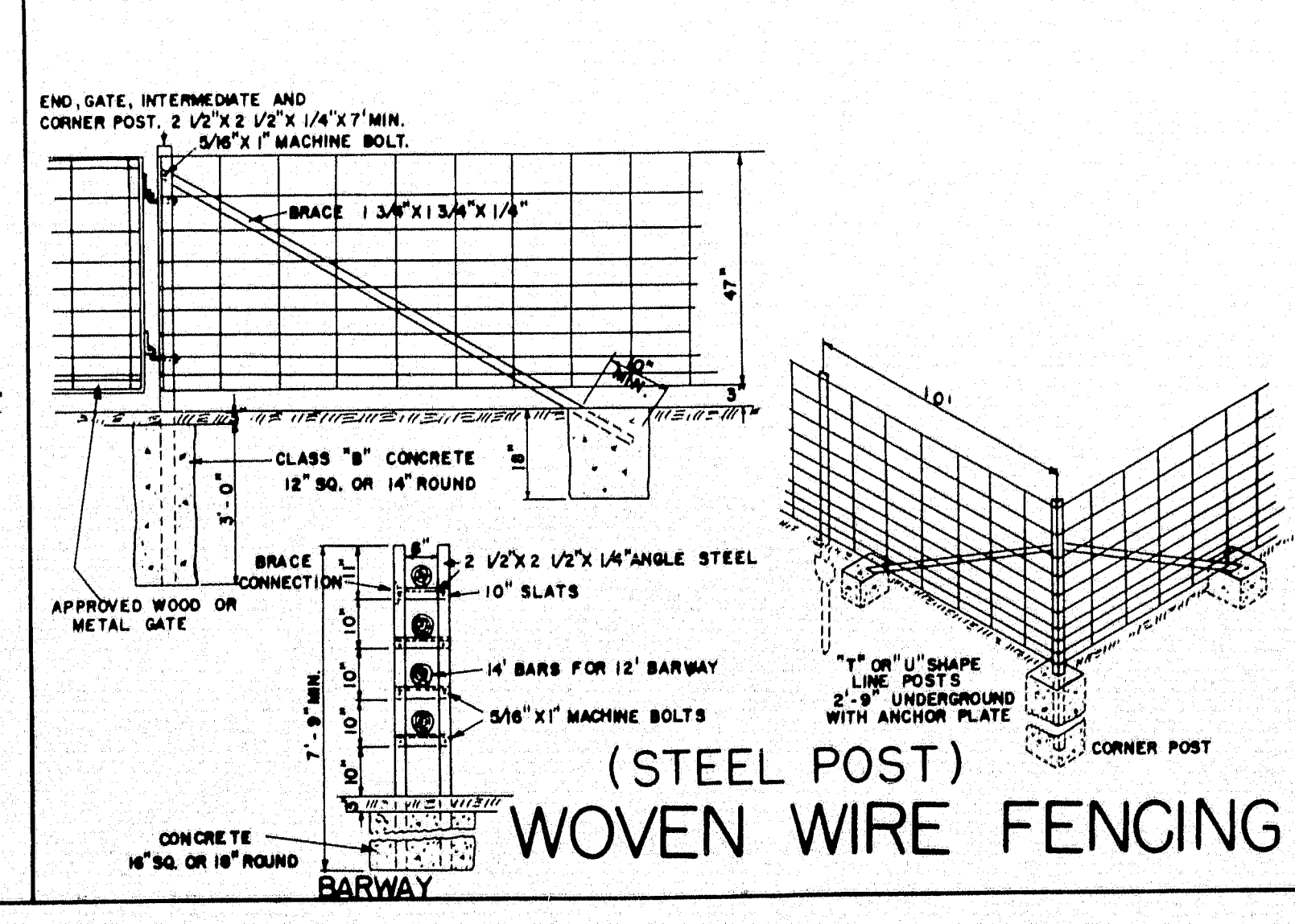
GUARD RAIL TYPE "H"



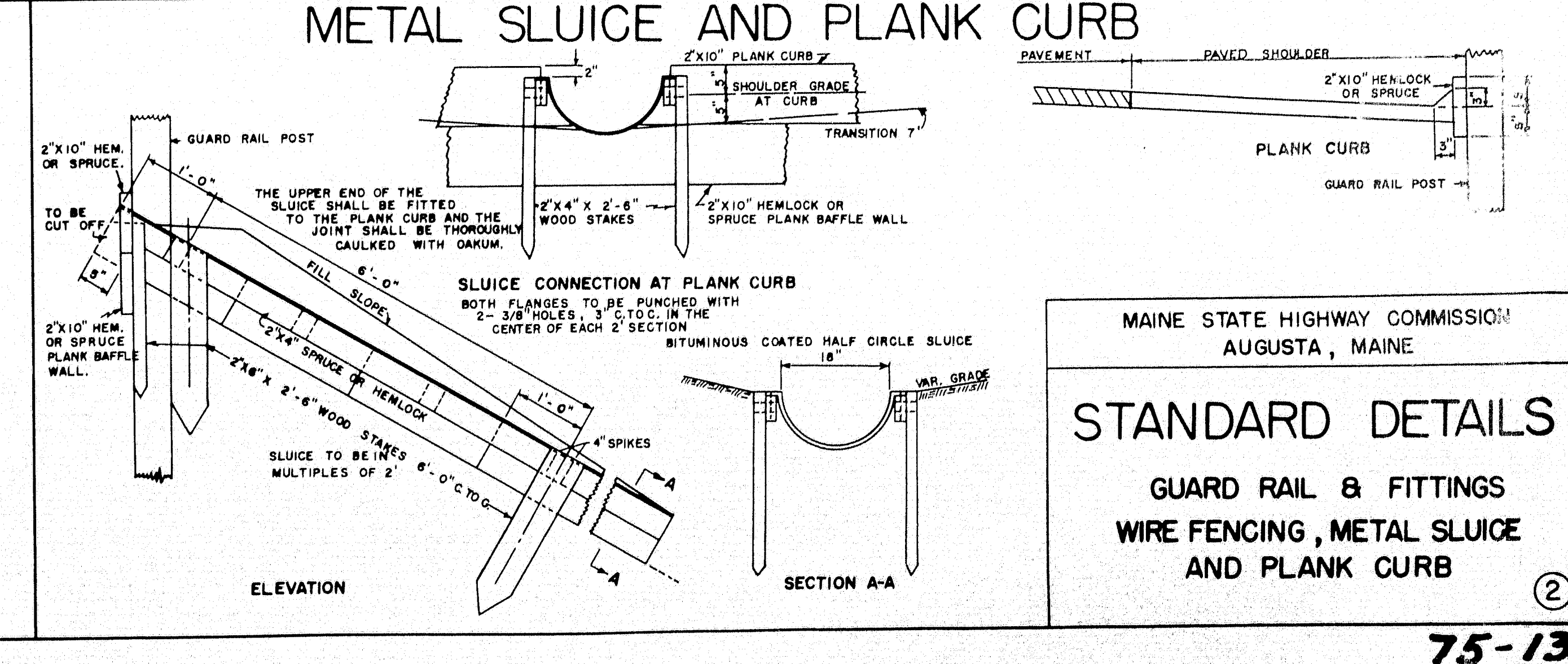
STANDARD FITTINGS
ALL FITTINGS TO BE GALVANIZED



(WOOD POST)
WOVEN WIRE FENCING



(STEEL POST)
WOVEN WIRE FENCING



METAL SLUICE AND PLANK CURB

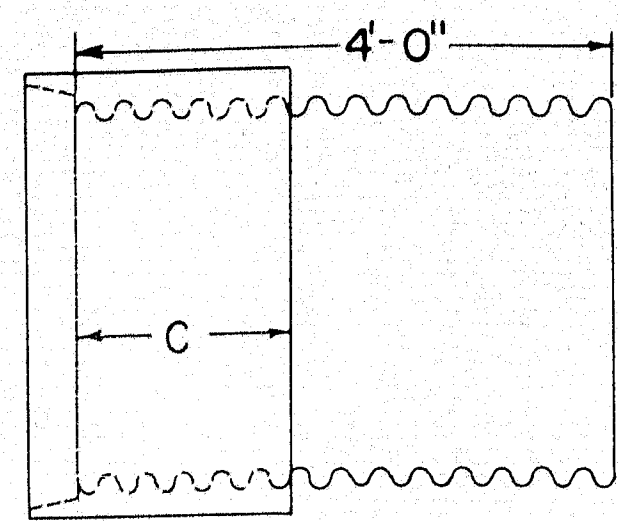
MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

STANDARD DETAILS

GUARD RAIL & FITTINGS
WIRE FENCING, METAL SLUICE
AND PLANK CURB

(2)

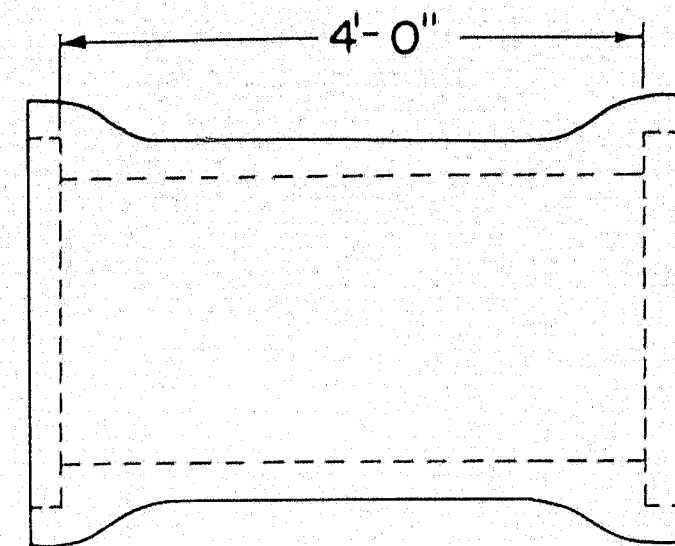
PIPE CONNECTIONS



GROOVE END COMBINATION
For 30" to 72" inclusive, diameter connection
between concrete and metal pipe

"C" = 17" min. for sizes 30" to 48" incl.
"C" = 23" min. for sizes over 48"

Asphalt coated corrugated metal pipe
shall conform to the latest
standard specifications

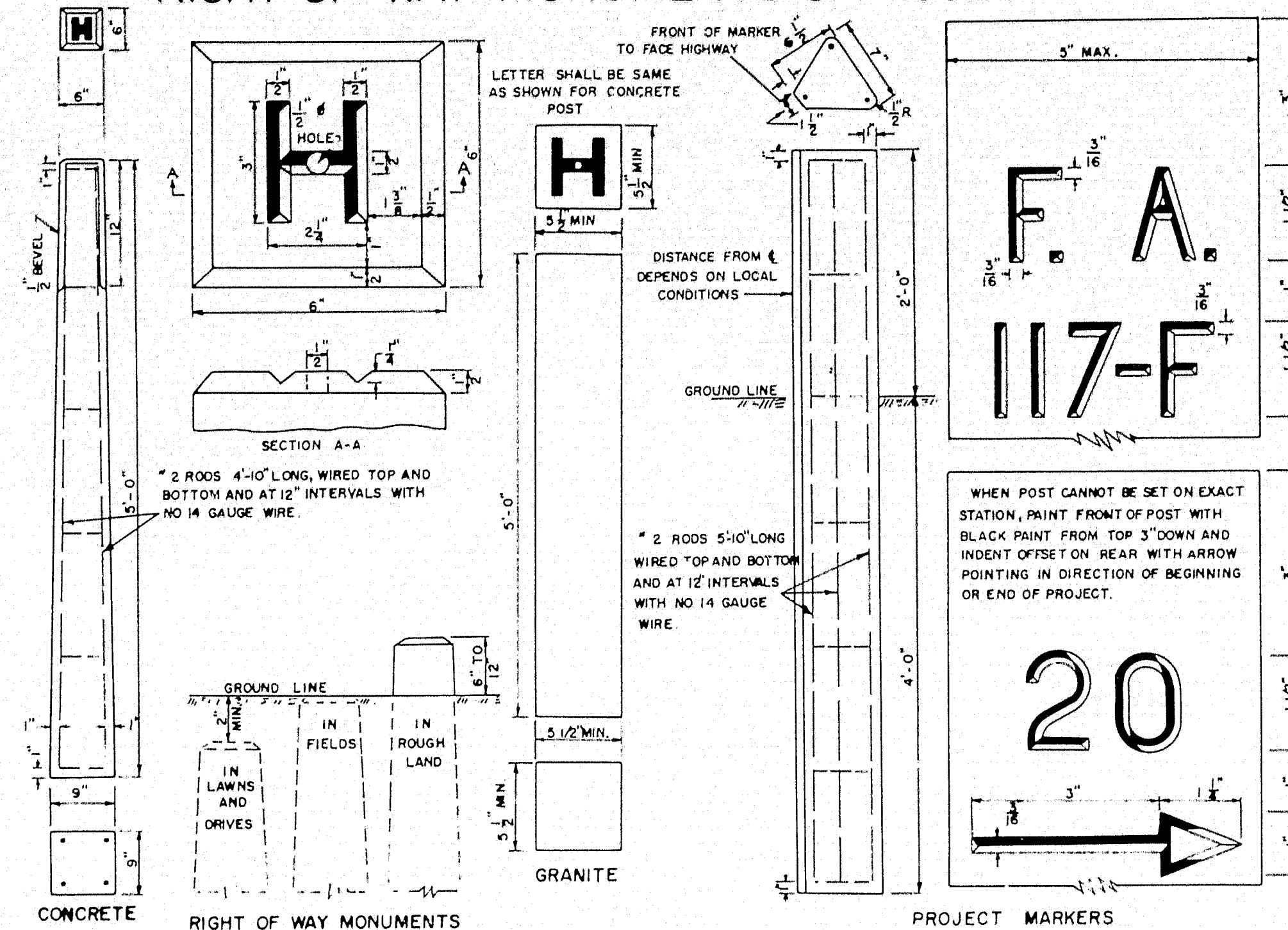


**REINFORCED CONCRETE PIPE CONNECTOR
DOUBLE BELL**

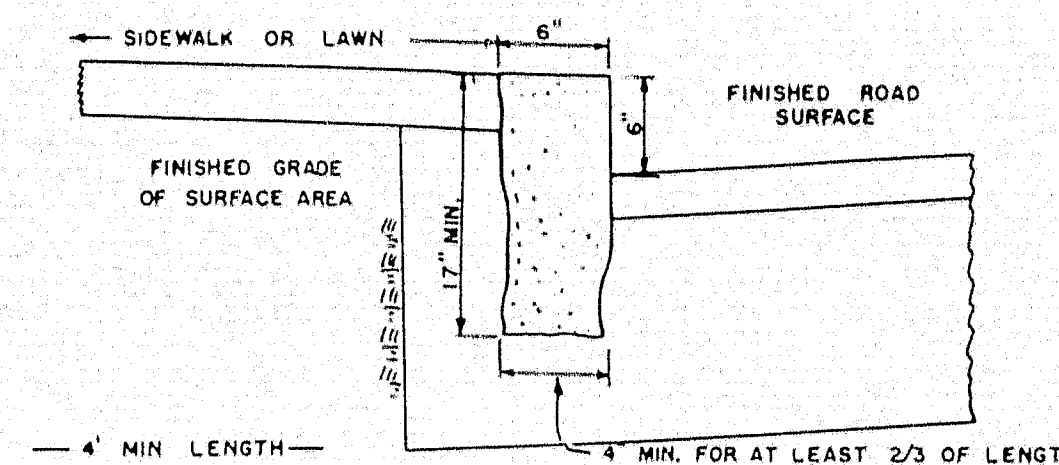
For 12" to 24" inclusive, diameter connection
between concrete and metal pipe

Reinforced concrete pipe shall
conform to the latest standard
specifications

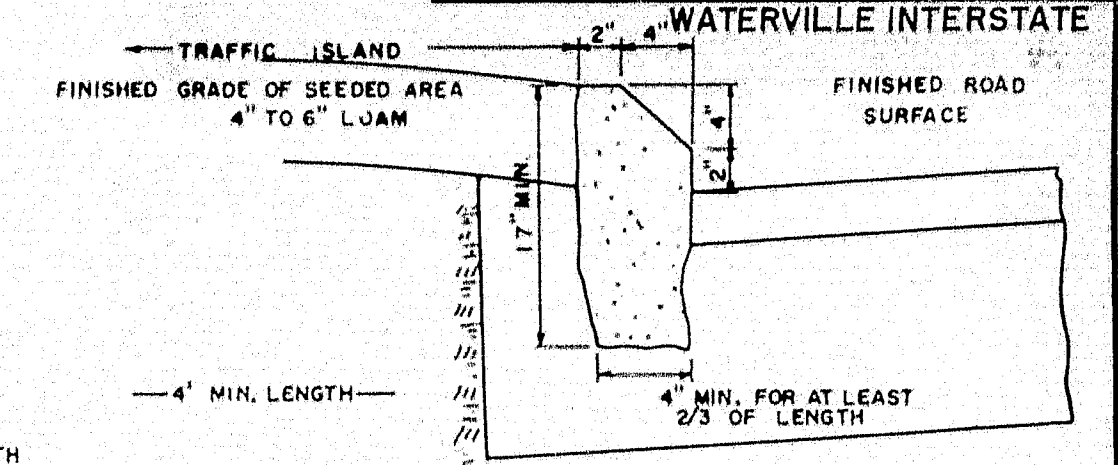
RIGHT OF WAY MONUMENTS & PROJECT MARKERS



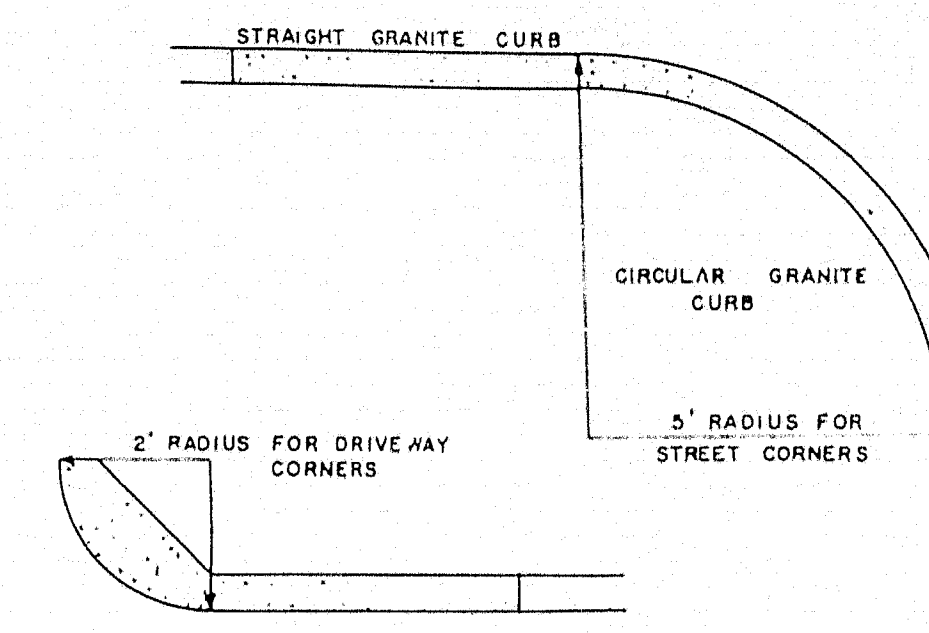
GRANITE CURB & EDGING



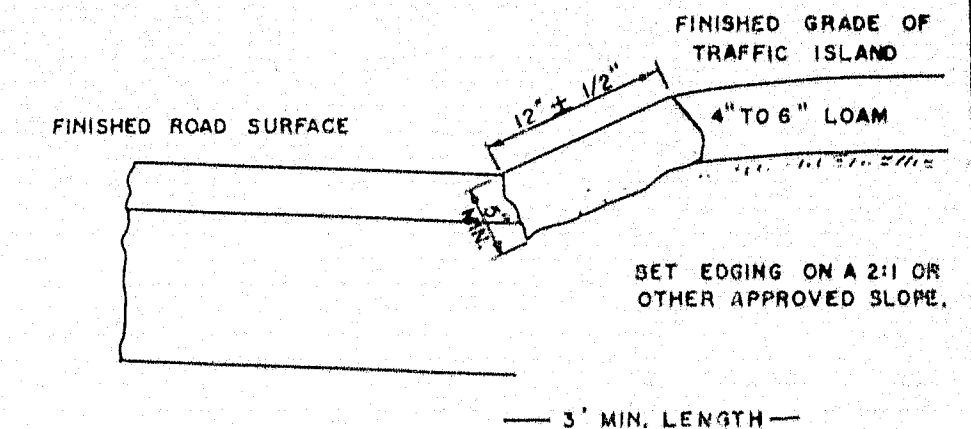
STRAIGHT GRANITE CURB



SLOPED GRANITE CURB

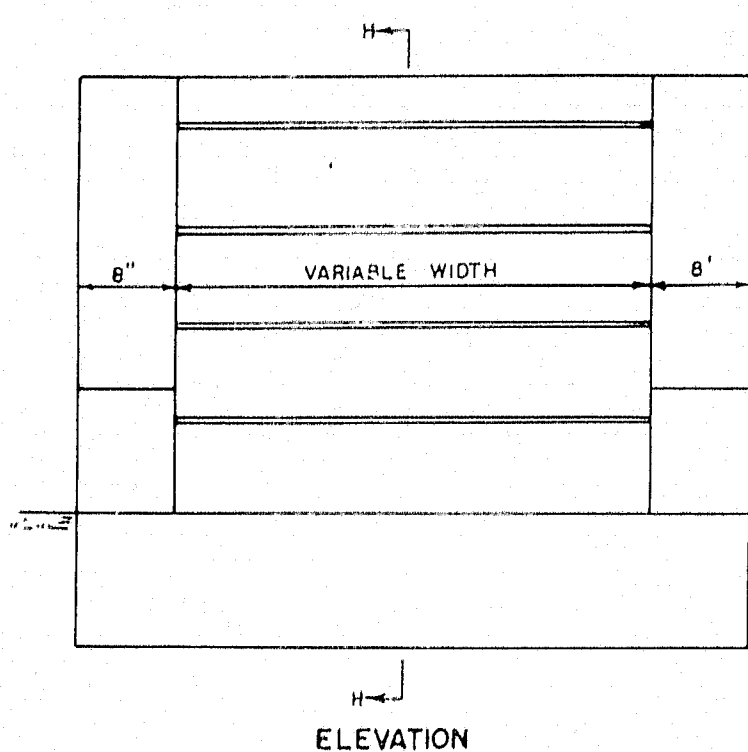


CIRCULAR GRANITE CURB

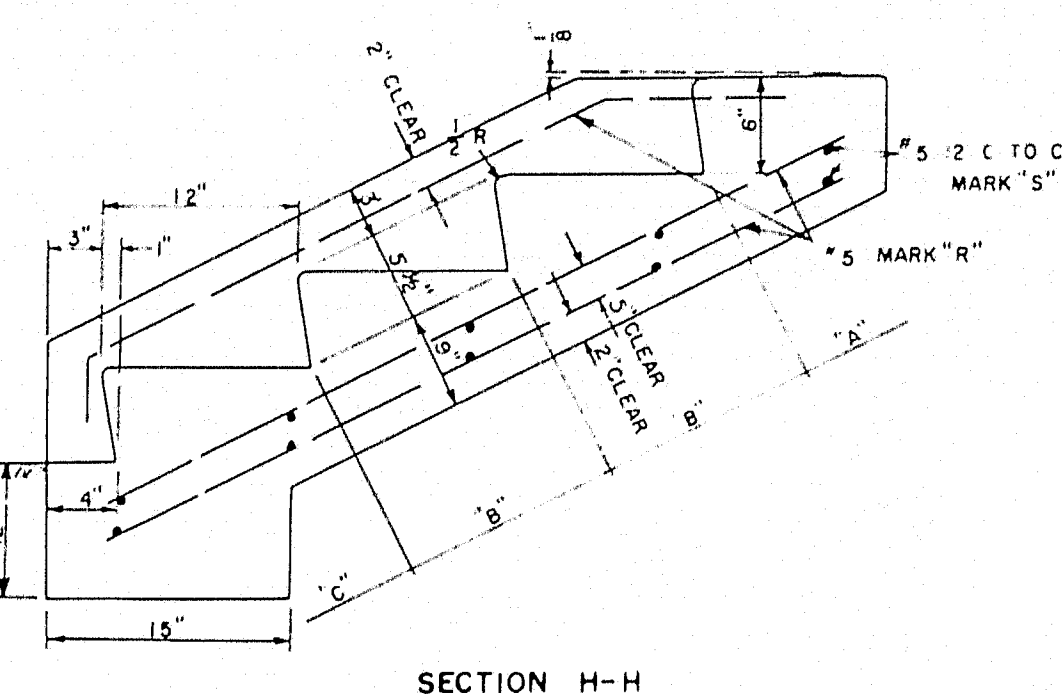


GRANITE EDGING

CONCRETE STEPS



ELEVATION

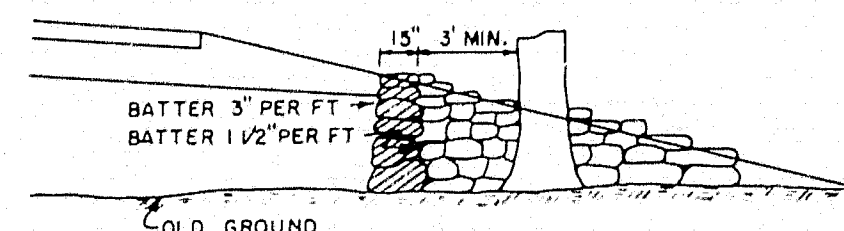


SECTION H-H

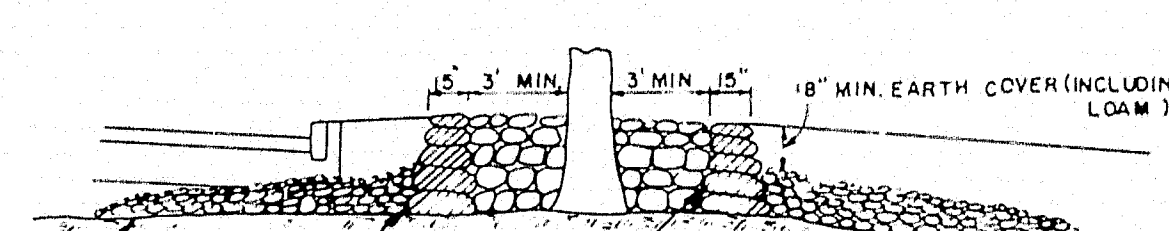
CONCRETE CLASS "A"			
SECTION	STEPS PER FT. OF WIDTH	PARAPET EACH WALL	
"A" HEADER	0.32 CU. YDS.	0.22 CU. YDS.	
"B" EA. INTER ST.	0.40 CU. YDS.	0.40 CU. YDS.	
"C" FOOTER	0.71 CU. YDS.	0.65 CU. YDS.	

REINFORCING STEEL			
MARK	SIZE	NUMBER	LENGTH (EACH)
R	#5	3 EACH PARAPET	8" FOR "A"
	1043 LBS PER FT.	2 EACH FT. OF WIDTH	+13" FOR EACH "B"
			+16" FOR "C"
S	#5	2 FOR "A"	6 EACH PARAPET
	1043 LBS PER FT.	2 FOR EACH "B"	+12" PER FT. OF WIDTH
		4 FOR "C"	

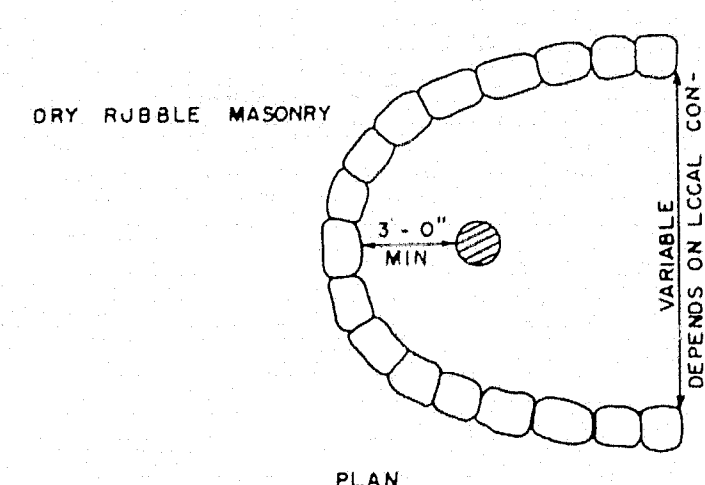
TREE WELLS



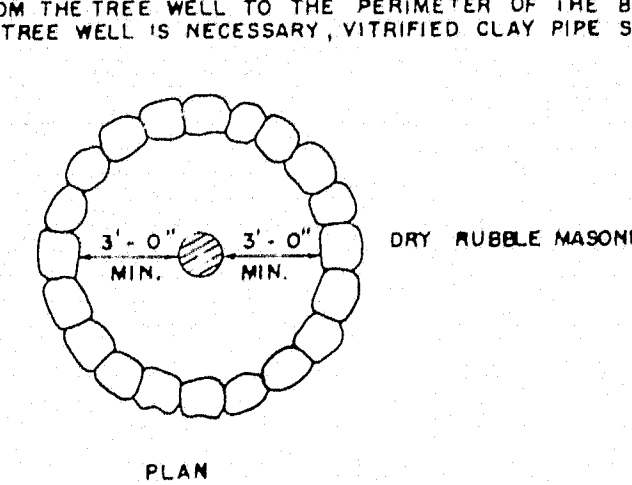
OLD GROUND



OLD GROUND

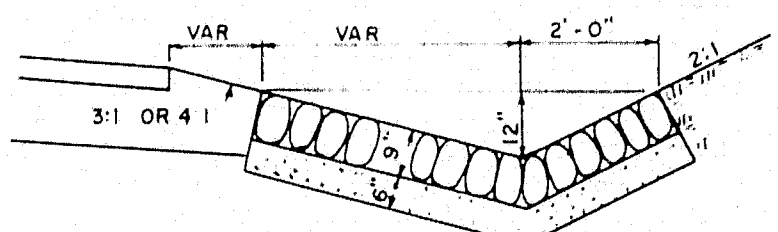


PLAN

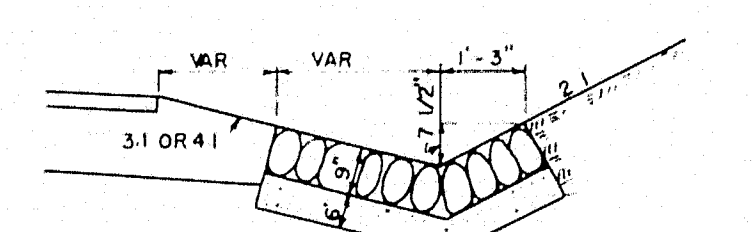


PLAN

STONE GUTTER

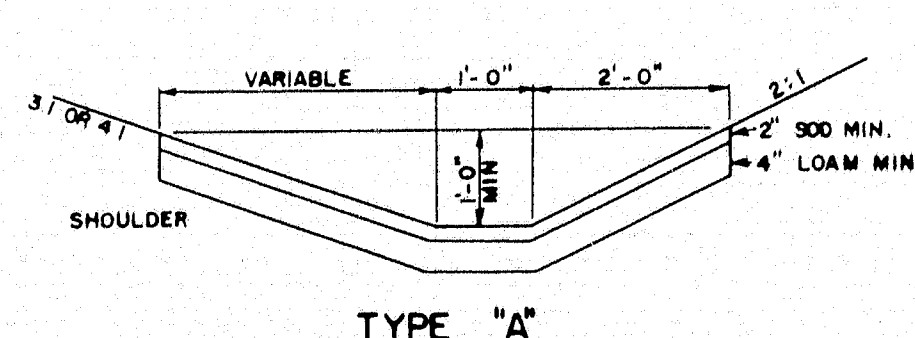


GRAVEL BED FOR GROUTED STONE GUTTER ONLY.

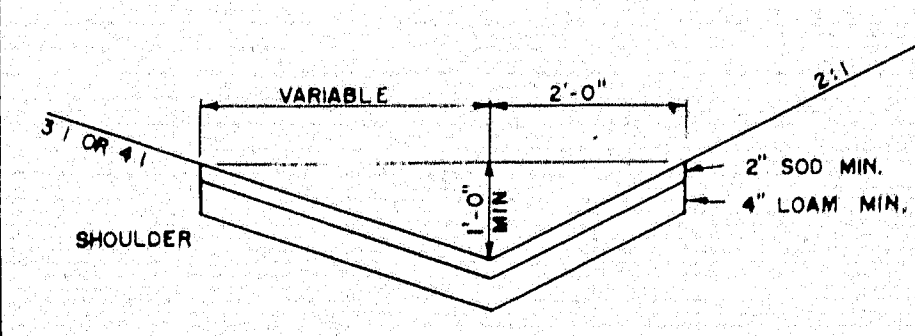


GRAVEL BED FOR GROUTED STONE GUTTER ONLY.

SODDED GUTTER

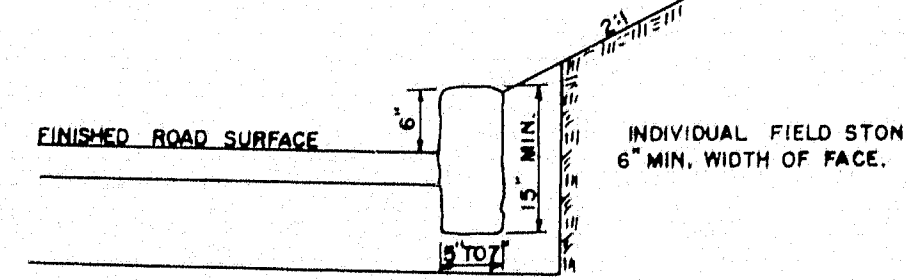


TYPE "A"



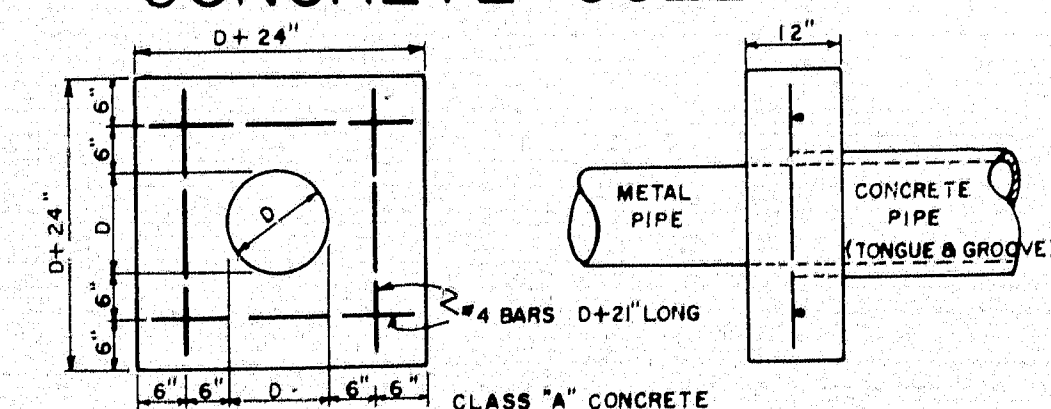
TYPE "B"

FIELD STONE CURB



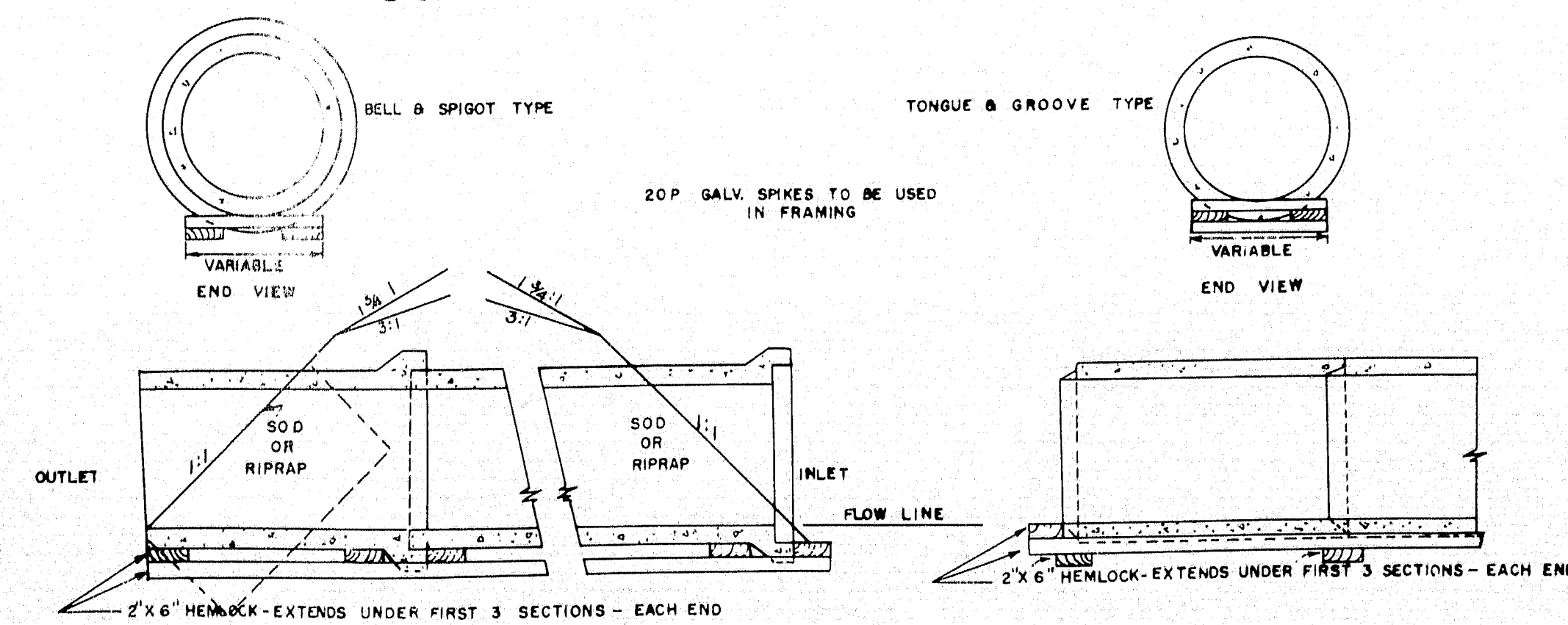
INDIVIDUAL FIELD STONE
6" MIN. WIDTH OF FACE.

CONCRETE COLLAR



CLASS "A" CONCRETE

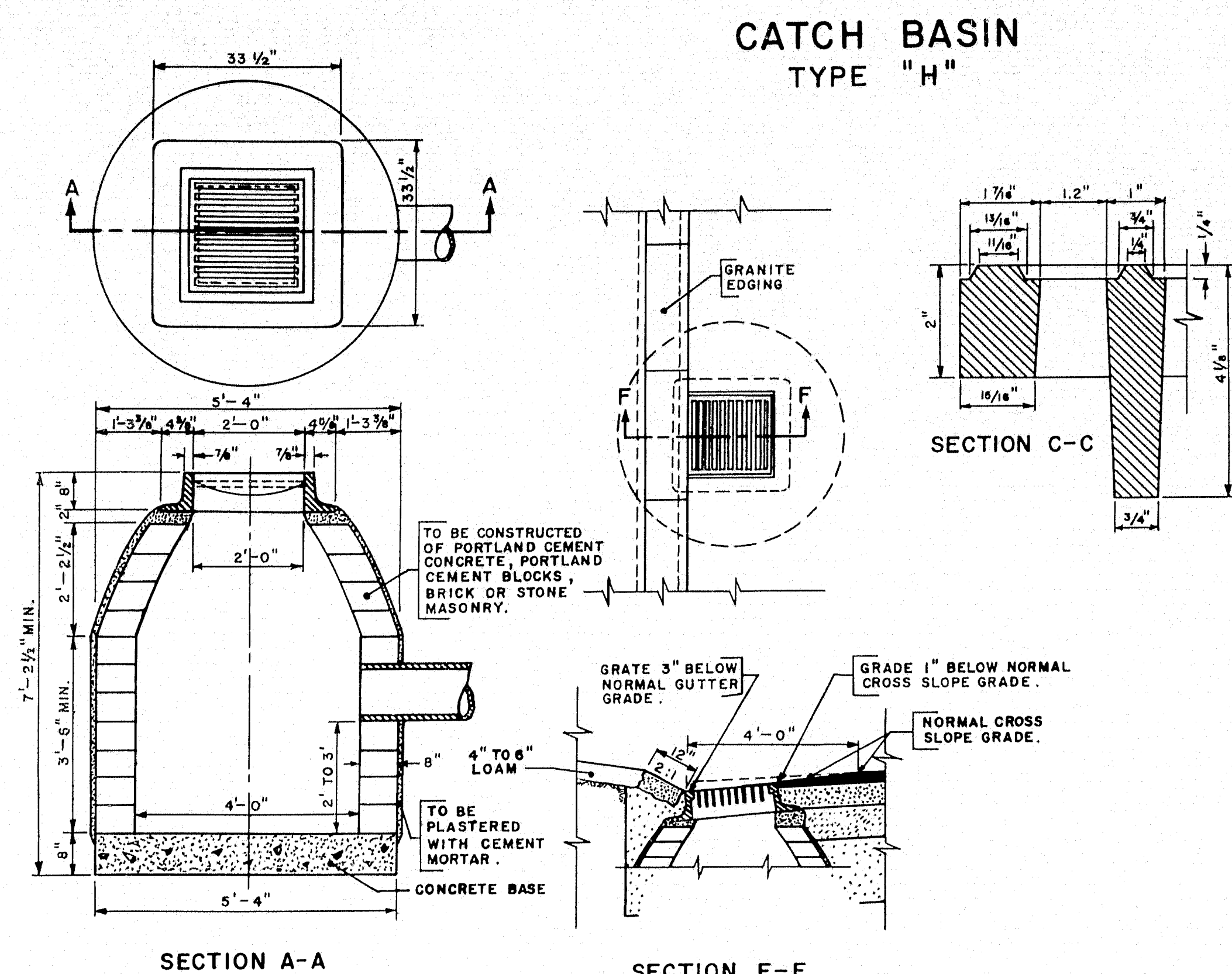
CONCRETE PIPE CRADLE



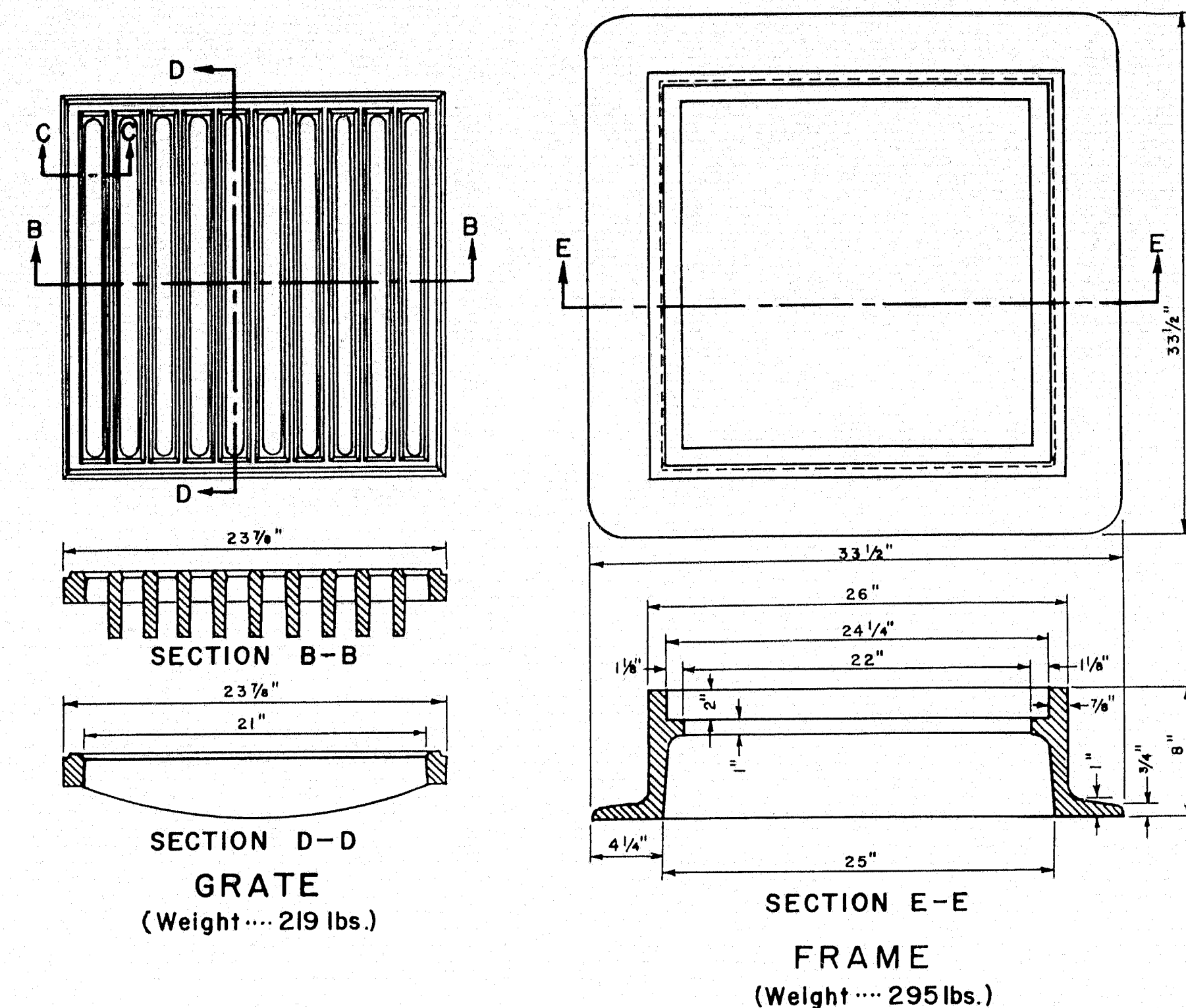
MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

STANDARD DETAILS

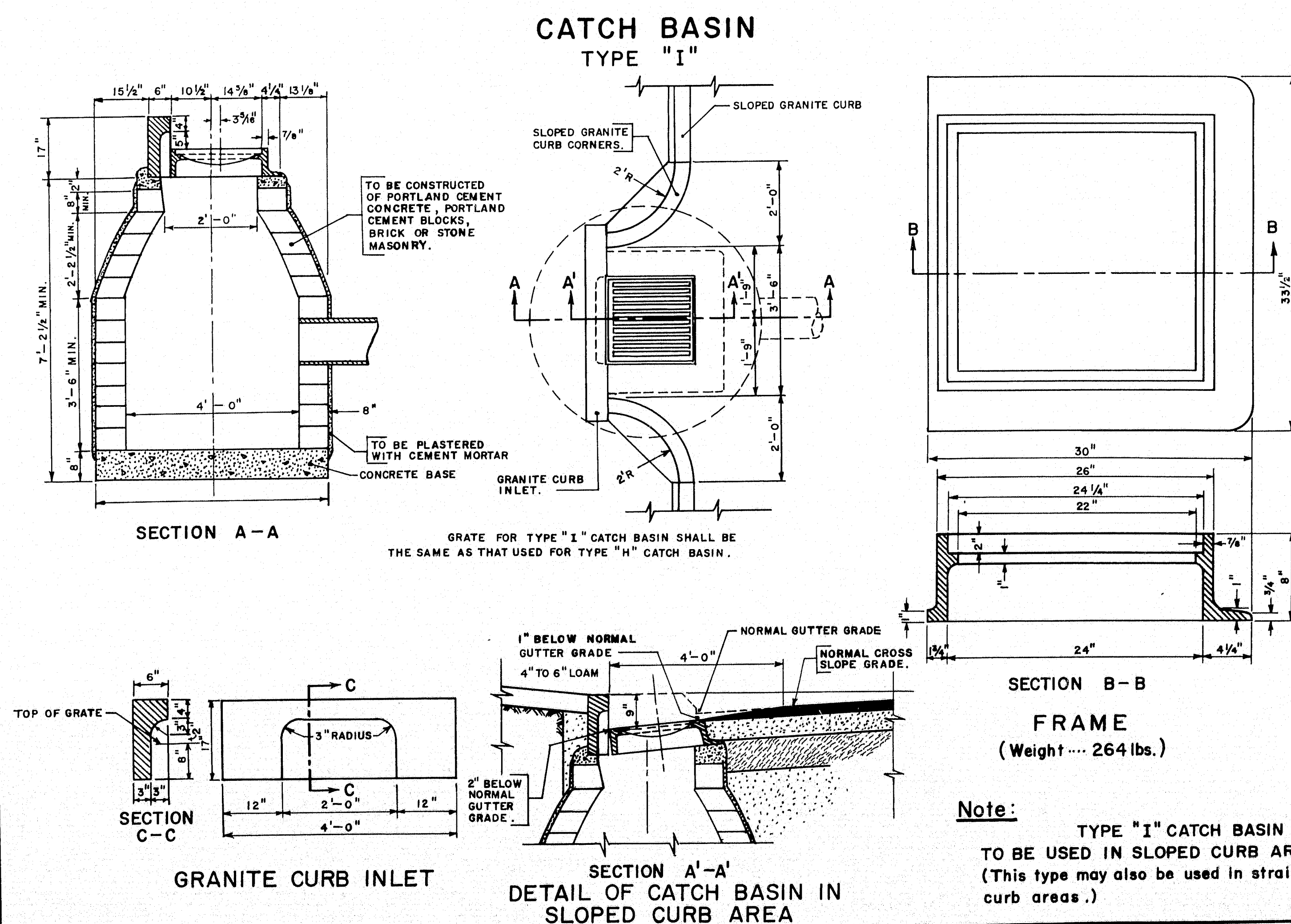
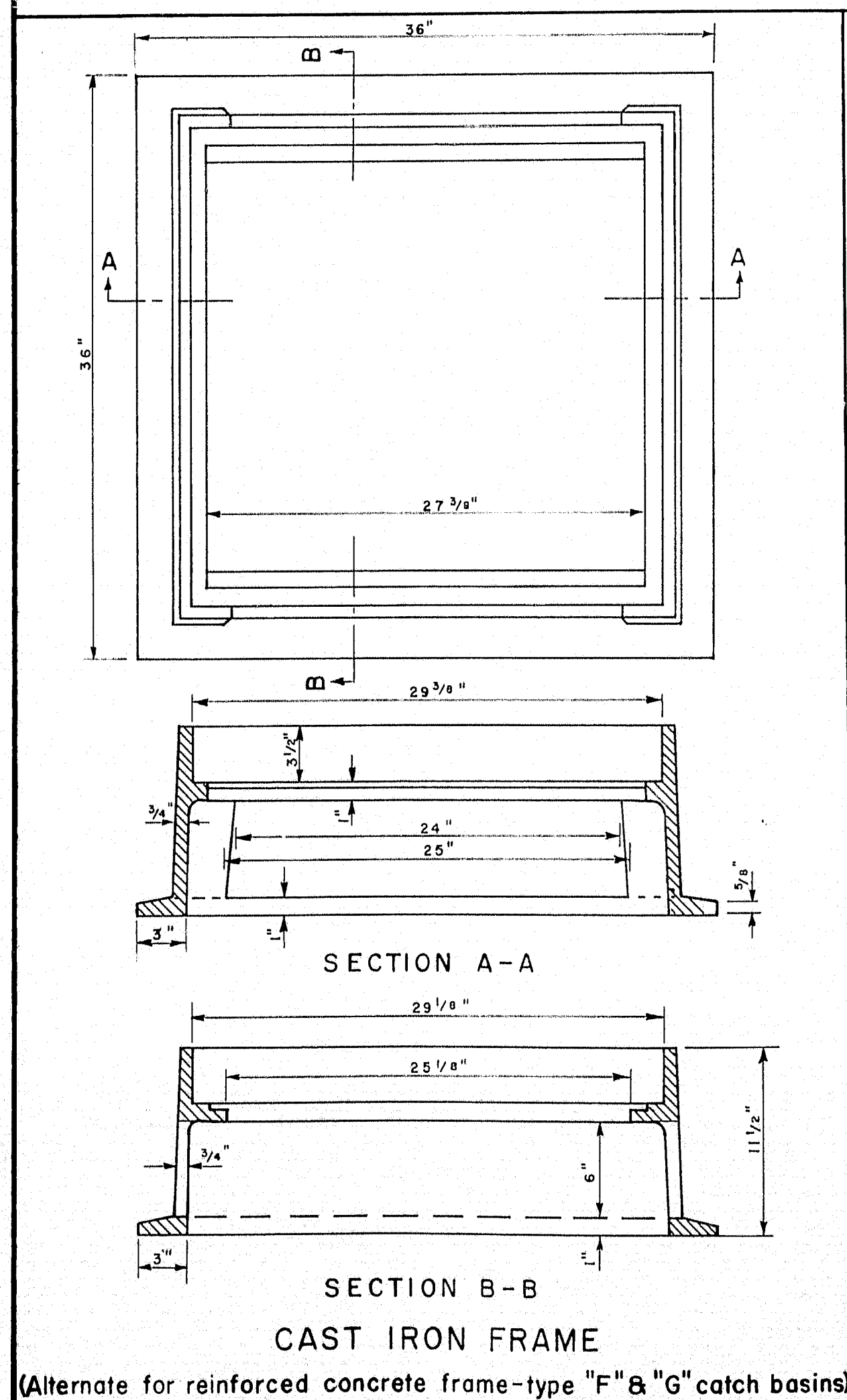
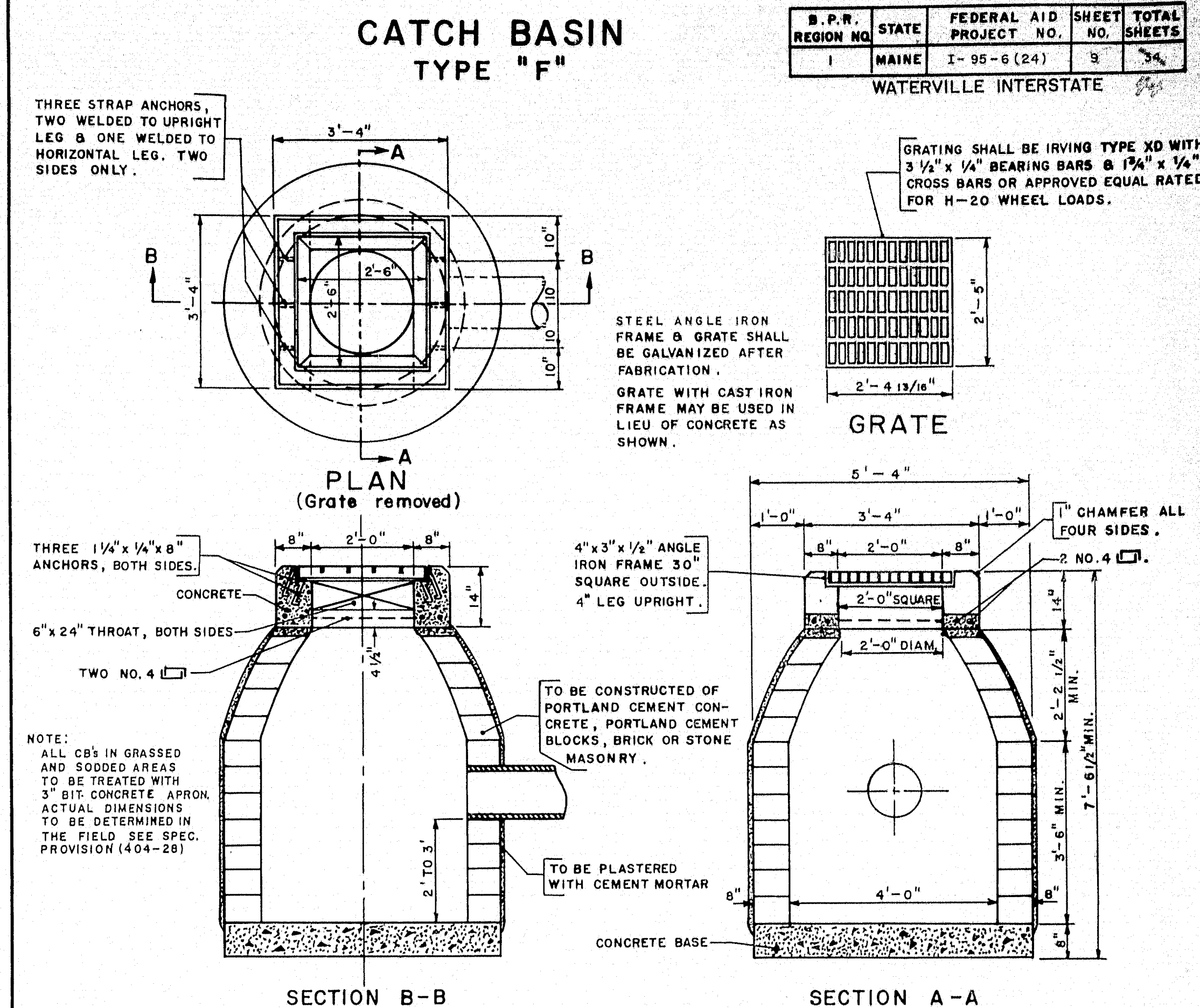
MISCELLANEOUS ITEMS



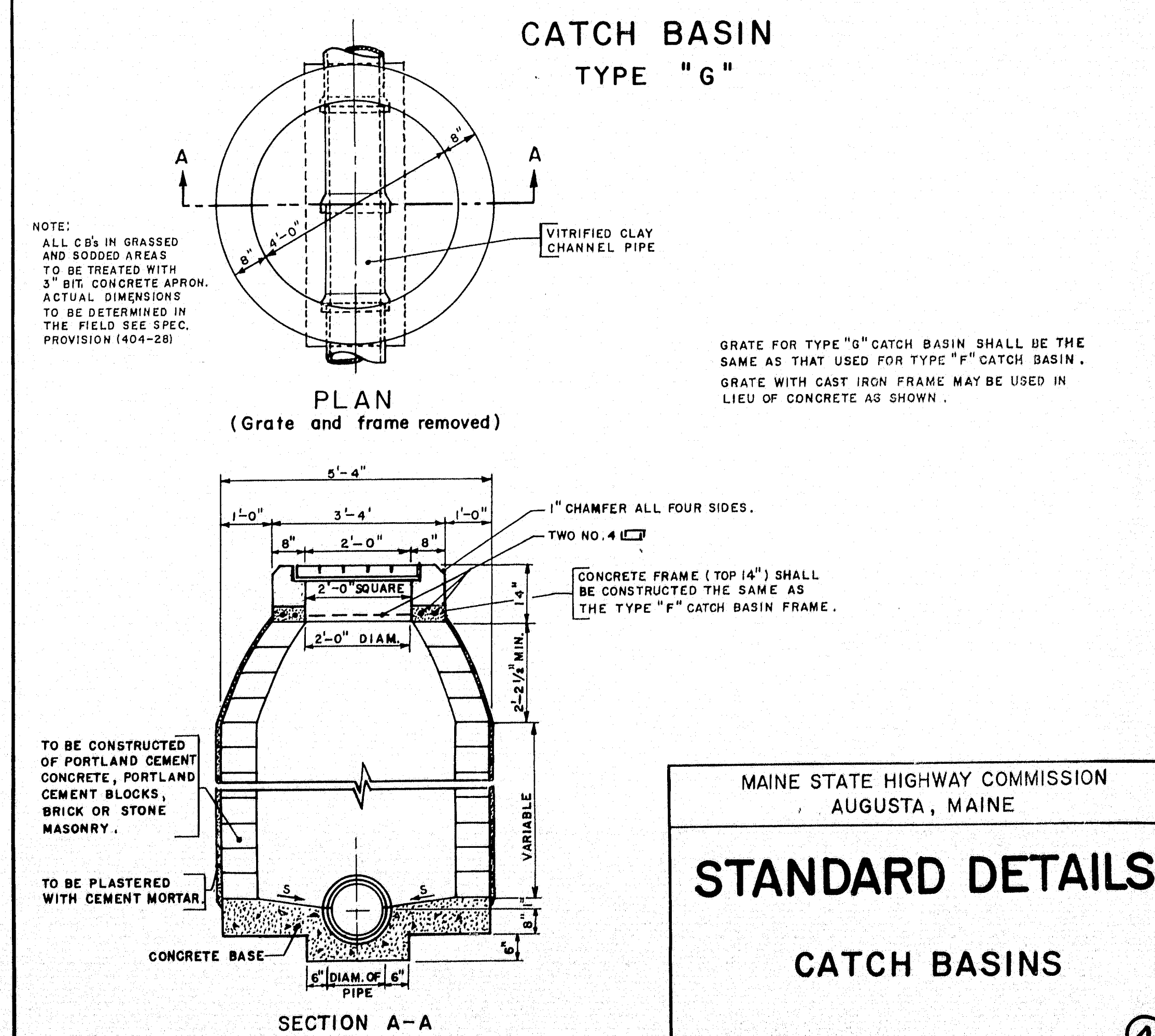
DETAIL OF CATCH BASIN
IN GRANITE EDGING AREA



Note: TYPE "H" CATCH BASIN IS TO BE USED IN GRANITE EDGING AREA. (When this type of catch basin is used with sloped or straight curb, the curb on gutter line shall be cut to fit flange.)



Note: TYPE "I" CATCH BASIN IS TO BE USED IN SLOPED CURB AREA. (This type may also be used in straight curb areas.)



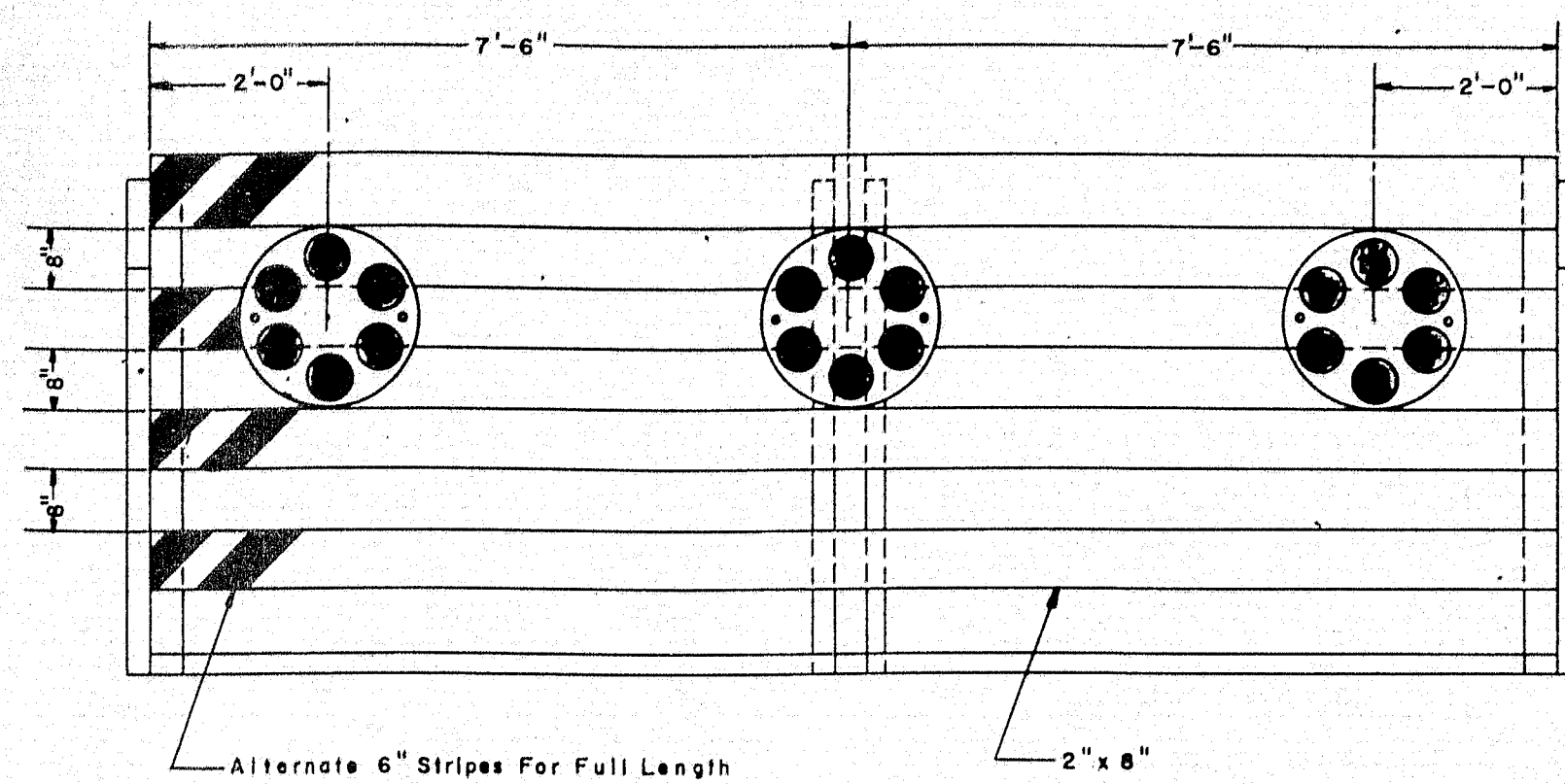
MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

STANDARD DETAILS

CATCH BASINS

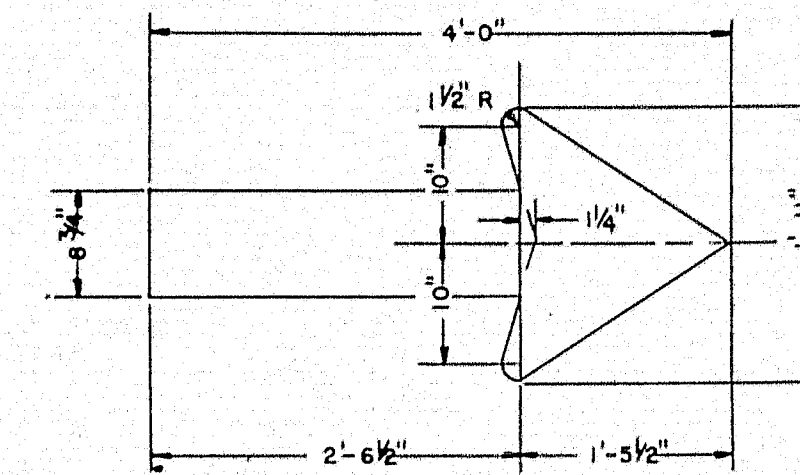
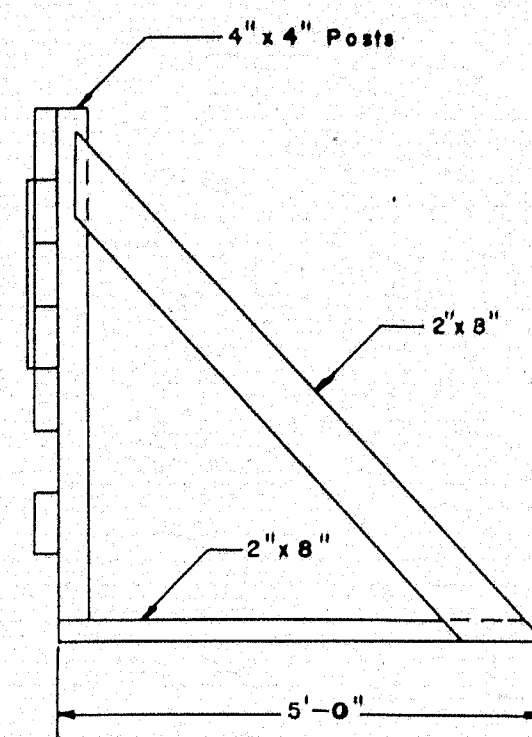
S.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-6(24)	11	34

WATERVILLE INTERSTATE

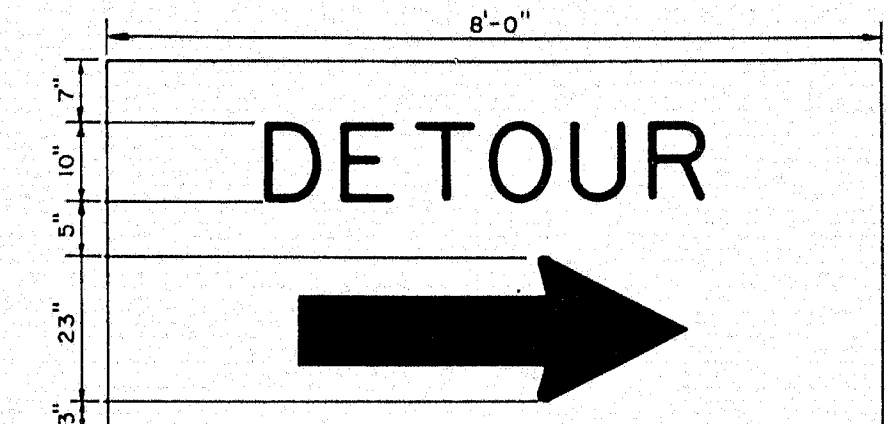


PORTABLE BARRICADE

Scale: $\frac{1}{2}" = 1'$



Scale: $\frac{3}{4}" = 1'$

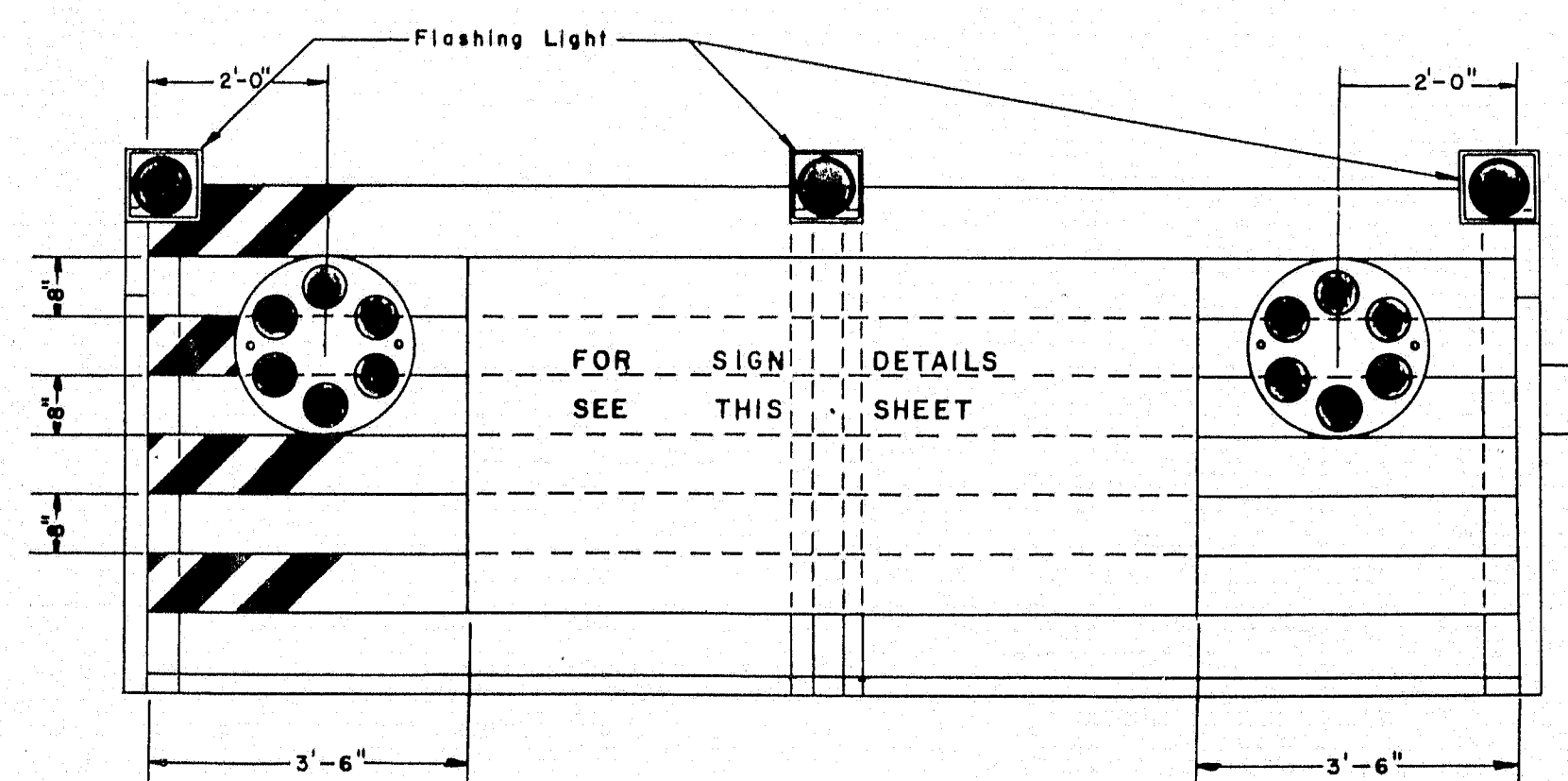


DETOUR SIGN FOR PORTABLE BARRICADE

Scale: $\frac{1}{2}" = 1'$

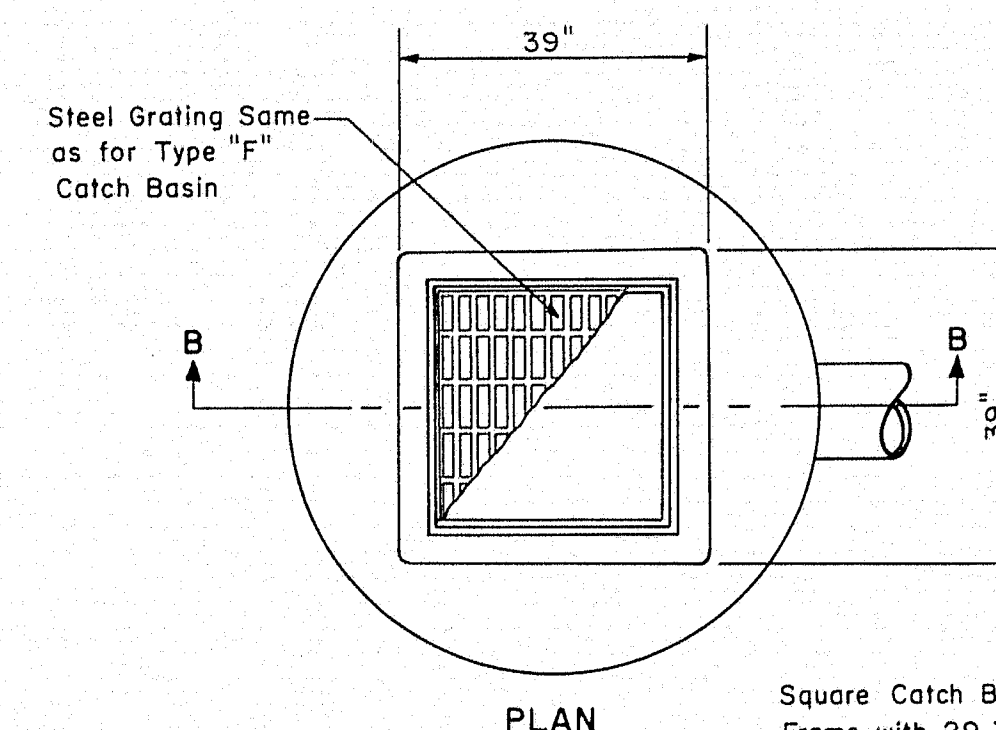
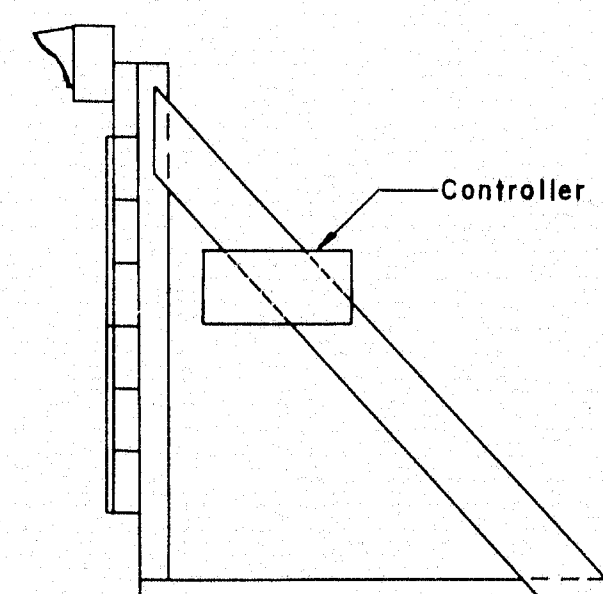
GENERAL NOTES:

1. Lumber sizes for use on Barricades shall be 2" x 8" except for Posts which shall be 4" x 4".
2. Detour Sign shall be $\frac{5}{8}$ " thick plywood.
3. Alternate 6" stripes shall be painted or screened in black on a background of silver reflective sheeting.
4. The word "DETOUR" and Arrow shall be painted or screened in black on a background of yellow reflective sheeting.
5. Flashing Lights shall be Mounted to permit rotation to face oncoming traffic.
6. Reflector clusters shall consist of a red background with red Reflectors similar to AGA Designation #1816-A1 (PDON).
7. The Barricades shall be securely anchored in place by means of sandbags, weights, or large stones.
8. Location of Service and Meter to be determined after Power Source has been decided.



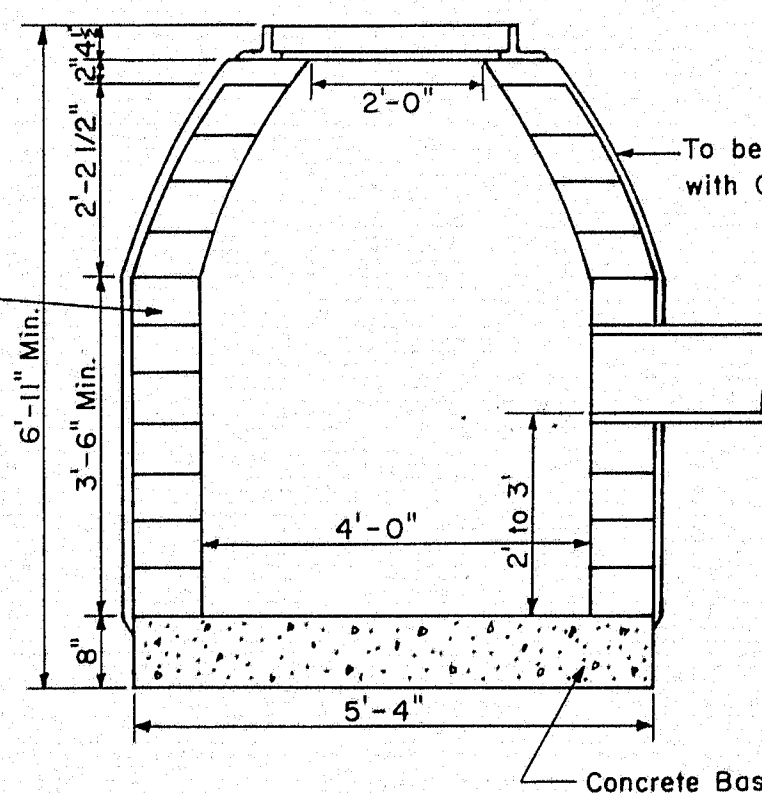
PORTABLE BARRICADE WITH FLASHING LIGHTS AND DETOUR SIGN

Scale: $\frac{1}{2}" = 1'$



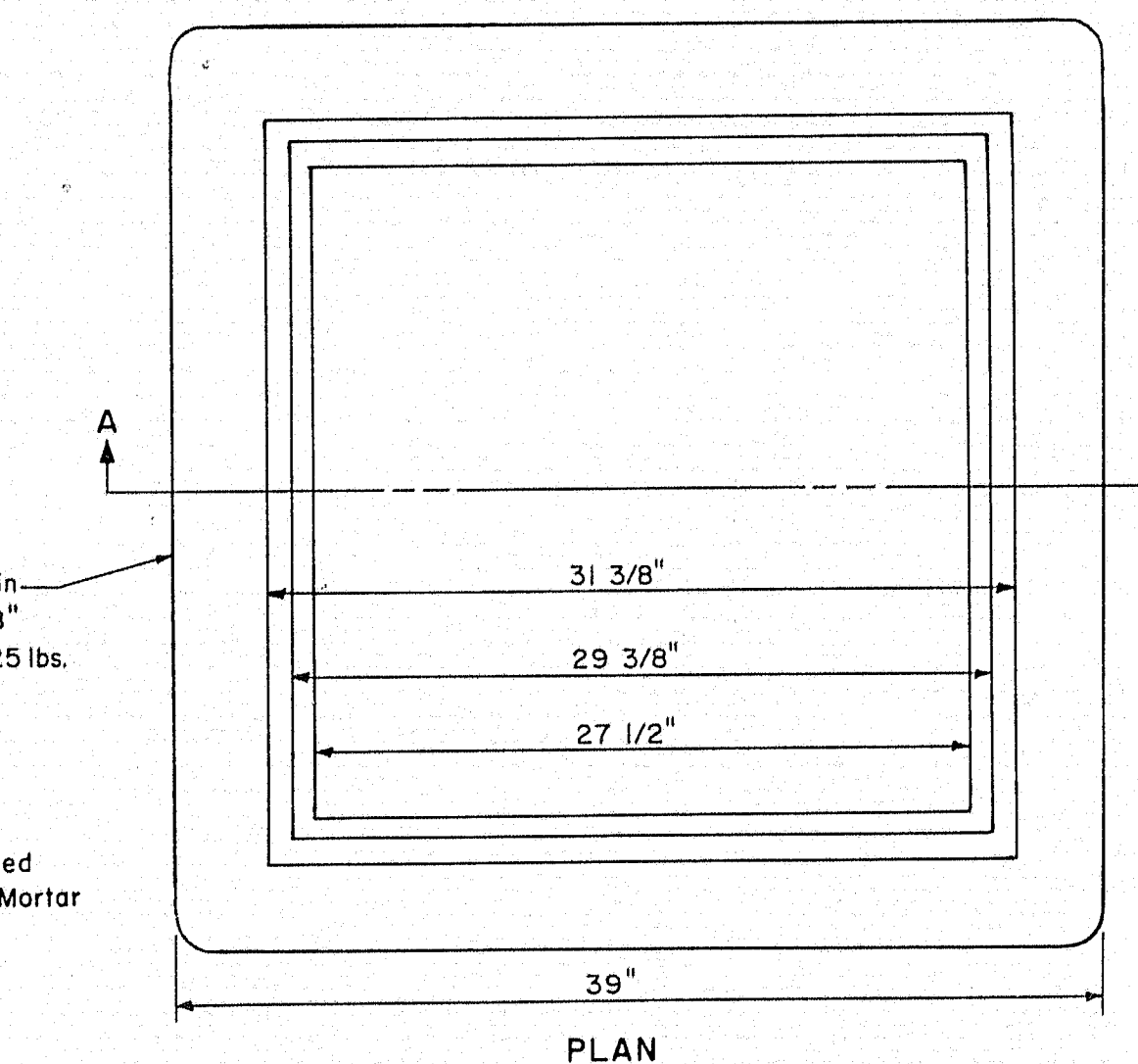
CATCH BASIN TYPE J

To be Constructed of Portland Cement Concrete, Portland Cement Blocks, Brick or Stone Masonry

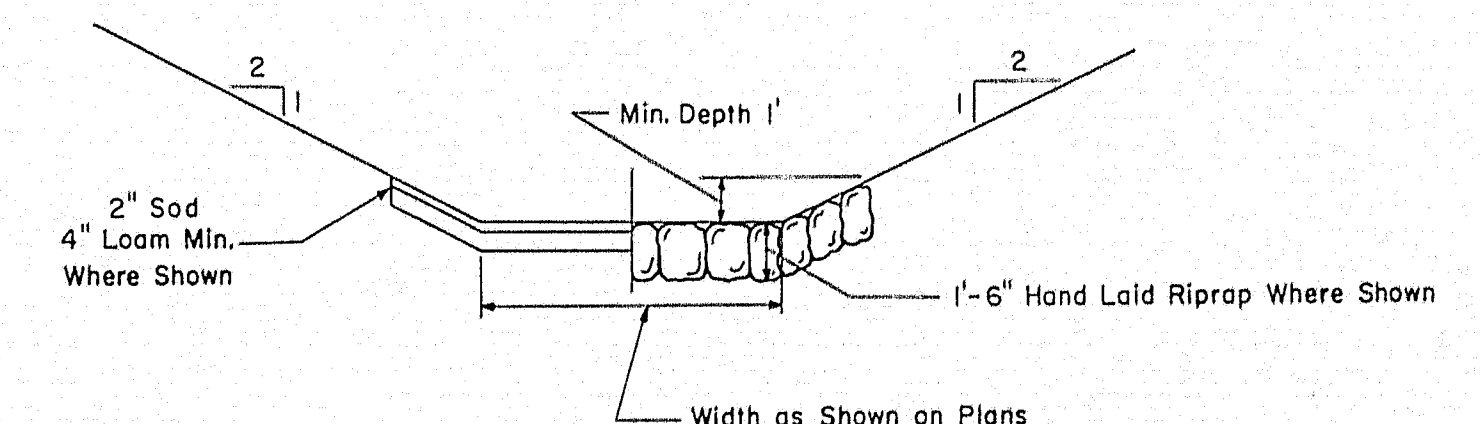


SECTION B-B Scale: $\frac{1}{2}" = 1'-0"$

CATCH BASIN TYPE J



SECTION A-A Scale: $\frac{1}{2}" = 1'-0"$



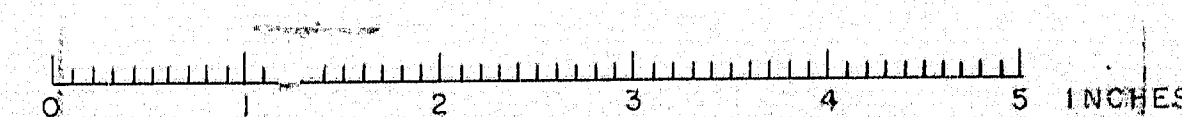
TYPICAL DITCH Not to Scale

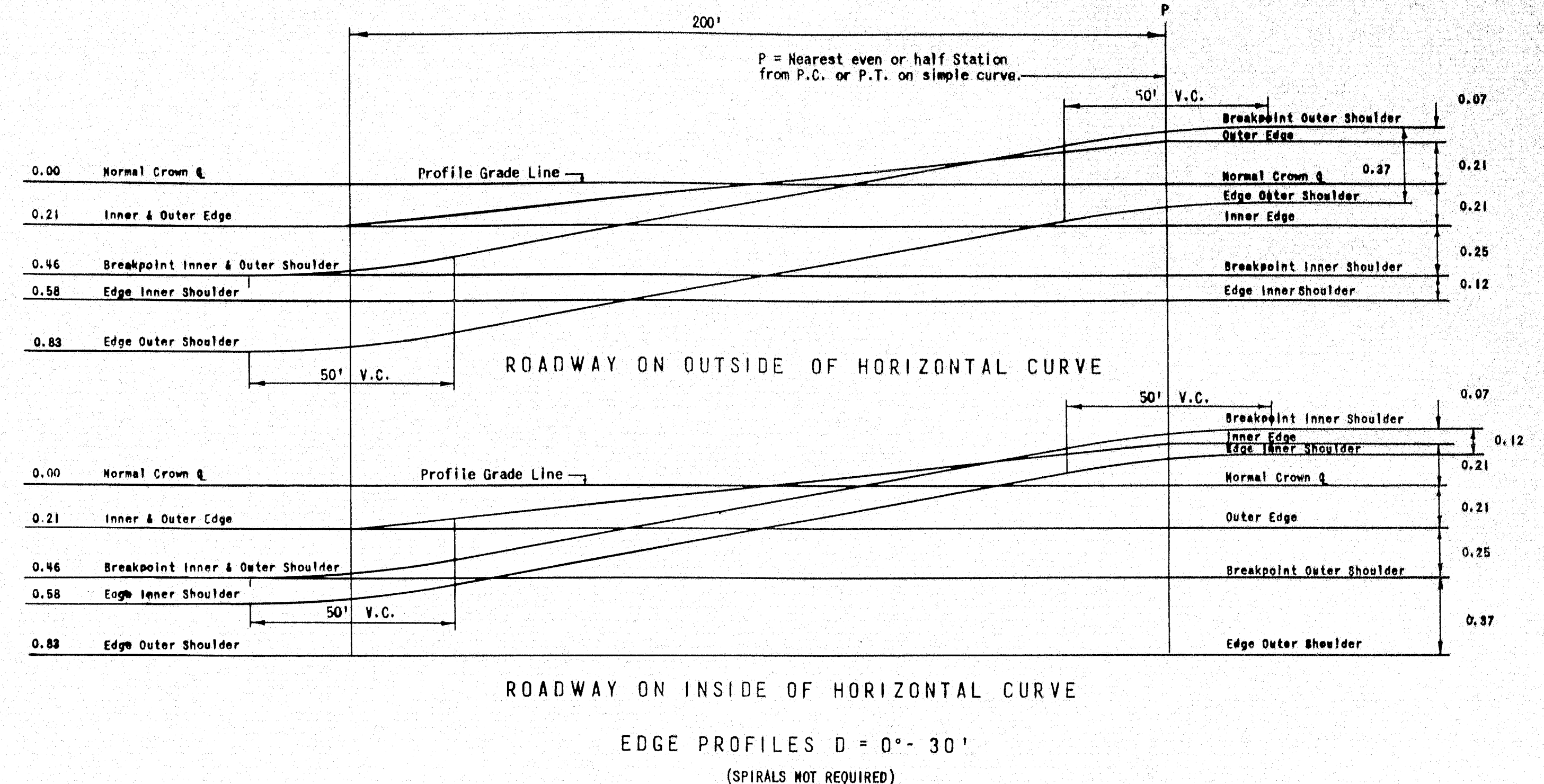
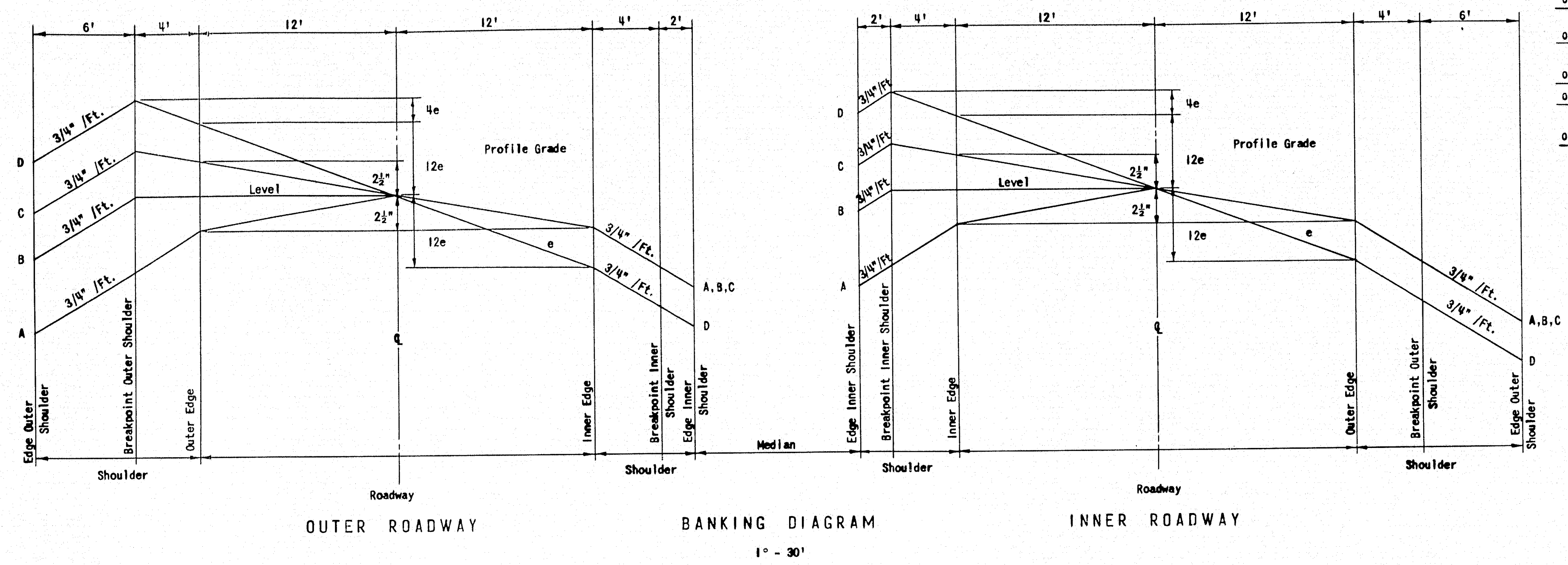
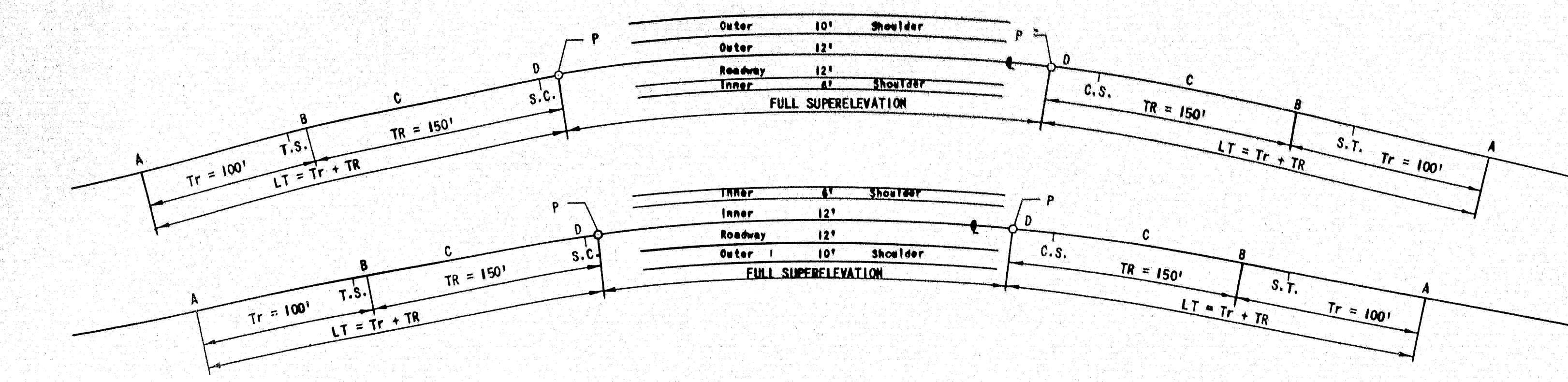
MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

SPECIAL DETAILS
DRAINAGE &
PORTABLE BARRICADES
SCALE: AS NOTED

THE CLARKESON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS

75-136

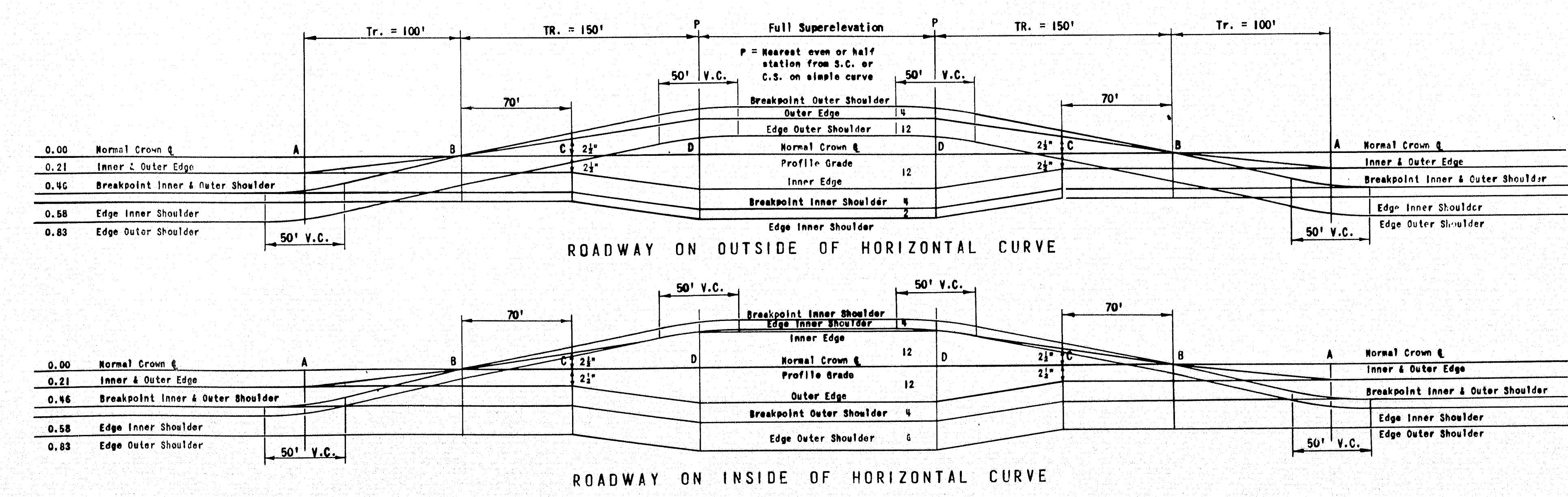




Distance From "P" Along $\frac{Q}{L}$	OUTER ROADWAY							INNER ROADWAY						
	Edge Outer Shoulder	Breakpoint Outer Shoulder	Outer Edge	Q	Inner Edge	Breakpoint Inner Shoulder	Edge Inner Shoulder	Edge Inner Shoulder	Breakpoint Inner Shoulder	Inner Edge	Q	Outer Edge	Breakpoint Outer Shoulder	Edge Outer Shoulder
Full Sup. elev.	-0.09	+0.28	+0.21	0.00	-0.21	-0.46	-0.58	+0.16	+0.28	+0.21	0.00	-0.21	-0.46	-0.83
P	-0.11	+0.26	+0.21	0.00	-0.21	-0.46	-0.58	+0.14	+0.26	+0.21	0.00	-0.21	-0.46	-0.83
50	-0.28	+0.09	+0.105	0.00	-0.21	-0.46	-0.58	-0.03	+0.09	+0.105	0.00	-0.21	-0.46	-0.83
100	-0.46	-0.09	0.00	0.00	-0.21	-0.46	-0.58	-0.21	-0.09	0.00	0.00	-0.21	-0.46	-0.83
150	-0.65	-0.28	-0.105	0.00	-0.21	-0.46	-0.58	-0.40	-0.28	-0.105	0.00	-0.21	-0.46	-0.83
200	-0.81	-0.44	-0.21	0.00	-0.21	-0.46	-0.58	-0.56	-0.44	-0.21	0.00	-0.21	-0.46	-0.83
Normal	-0.83	-0.46	-0.21	0.00	-0.21	-0.46	-0.58	-0.58	-0.46	-0.21	0.00	-0.21	-0.46	-0.83

D = 1° - 30'

Distance From "P" Along \angle	OUTER ROADWAY							INNER ROADWAY						
	Edge Outer Shoulder	Breakpoint Outer Shoulder	Outer Edge	\angle	Inner Edge	Breakpoint Inner Shoulder	Edge Inner Shoulder	Edge Inner Shoulder	Breakpoint Inner Shoulder	Inner Edge	\angle	Outer Edge	Breakpoint Outer Shoulder	Edge Outer Shoulder
Full Sup. elev.	+ 0.23	+ 0.60	+ 0.45	0.00	- 0.45	- 0.70	- 0.83	+ 0.48	+ 0.60	+ 0.45	0.00	- 0.45	- 0.70	- 1.07
P	+ 0.21	+ 0.58	+ 0.45	0.00	- 0.45	- 0.70	- 0.83	+ 0.46	+ 0.58	+ 0.45	0.00	- 0.45	- 0.70	- 1.07
50	+ 0.03	+ 0.40	+ 0.30	0.00	- 0.30	- 0.55	- 0.67	+ 0.28	+ 0.40	+ 0.30	0.00	- 0.30	- 0.55	- 0.92
100	- 0.17	+ 0.20	+ 0.15	0.00	- 0.21	- 0.46	- 0.58	+ 0.08	+ 0.20	+ 0.15	0.00	- 0.21	- 0.46	- 0.83
150	- 0.37	0.00	0.00	0.00	- 0.21	- 0.46	- 0.58	- 0.12	0.00	0.00	0.00	- 0.21	- 0.46	- 0.83
200	- 0.60	- 0.23	- 0.105	0.00	- 0.21	- 0.46	- 0.58	- 0.35	- 0.23	- 0.105	0.00	- 0.21	- 0.46	- 0.83
250	- 0.80	- 0.43	- 0.21	0.00	- 0.21	- 0.46	- 0.58	- 0.55	- 0.43	- 0.21	0.00	- 0.21	- 0.46	- 0.83
Normal	- 0.83	- 0.46	- 0.21	0.00	- 0.21	- 0.46	- 0.58	- 0.58	- 0.46	- 0.21	0.00	- 0.21	- 0.46	- 0.83



P = Point of Full Superelevation, nearest even or half station on Simple Curve, unless otherwise noted.
e = Rate of Superelevation (foot per foot).
Tr = Transition Runoff (100 feet).
TR = Transition Runoff (Varies).
LT = Total Length of Transition (Tr + T).

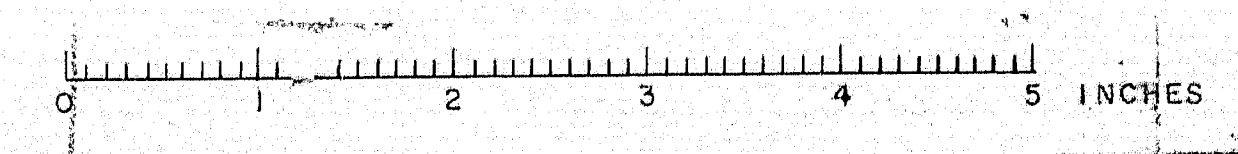
NOTES:
1. All distances are in feet and are added (+) to or subtracted (-) from Profile Elevation.
2. Fifty (50) foot Vertical Curves, shown, are to be used when grade changes are more than 0.30%.

MAINE STATE HIGHWAY COMMISSION
AUGUSTA MAINE

SPECIAL DETAILS

SUPERELEVATION DATA

THE CLARKESON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS



ROD SOUNDINGS*
MAIN STREET

Sta. 48 + 32 35' Rt.

Ground Elevation 213.0			
ELEV.	DEPTH	BLOW COUNT	REMARKS
212.0	0.0'-1.0'		
211.0	1.0'-2.0'		
210.0	2.0'-3.0'	24	
209.0	3.0'-4.0'	54	
208.0	4.0'-5.0'	56	
207.0	5.0'-6.0'	194	
206.0	6.0'-7.0'	103	
205.0	7.0'-8.0'	101	
204.7	8.0'-8.3'	39	Refusal

Sta. 48 + 65 25' Lt.

Ground Elevation 212.6			
ELEV.	DEPTH	BLOW COUNT	REMARKS
211.6	0.0'-1.0'		
210.6	1.0'-2.0'	3	
209.6	2.0'-3.0'	6	
208.6	3.0'-4.0'	7	
207.6	4.0'-5.0'	10	
206.6	5.0'-6.0'	36	
205.6	6.0'-7.0'	122	
204.6	7.0'-8.0'	241	Refusal

Sta. 49 + 68 35' Rt.

Ground Elevation 208.5			
ELEV.	DEPTH	BLOW COUNT	REMARKS
207.5	0.0'-1.0'		
206.5	1.0'-2.0'	9	
205.5	2.0'-3.0'	24	
204.5	3.0'-4.0'	42	
203.5	4.0'-5.0'	58	
202.5	5.0'-6.0'	120	
201.5	6.0'-7.0'	229	Refusal

Sta. 50 + 38 35' Lt.

Ground Elevation 204.3			
ELEV.	DEPTH	BLOW COUNT	REMARKS
203.3	0.0'-1.0'		
203.3	1.0'-2.0'	14	
201.3	2.0'-3.0'	17	
200.3	3.0'-4.0'	24	
199.3	4.0'-5.0'	34	
198.3	5.0'-6.0'	41	
197.3	6.0'-7.0'	98	
196.3	7.0'-8.0'	139	
195.5	8.0'-8.8'	137	Refusal

NORTH BOUND ROADWAY

Sta. 1082 + 00 Q
A.B.

Ground Elevation 208.5		
ELEV.	DEPTH	DESCRIPTION
207.5	0.0'-1.0'	Loam
202.5	1.0'-6.0'	Wet Silty Stony Till
		Rocks
Ground Water Elevation 206.5		

Sta. 1083 + 00 Q
A.B.

Ground Elevation 203.5		
ELEV.	DEPTH	DESCRIPTION
202.7	0.0'-0.8'	Loam
198.7	0.8'-4.8'	Silty Till
197.7	4.8'-5.8'	Wet Pebbly Sand

SOUTH BOUND ROADWAY

Sta. 1082 + 00 Q
A.B.

Ground Elevation 218.8		
ELEV.	DEPTH	DESCRIPTION
218.3	0.0'-0.5'	Loam
213.6	0.5'-5.2'	Moist Silty Sand
212.7	5.2'-6.1'	Hard Till
		Rocks

LEGEND:

A.B. = Auger Boring
B & S = Boring & Sounding
R.S. = Rod Soundings
Hammer = 60 Lb.
Drop = 18 In.
Rod = 1 In.
R = Refusal
N = No Refusal

NOTES:

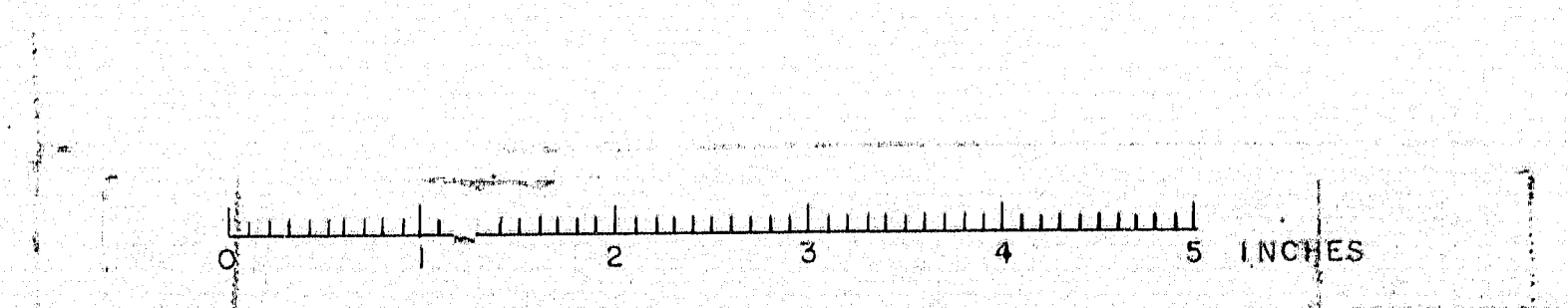
- Subsurface data were supplied by the Maine State Highway Commission during the period of July-August 1958.
- Subsurface investigations were taken for the purpose of design and show conditions at test points only. They do not necessarily show the nature of materials to be encountered during construction.
- The contractor is to form his own opinion of the character of the material, and is to make his own interpretation of subsurface data. The Engineer does not warrant the findings as being accurate or complete.

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

SUBSURFACE DATA

THE CLARKESON ENGINEERING CO., INC.
CONSULTING ENGINEERS
BOSTON MASSACHUSETTS

75-138



B. P. R.	STATE	PROJECT	NO.	SHEET	TOTAL
REG. NO.				NO.	SHEETS
1	MAINE	I-95-6(24)	14	34	

WATERVILLE INTERSTATE

LEGEND

- NEW POLE
- ✕ EXISTING POLE
- △ ANCHOR

NOTE: REMOVE EXISTING POLES #55 thru #60 INCLUSIVE.

GROUND LINE

INTERSECTIONS CONDUIT

Construct Driveways

- Sta. 44+40± R.I.
- Sta. 45+15± R.I.
- Sta. 45+20± R.I.
- Sta. 45+90± L.I.
- 1" Gravel Surface
- 12" Gravel Base

Construction &

- Southbound
- P.I. Sta. 1099+26.45
- $\Delta = 57^{\circ}-24'-09''$
- $\Delta c = 54^{\circ}-24'-09''$
- $Dc = 1^{\circ}-30'-00''$
- $Lc = 366.83'$
- $Tc = 196.17'$
- $Rc = 3819.72'$
- $Es = 535.54'$
- $As = 1^{\circ}-30'-00''$
- $Ls = 200.00'$
- $Xs = 199.99'$
- $Ys = 1.74'$
- $Ts = 2191.58'$

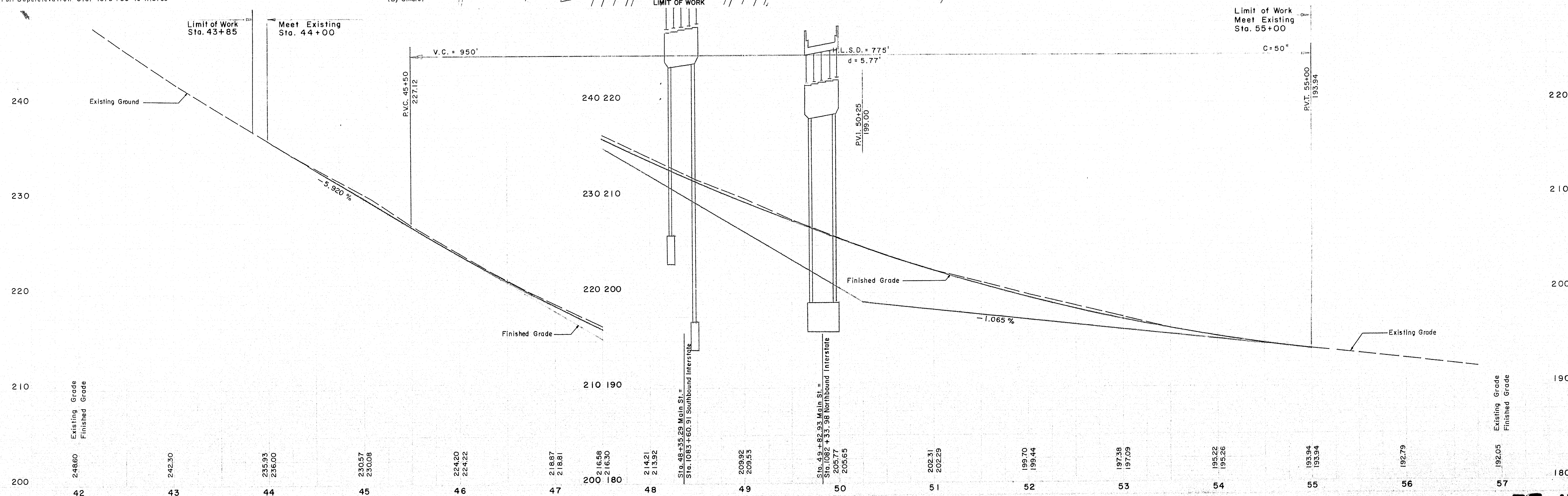
Full Superelevation Sta. 1079+50 to 1115+50

Construction &

- Northbound
- P.I. Sta. 1100+07.42
- $\Delta = 57^{\circ}-56'-51''$
- $\Delta c = 54^{\circ}-56'-51''$
- $Dc = 1^{\circ}-30'-00''$
- $Lc = 366.17'$
- $Tc = 1986.20'$
- $Rc = 3819.72'$
- $Es = 546.96'$
- $As = 1^{\circ}-30'-00''$
- $Ls = 200.00'$
- $Xs = 199.99'$
- $Ys = 1.74'$
- $Ts = 2215.26'$

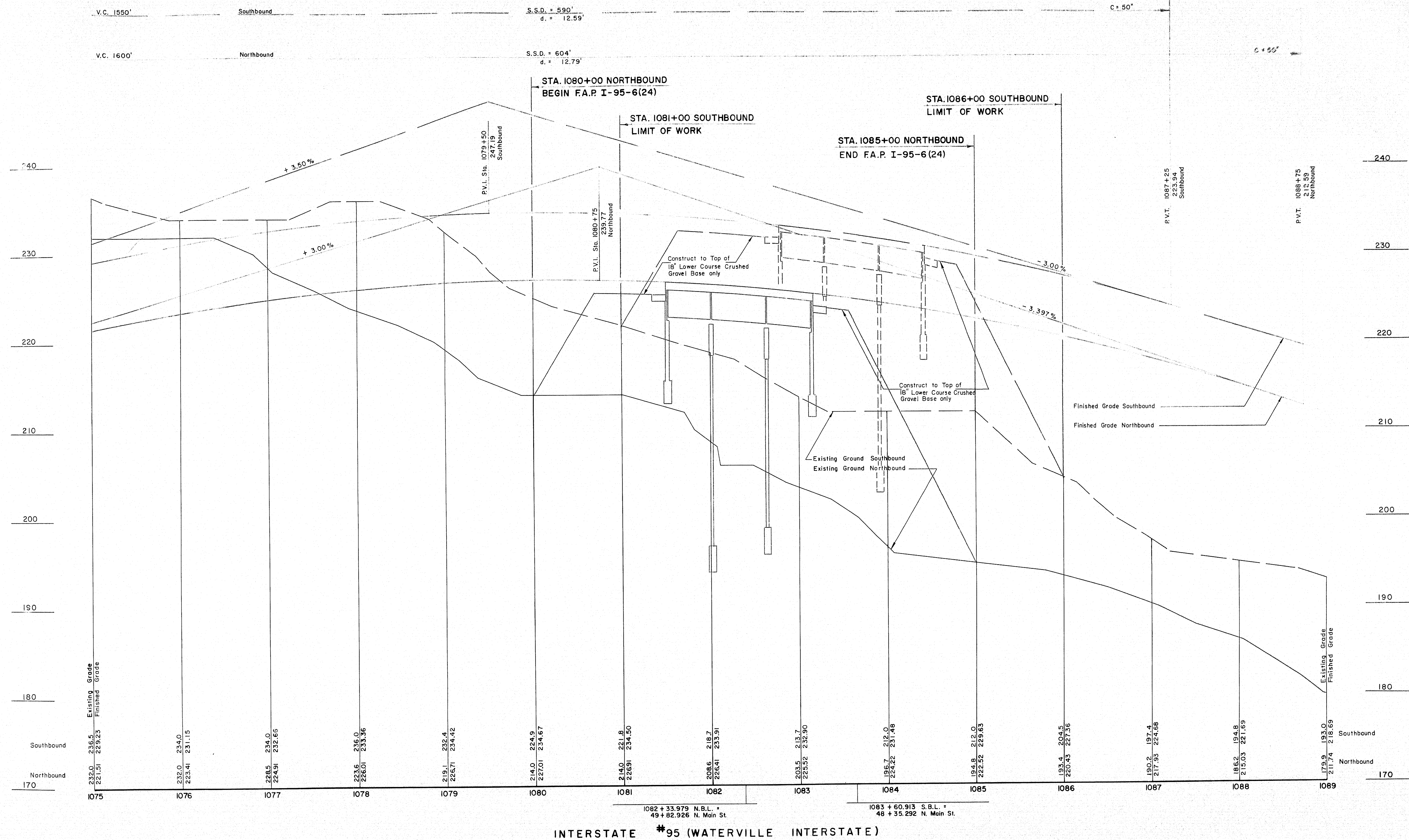
Note:
Remove Existing Driveway Culverts
44+00 to 55+00 Main St.

Full Superelevation Sta. 1080+00 to 1116+50

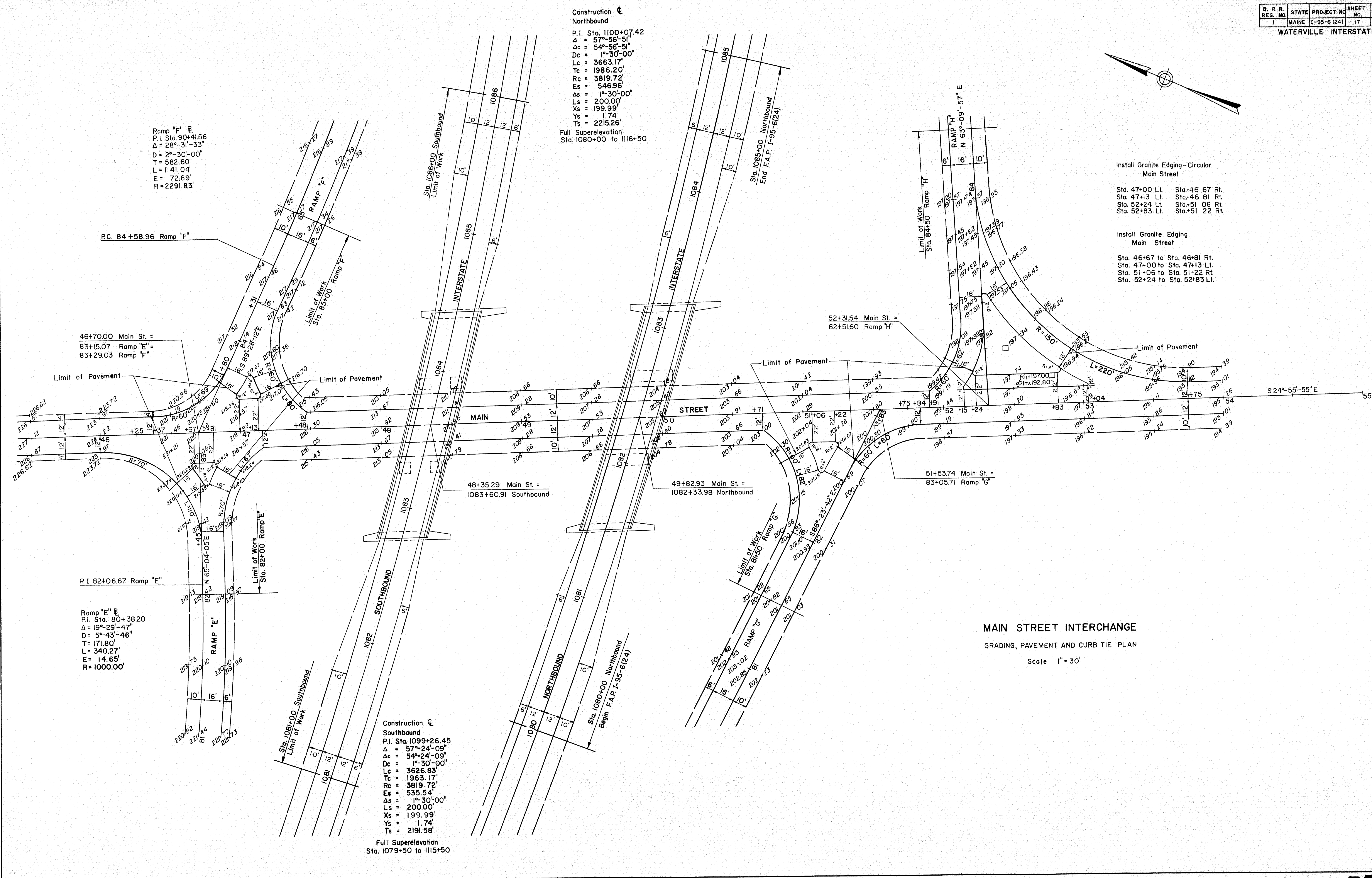


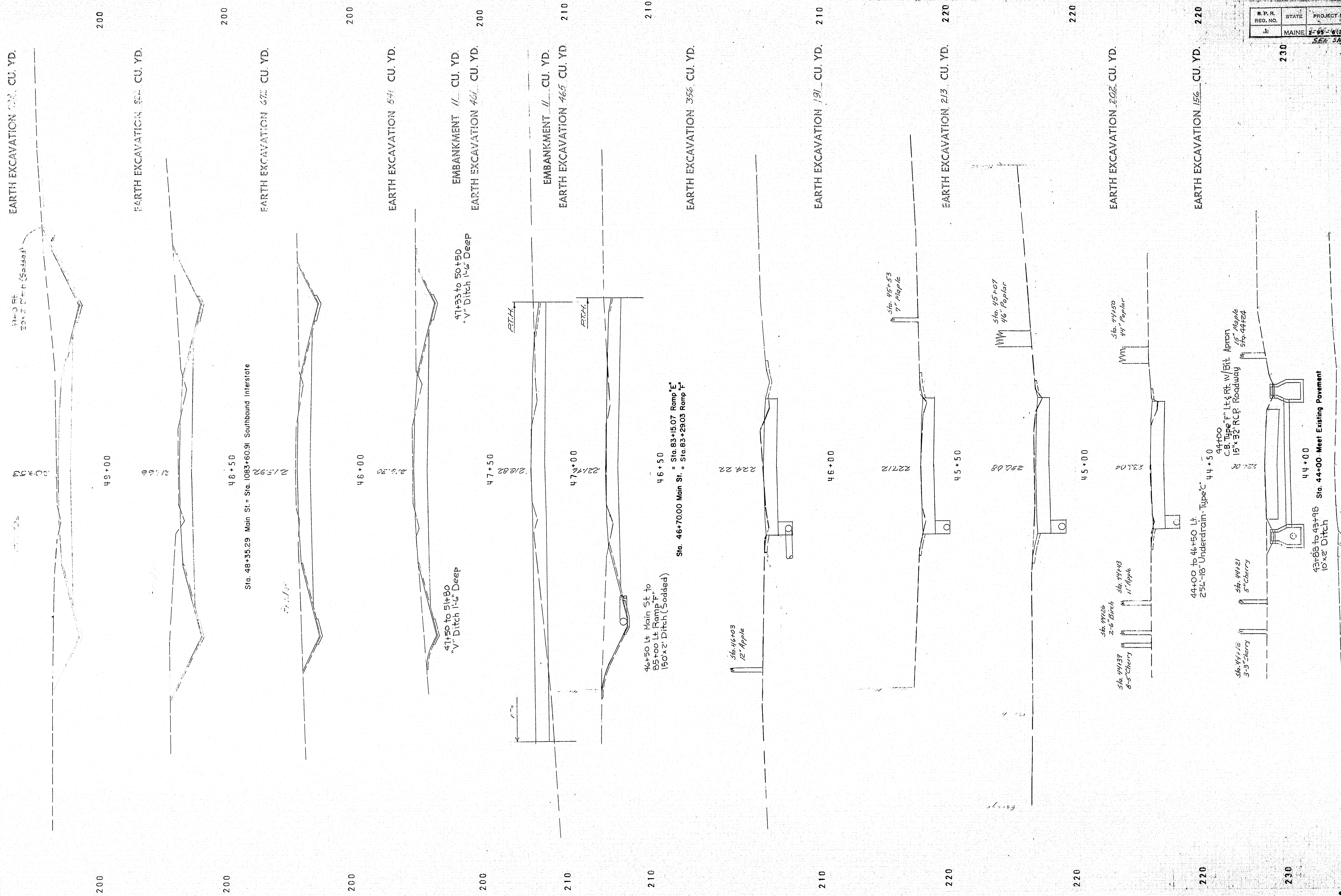
R.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(24)	15	54

WATERVILLE INTERSTATE



WATERVILLE INTERSTATE 44

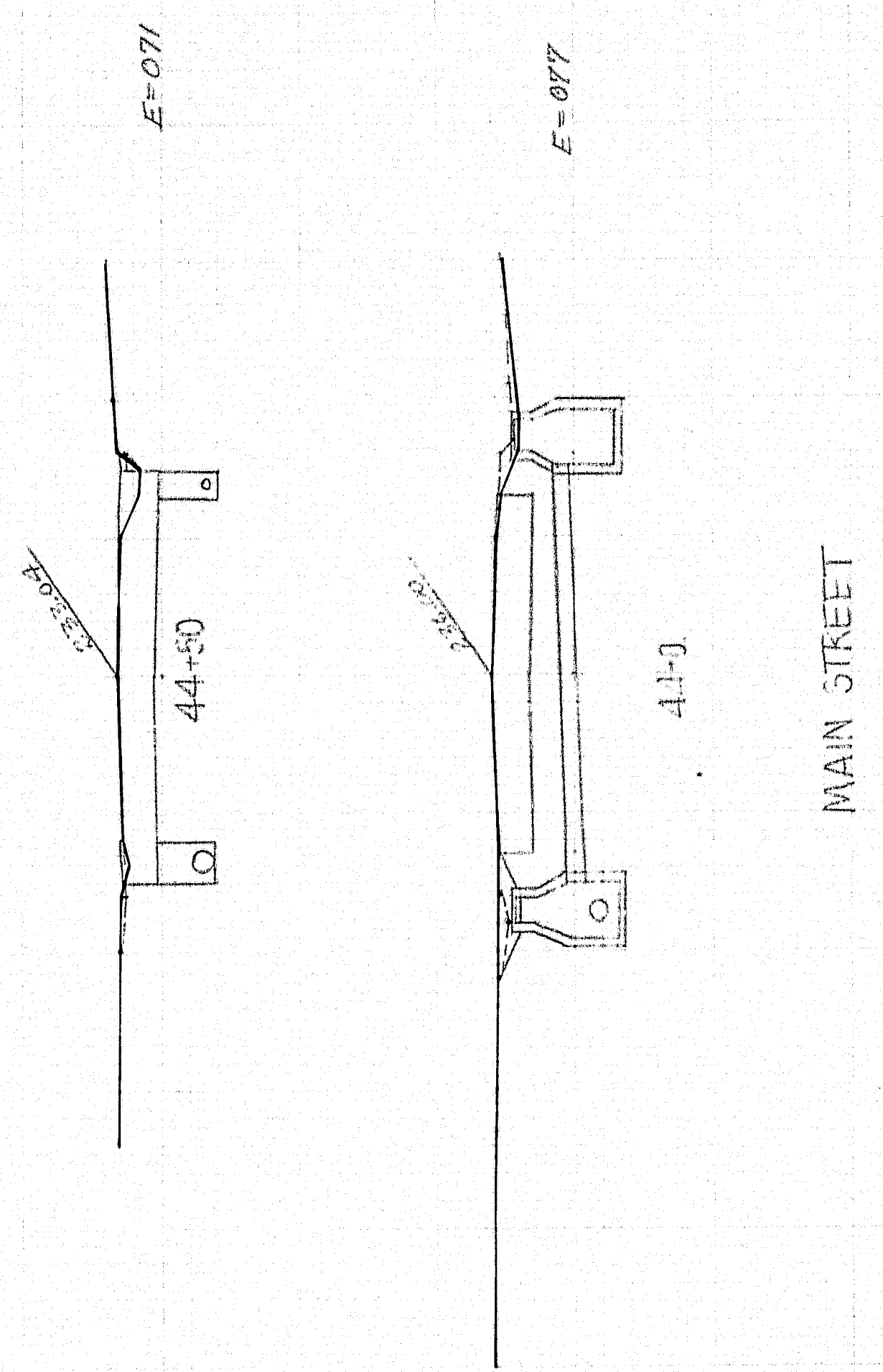




MAIN STREET - WATERVILLE INTERSTATE

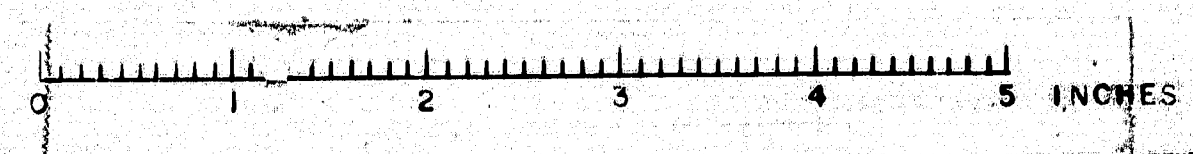
B.P.R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
41	MAINE	1-58-10(24)	10	24

24-7
R.C.
C.N.W.
2-10-60



230

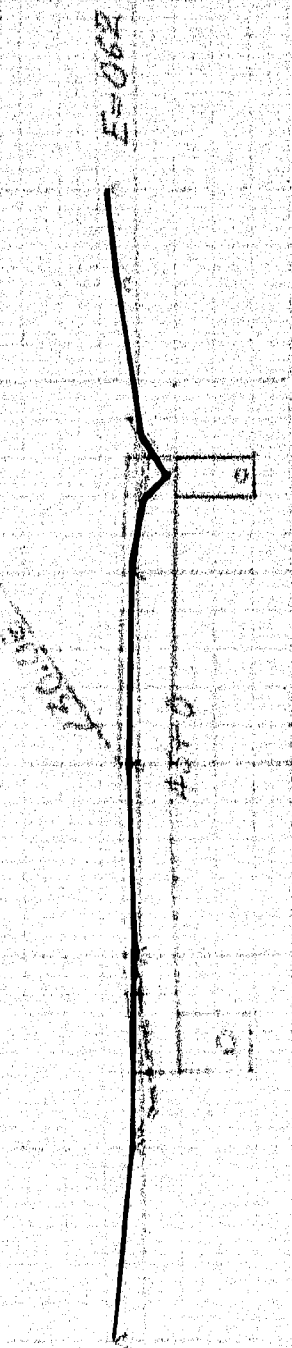
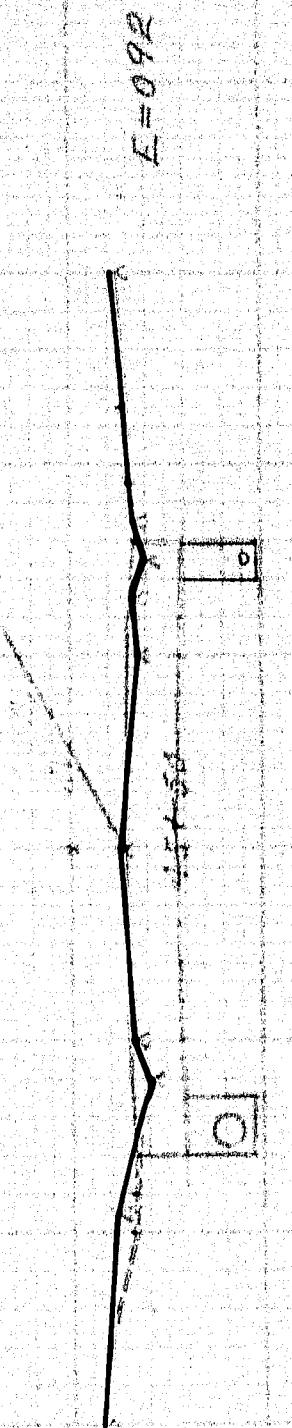
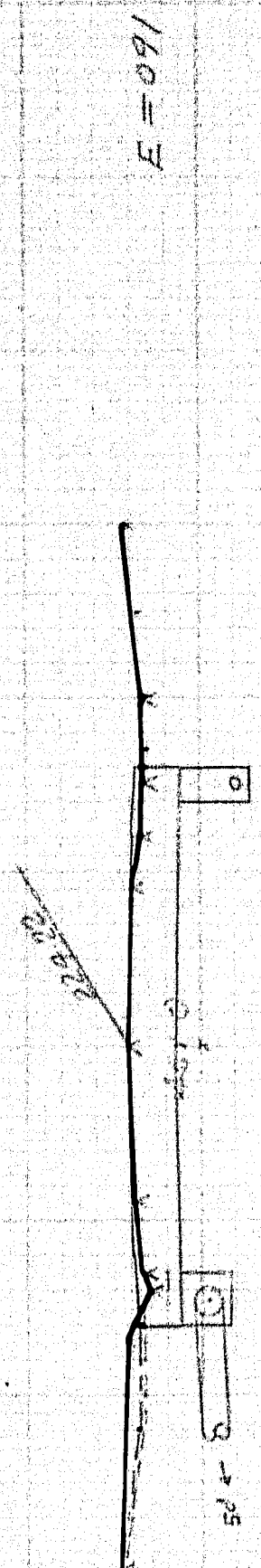
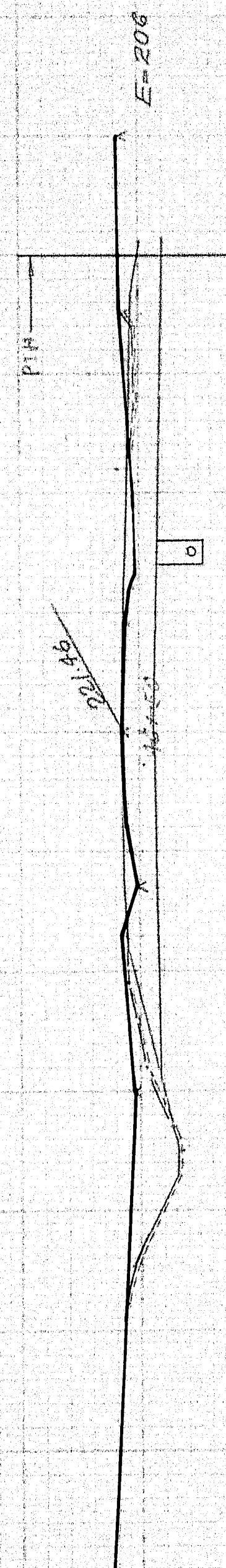
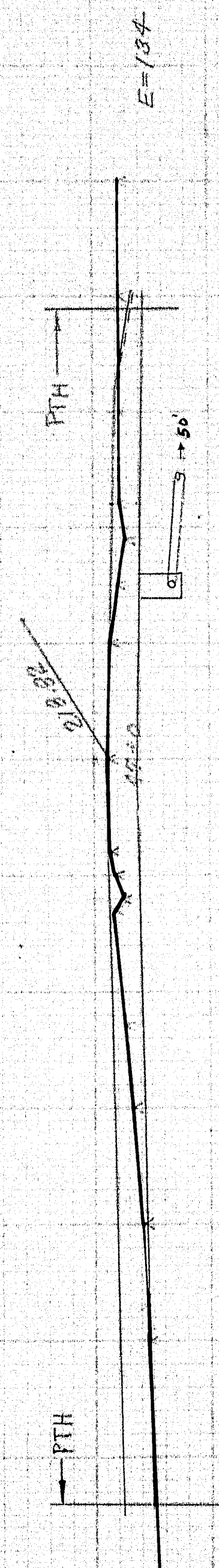
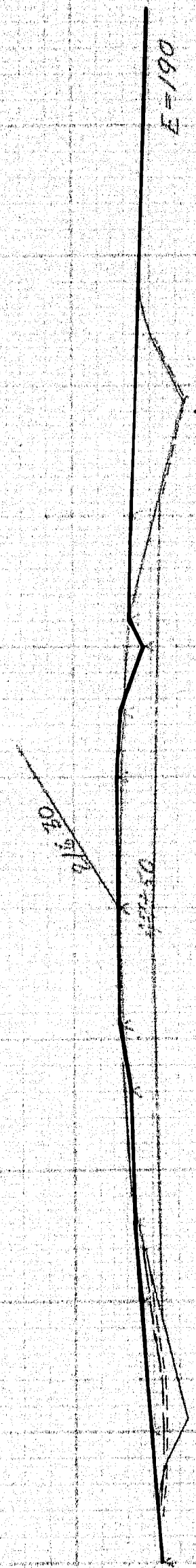
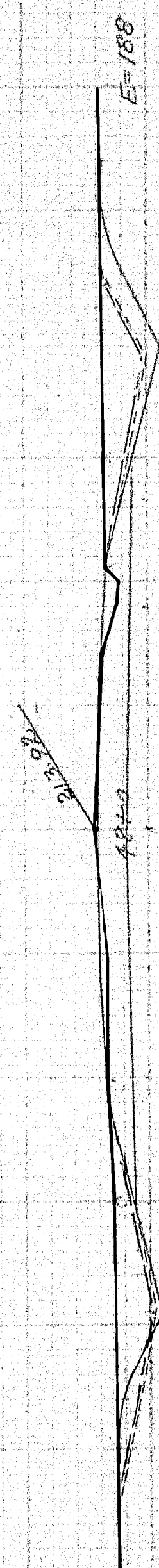
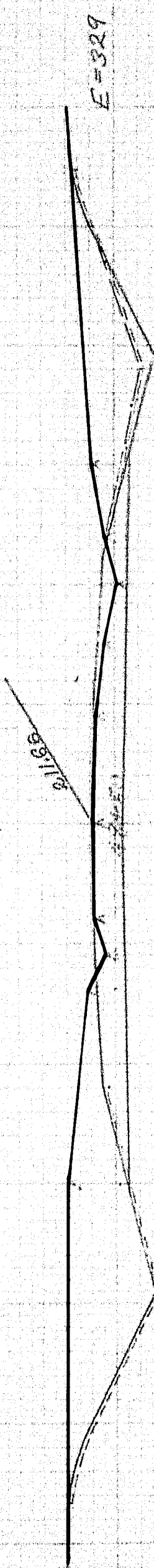
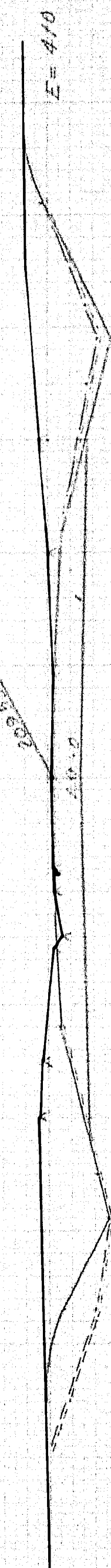
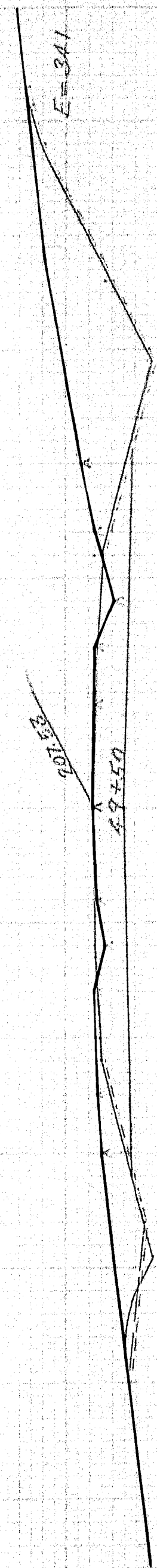
230



TOTAL 15
 8-8-44
 2-0-00

8-8-44
 2-0-00

Sheet 44



0 1 2 3 4 5 INCHES

75-145

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	1-25-100	0	1

EARTH EXCAVATION 241 CU. YD.

180

53+50

EARTH EXCAVATION 344 CU. YD.

190

53+00

EARTH EXCAVATION 339 CU. YD.

190

52+50

EARTH EXCAVATION 381 CU. YD.

190

52+00

EARTH EXCAVATION 483 CU. YD.

190

51+50

EARTH EXCAVATION 444 CU. YD.

190

51+00

EARTH EXCAVATION 478 CU. YD.

190

50+50

EARTH EXCAVATION 576 CU. YD.

190

50+00

EARTH EXCAVATION 635 CU. YD.

200

49+50

82+93 Ramp "H"
C.B. Type "F" Rt. W/Bit Apron
15'x42' RCP Roadway
15'x16' ACME Lt.
Pipe Connector Lt.

82+93 Ramp "H" to 52+80 Main St.
C.B. Type "H" Main St. Lt. Sta. 52+31.54 Main St. Lt. Sta. 52+51.60 Ramp "H"
15'x40' RCP Main St. Lt.

51+80 Lt. Main St.
84+80 Lt. Ramp "H"
165'x2' Ditch (Sodded)

Sta. 51+53.74 Main St. = Sta. 83+05.71 Ramp "G"

Sta. 49+82.93 Main St. = Sta. 1082+3398 Northbound Interstate

75-146

DATE: 10-1-88

BY: [illegible]

Sta. 55+00 - Limit of Work - Meet Existing Pavement...

54+54.88 16" Elm
54+55.09 27" Elm
#9.94

180

54+54.53 22" Elm
54+54.39 23" Elm
54+54.68 22" Elm
194.54

EARTH EXCAVATION 206 CU. YD.

180

54+50

54+53.88 26" Elm
54+54.12 17" Elm
195.26

54+00 to 55+00
100' x 3' Ditch (Sealed) EARTH EXCAVATION 296 CU. YD.

54+00

FINAL	SURVEY	BY	DATE
SURVEY	PLATTED	12-1-50	12-1-50
NOTE BOOK	AREAS	2-2-51	2-2-51
NO. 2-1	AREAS CHECKED	2-2-51	2-2-51

ORIGINAL	SURVEY	BY	DATE
SURVEY	PLATTED	12-1-50	12-1-50
NOTE BOOK	AREAS	2-2-51	2-2-51
NO. 2-1	AREAS CHECKED	2-2-51	2-2-51

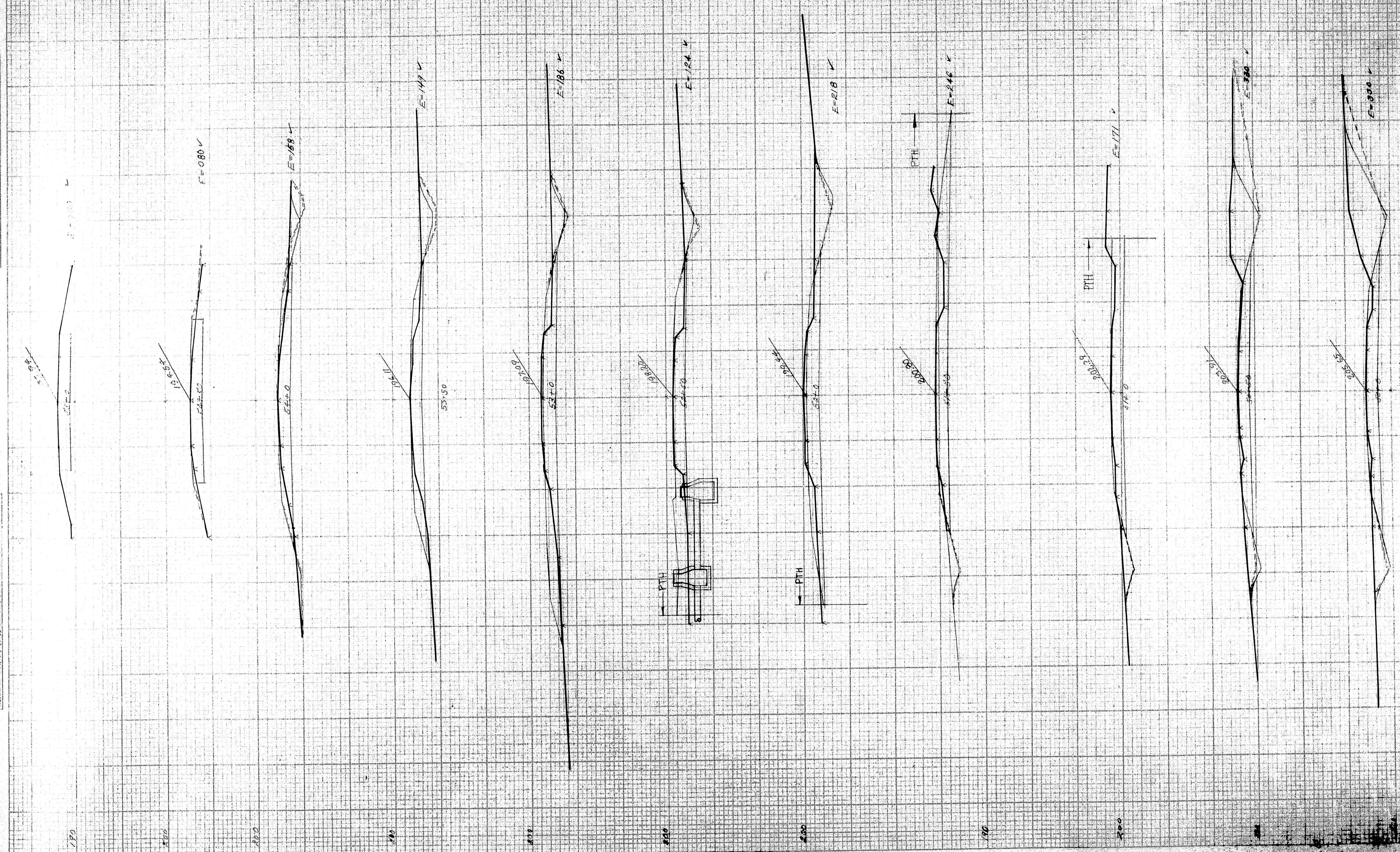
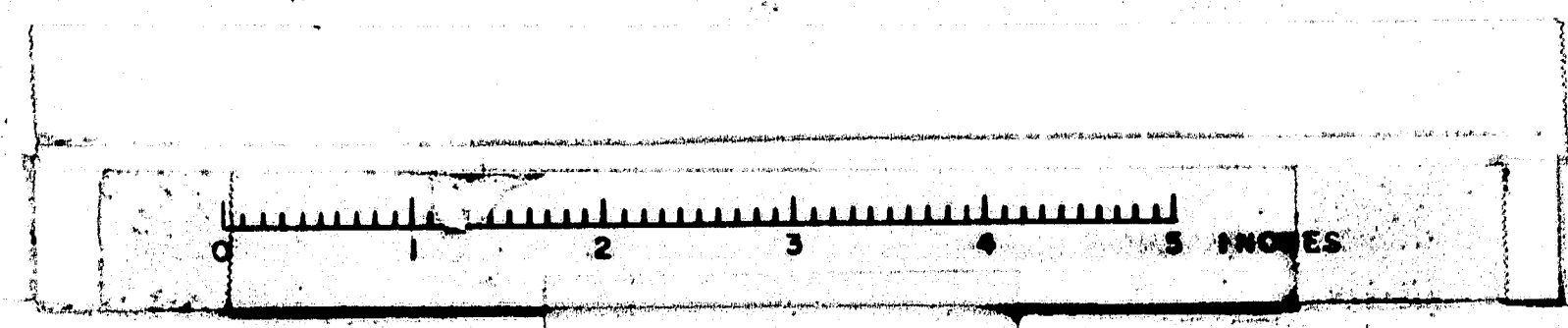


PLATE 3 - CROSS SECTION D.T.G.A.S.
 1950 AND 1951
 CHAS. H. HUNTER COMPANY, INC.

75-140

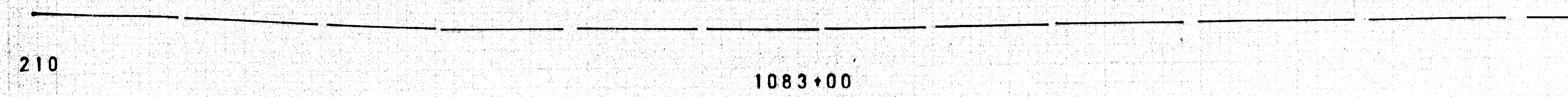


B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(24)	21	47

SEE SHEETS 21A & 21C

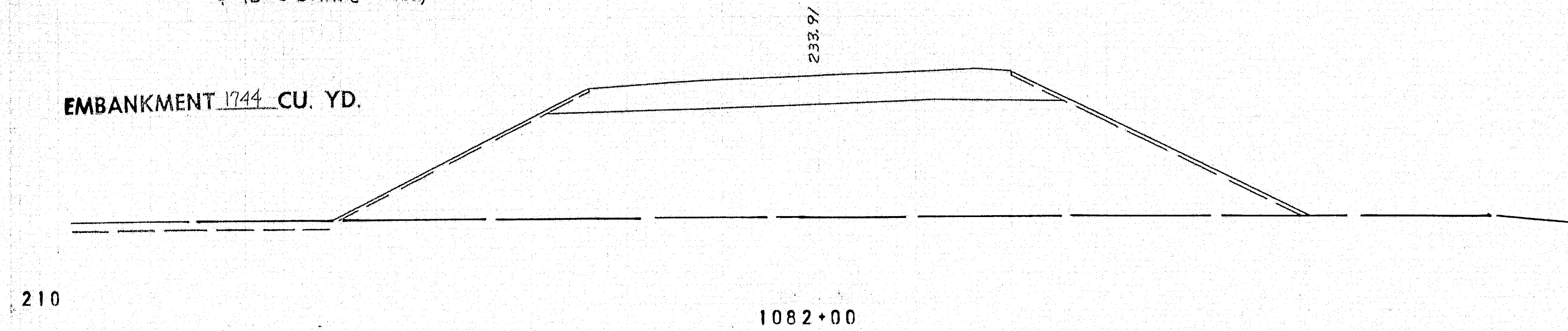
Sta. 1083+60.91 Southbound Interstate = Sta. 48+35.29 Main St.

Bridge

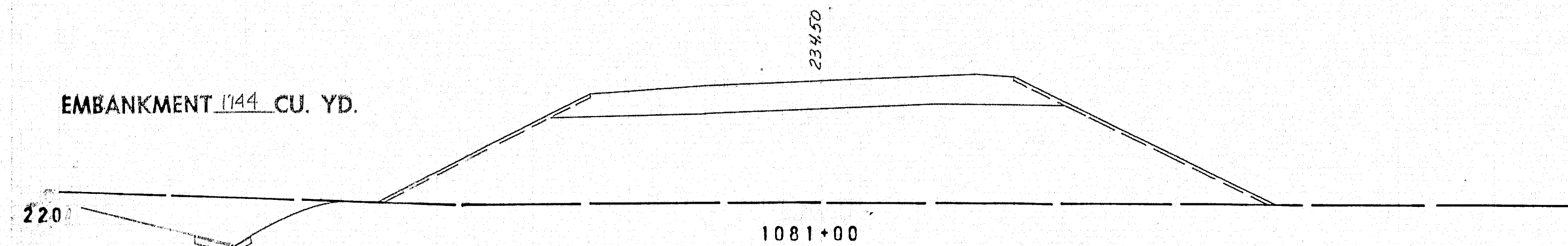


1082+47 Lt.
30' Sodded Gutter Outlet
& 40'x2' Ditch (Sodded)

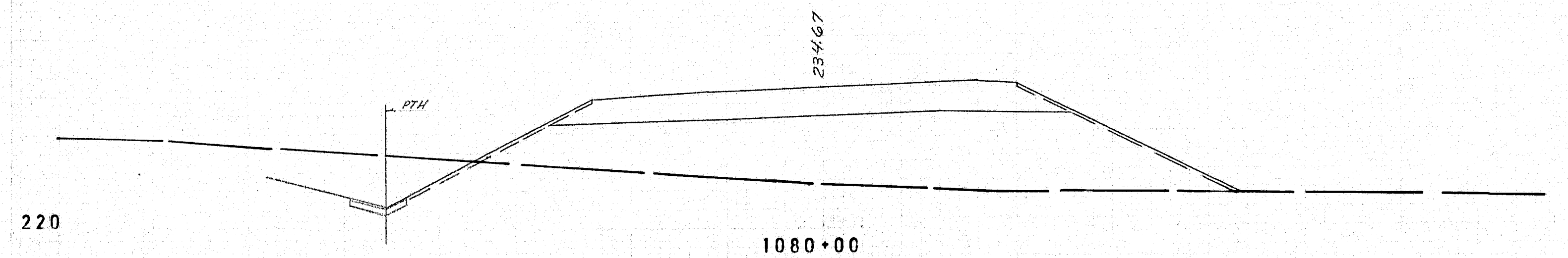
EMBANKMENT 1744 CU. YD.



EMBANKMENT 1144 CU. YD.



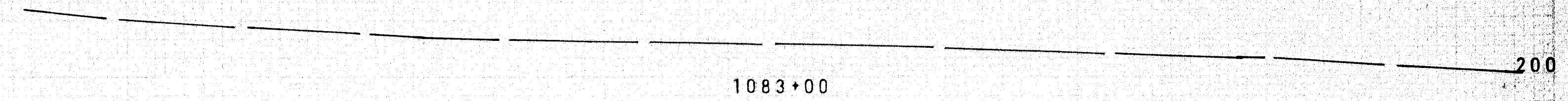
STA. 1081+00 SOUTHBOUND
LIMIT OF WORK



Southbound

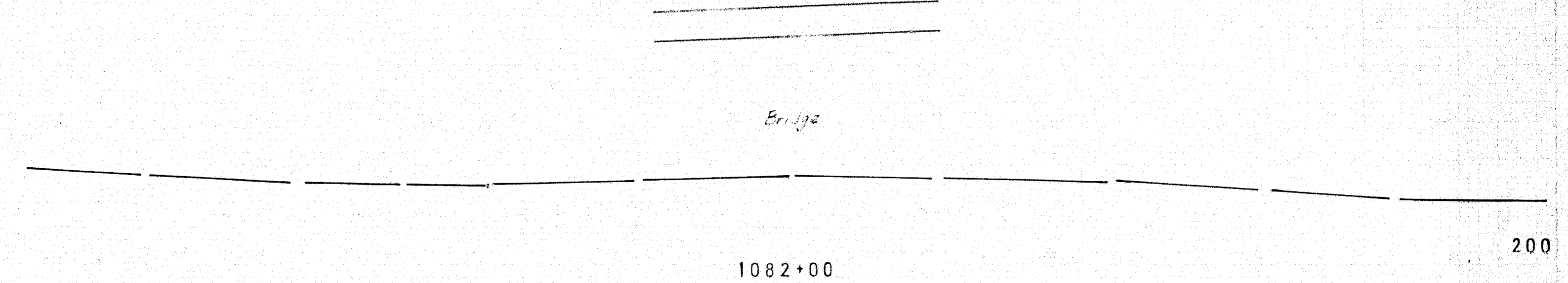
Sta. 1082+33.98 Northbound Interstate = Sta. 49+82.93 Main St.

Bridge

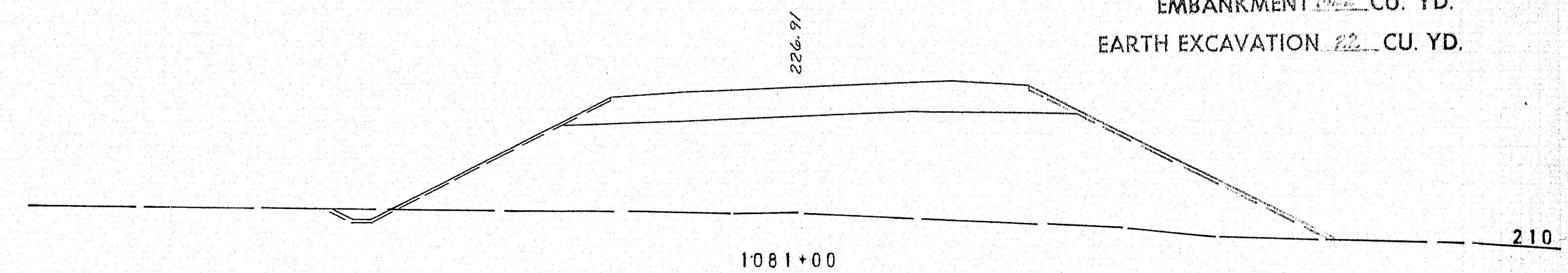


226.41

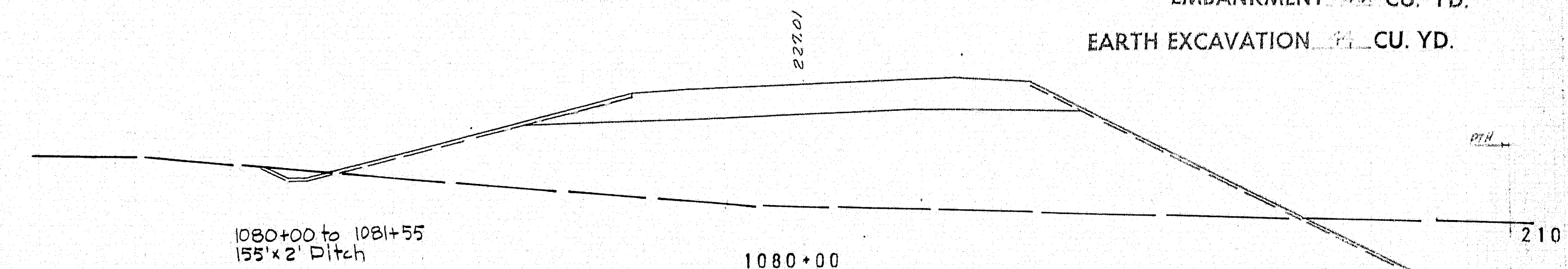
Bridge



EMBANKMENT 1622 CU. YD.
EARTH EXCAVATION 22 CU. YD.

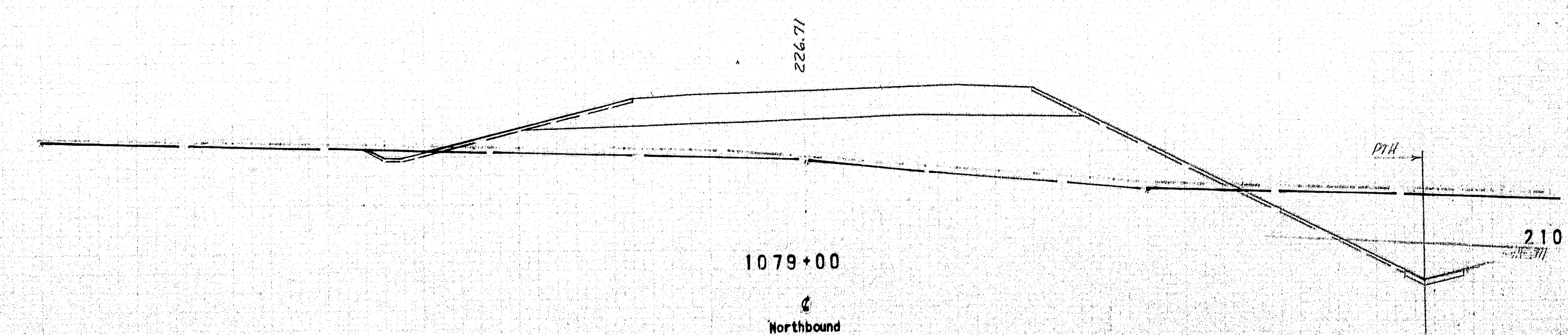


EMBANKMENT 1922 CU. YD.
EARTH EXCAVATION 24 CU. YD.



1080+00 to 1081+55
155'x2' Ditch

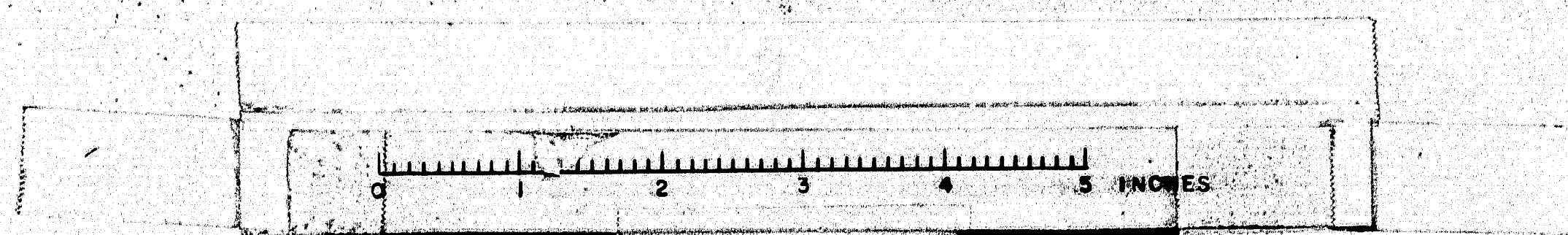
STA. 1080+00 NORTHBOUND
BEGIN F.A.P. I-95-6(24)



1079+00

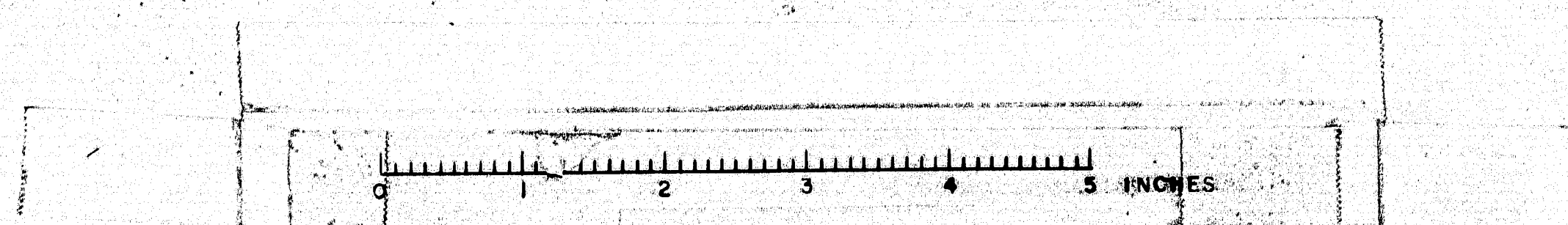
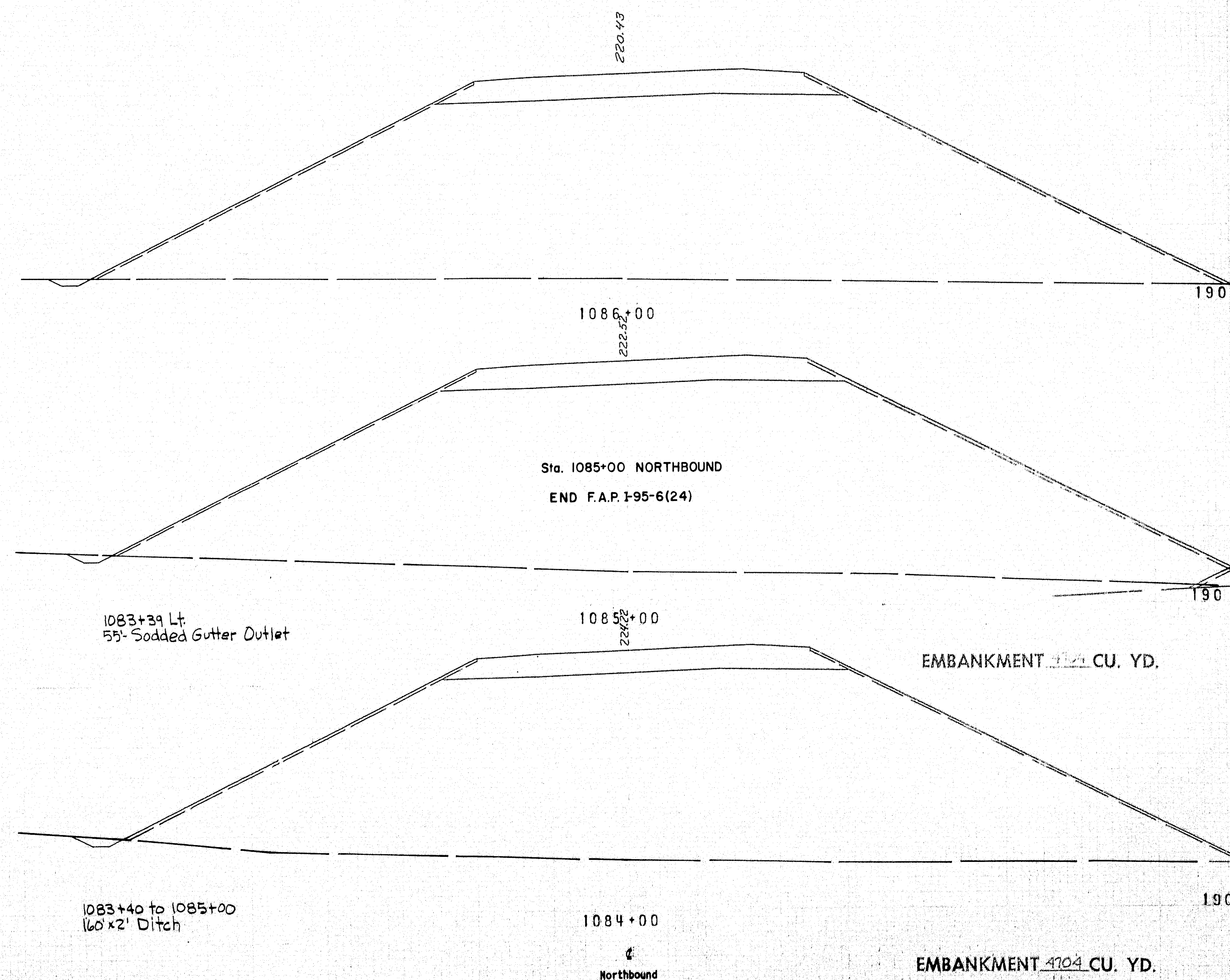
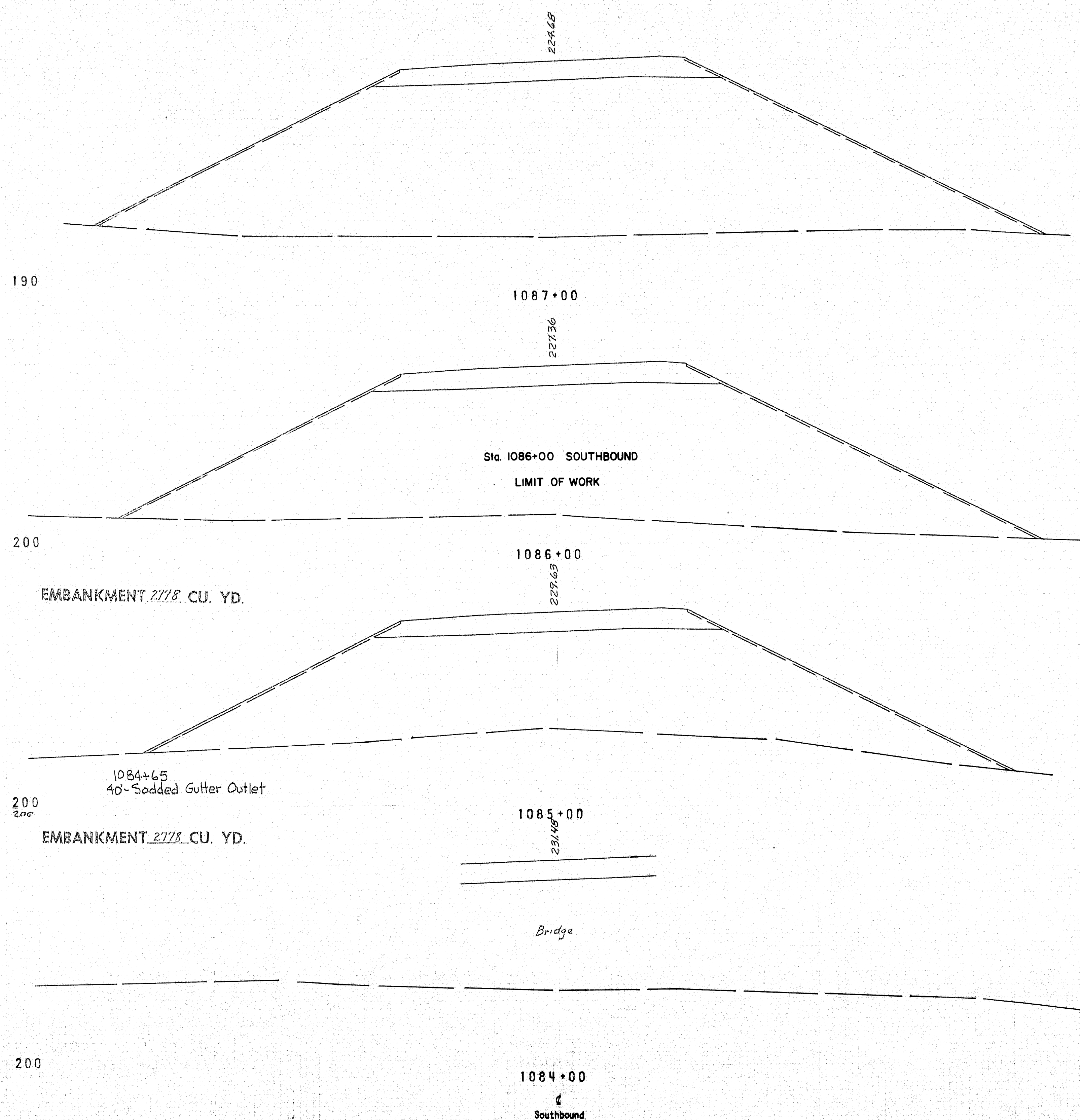
Northbound

75-149

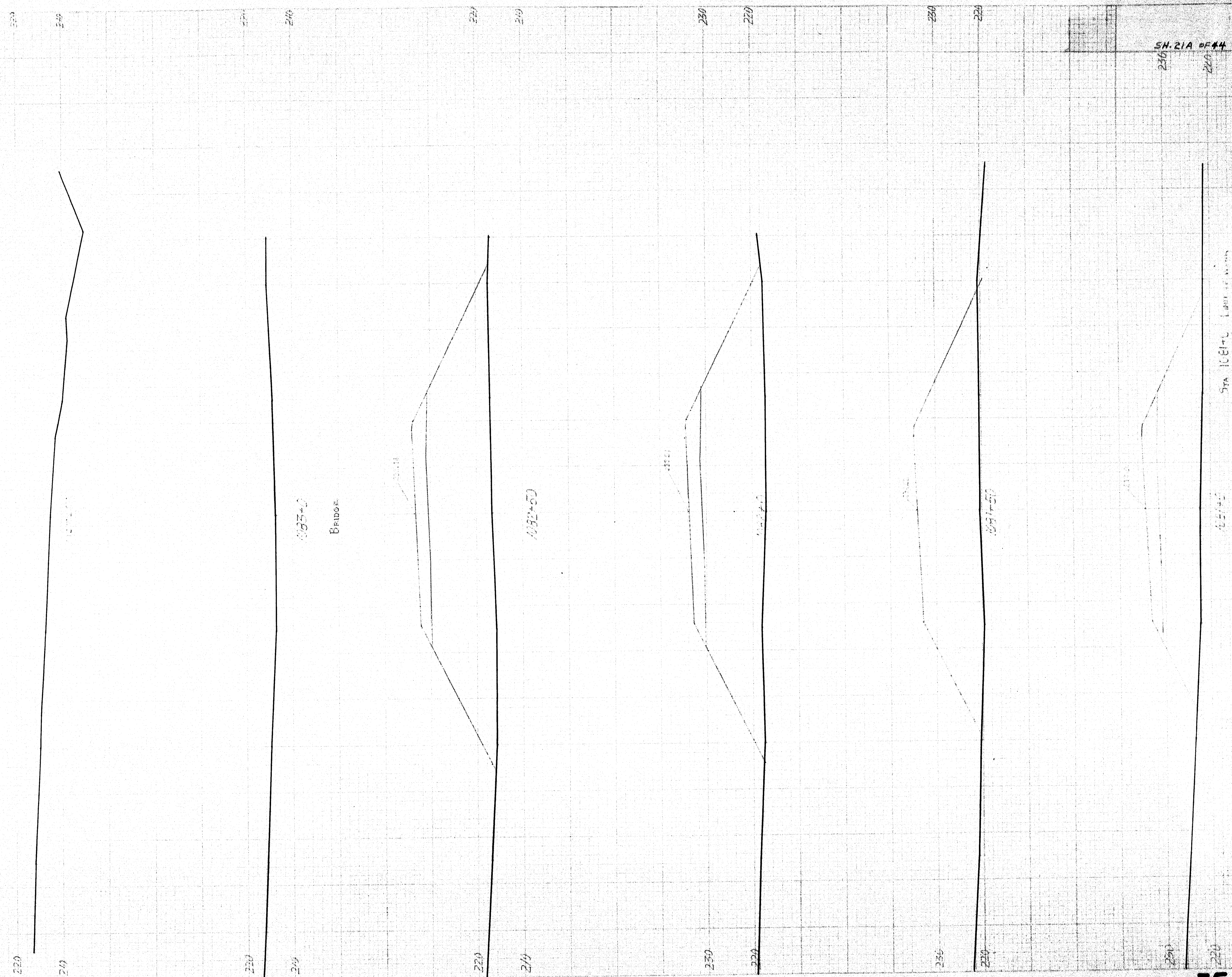


B. P. R. REG. NO.	STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-6(24)	22	24

See Sheet 215 & 216



75-150



BRIDGE

1083+0

1084+0

1085+0

1086+0

1087+0

1088+0

1089+0

1090+0

1091+0

1092+0

1093+0

1094+0

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1377+0

1378+0

1379+0

1380+0

1381+0

1382+0

1383+0

1384+0

1385+0

FINAL
 SURVEY
 DATE
 BY

ORIGINAL
 SURVEY
 DATE
 BY

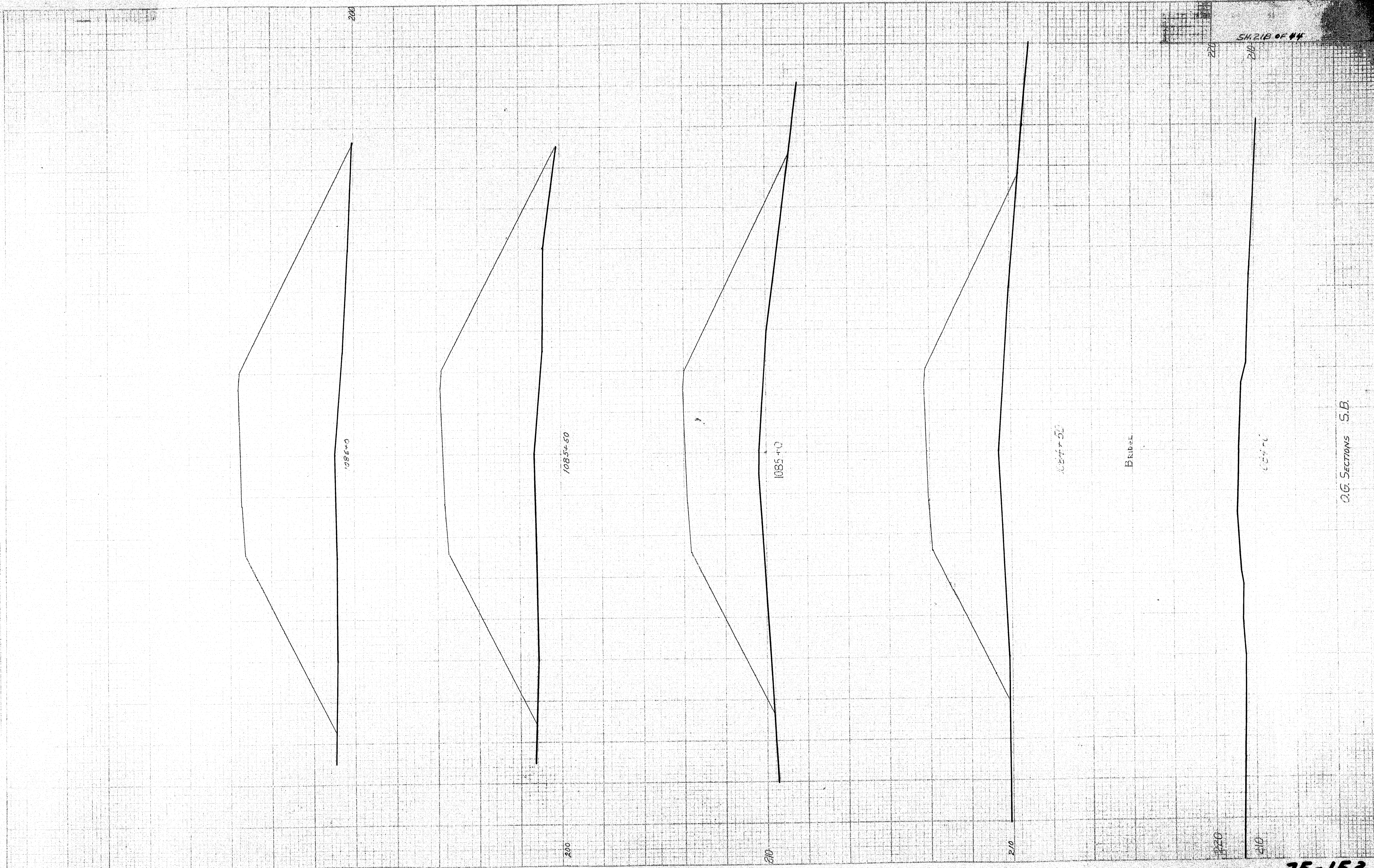


PLATE 3 CROSS SECTION A. P. 3. 4. 5.
 MADE FOR CHARTERED SURVEYOR
 DANIEL O'BRIEN SURVEYING, INC.

75-152

ORIGINAL SURVEY
 DATE
 BY
 PROJECT
 NO.

ORIGINAL SURVEY
 DATE
 BY
 PROJECT
 NO.

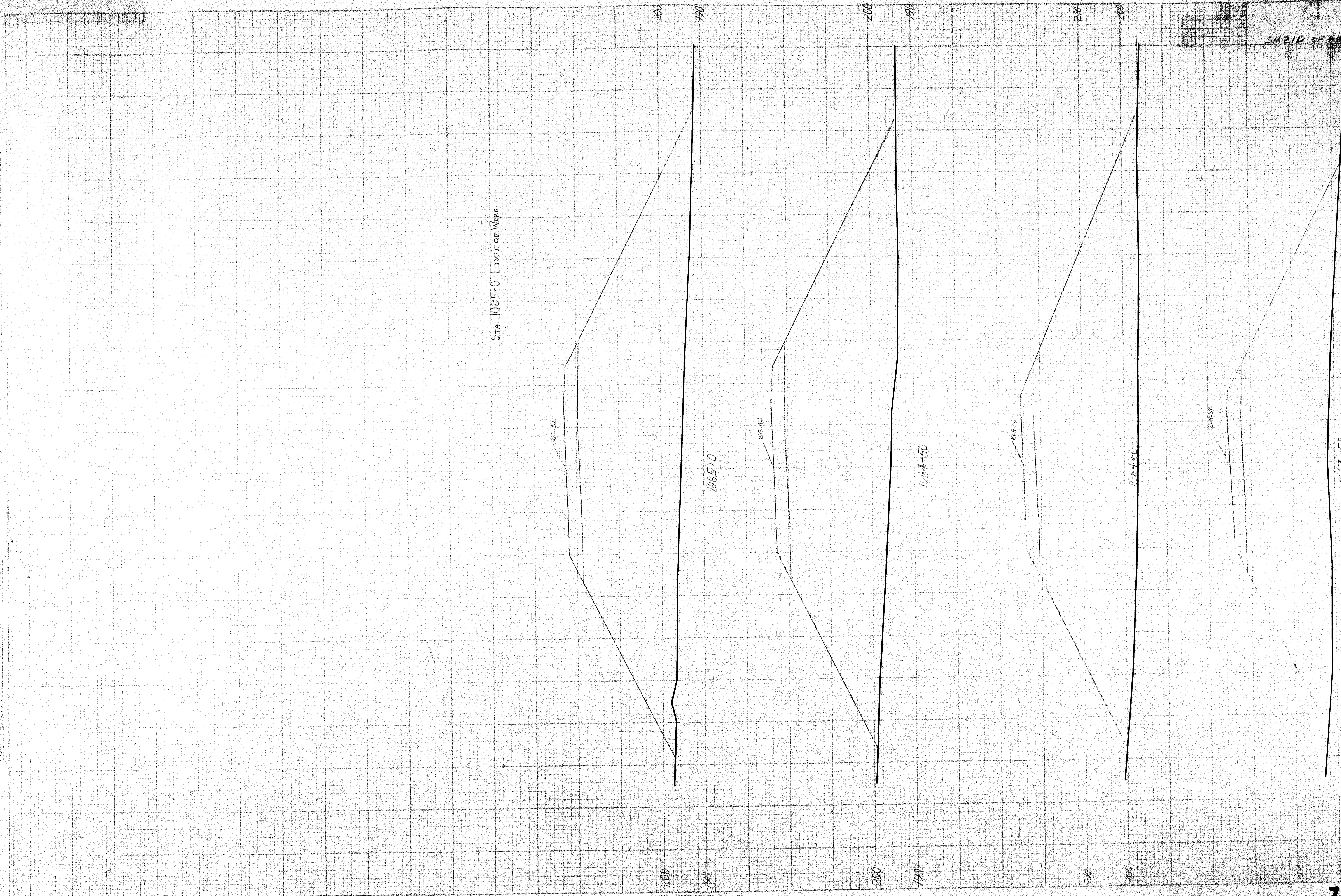
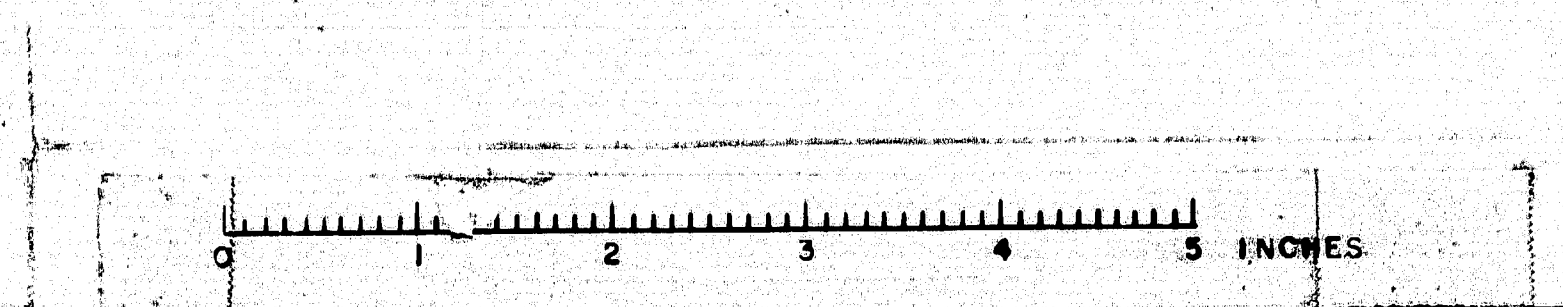


PLATE 3 CROSS SECTION B.P.C. & C.E.
 SCALE: 1" = 10' HORIZ. 1" = 2' VERT.
 DRAWN BY: J. H. HARRIS
 CHECKED BY: J. H. HARRIS

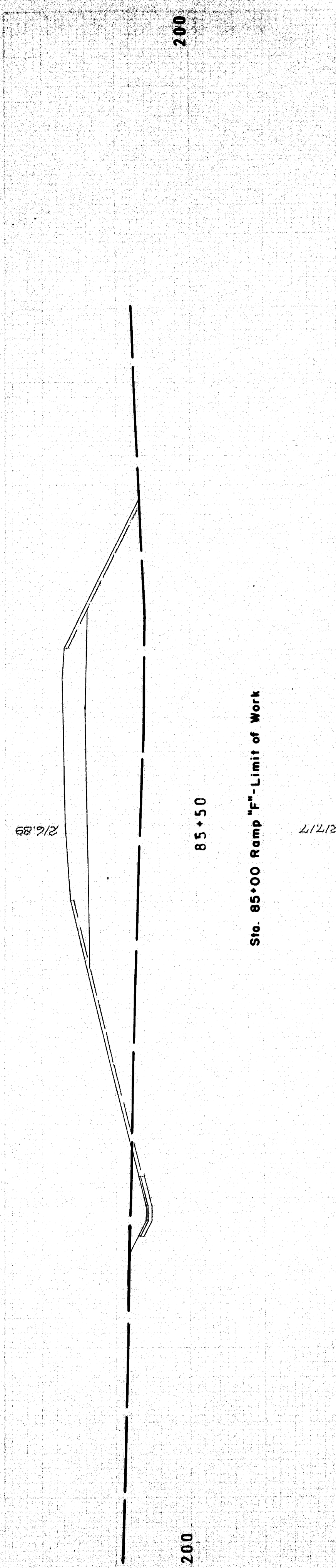


15-154

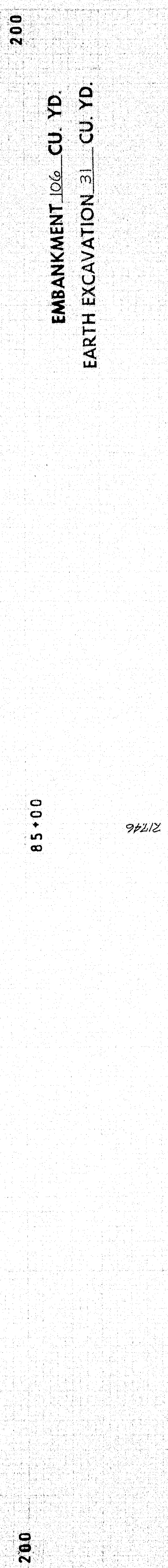
C.E. SECTIONS A.B.

B.R.R.	STATE	PROJECT NO.	DATE
REG. NO.			
1	MAINE	1-24-1000	73

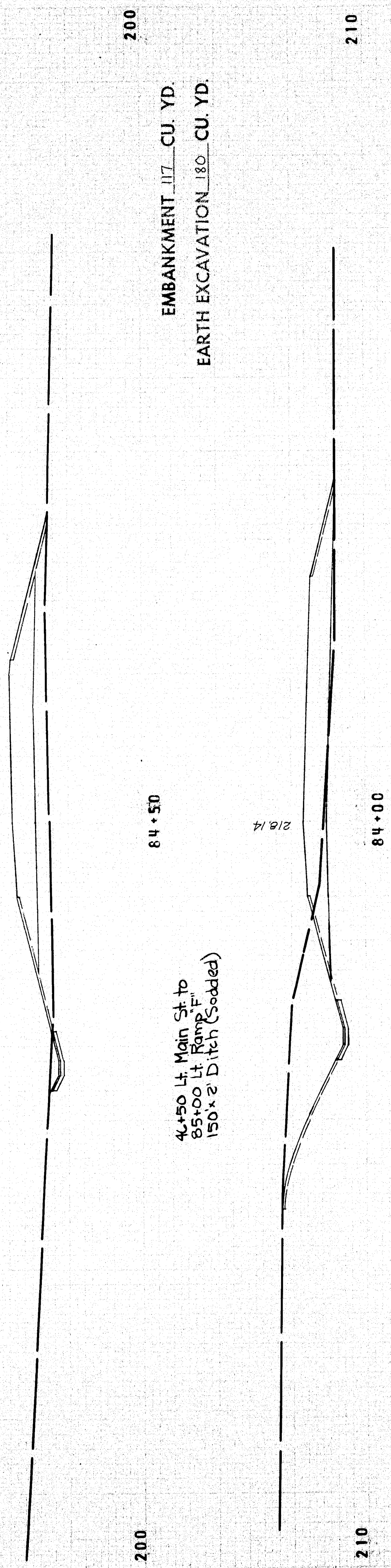
SAFE 34-23A



Sta. 85+00 Ramp "F" - Limit of Work



EMBANKMENT 106 CU. YD.
EARTH EXCAVATION 31 CU. YD.



EMBANKMENT 117 CU. YD.
EARTH EXCAVATION 180 CU. YD.

46+50 Lt. Main St. to
85+00 Lt. Ramp "F"
150 x 2' Ditch (Sodded)

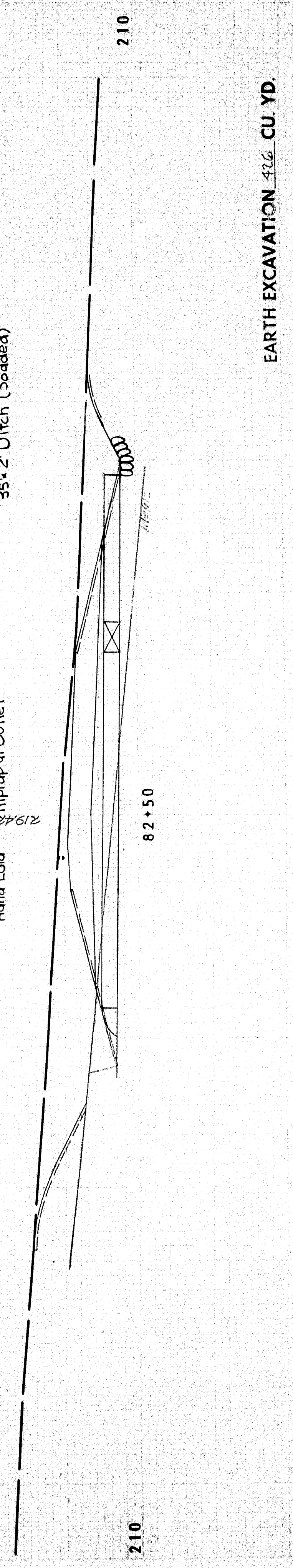
Sta. 83+29.03 Ramp "F" - Sta. 46+70.00 Main St.

Ramp "F"

Sta. 83+15.07 Ramp "E" - Sta. 46+70.00 Main St.

82+50
24 x 16 ACCMP L/W Metal Endwall
24 x 36 RCP Roadway
24 x 20 ACCMP RT
Pipe Connect to Rt.
Hand Laid Riprap at Outlet

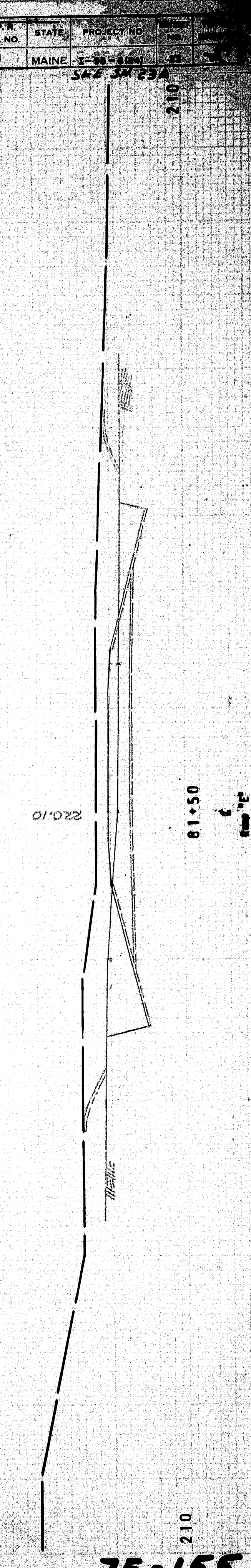
82+50 Ramp "E" to
41+33 Rt. Main St
35 x 2' Ditch (Sodded)



EARTH EXCAVATION 426 CU. YD.

82+00 to 82+50
"V" Ditch 2' Deep

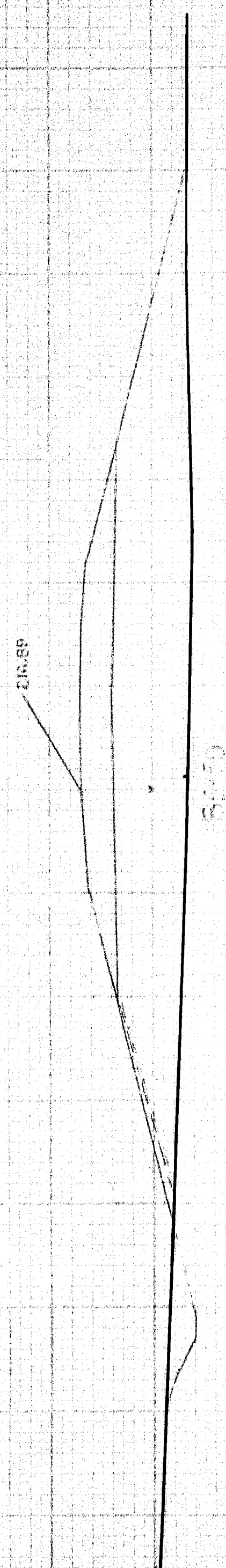
Sta. 82+00 Ramp "E" - Limit of Work



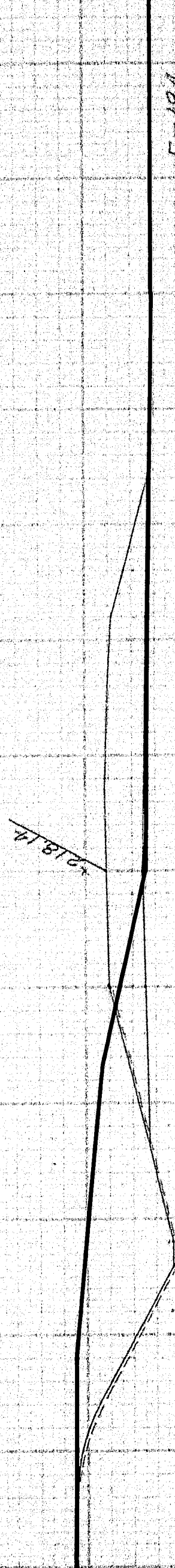
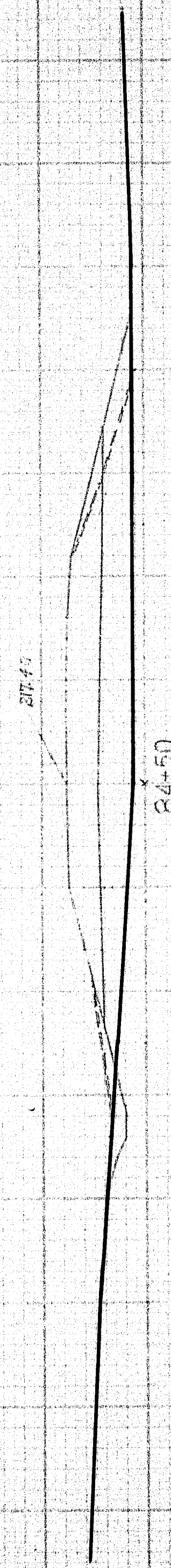
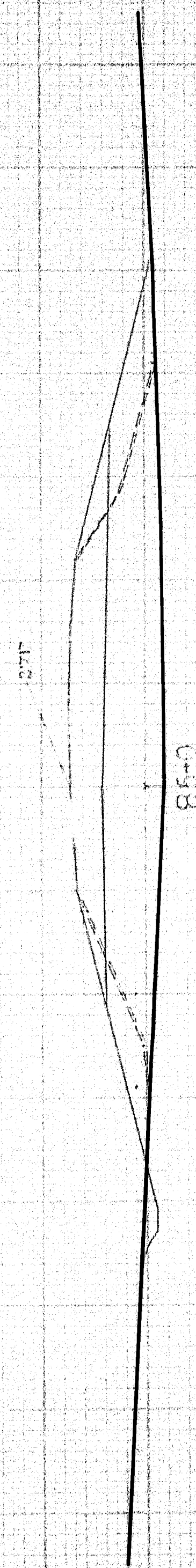
81+50
Ramp "E"

FINAL	DESIGNED	BY	DATE
REVISION	1	8/26	11/7/80
SURVEY	PAVING		
NO. 22	11/15/80		

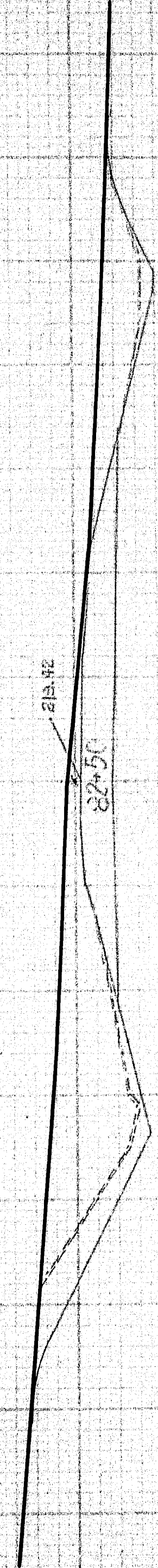
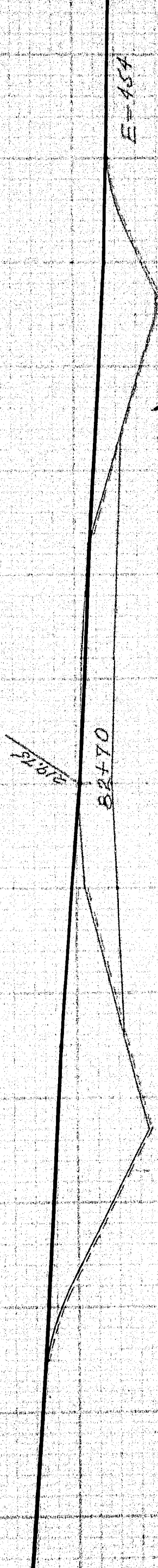
ORIGINAL	DATE	BY
11/15/80	11/7/80	
SURVEY	PAVING	
NO. 22	11/15/80	



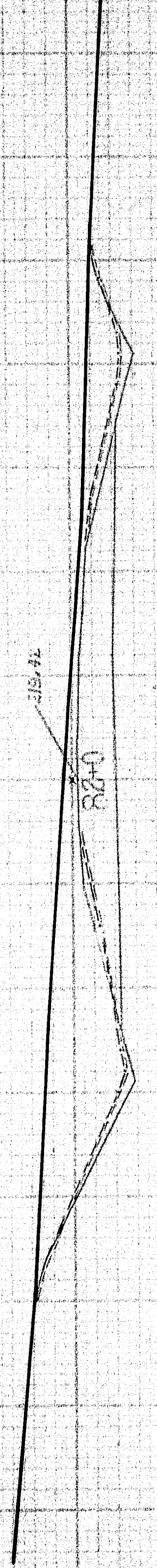
Sta 83+0 Limit of Work



RAMP F

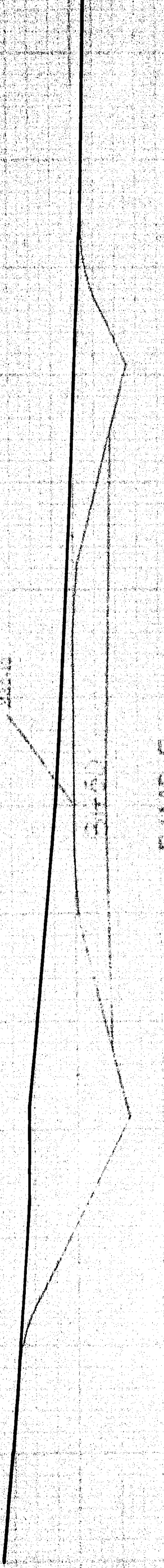


E=395



E=418

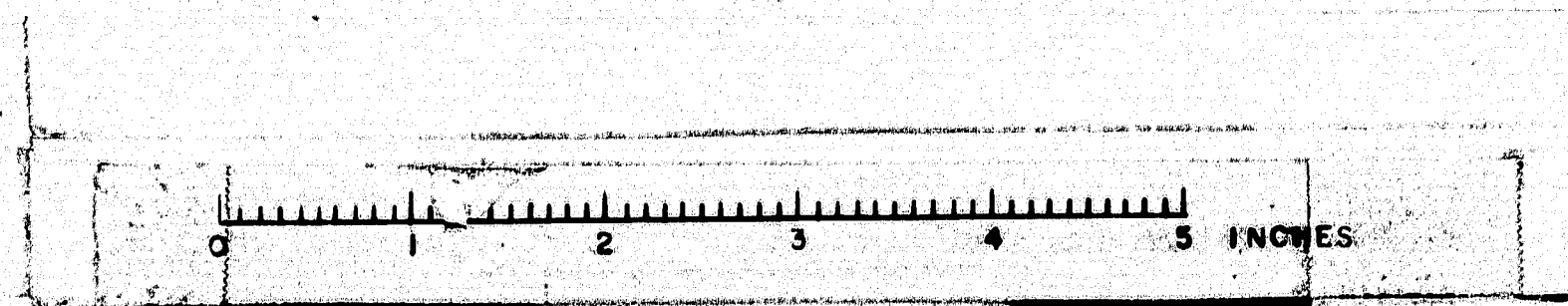
Sta 84+0 Limit of Work



RAMP E

PLATE 3 - CROSS SECTION OF P.N. 3.1.1
 ALL DIMENSIONS IN FEET
 CHANGES ENGINEERING COMPANY, INC.

75-156



198.93

85+00

Sta. 84+50 Ramp "H" - Limit of Work

198.17

84+50

EMBANKMENT 62 CU. YD.
EARTH EXCAVATION 33 CU. YD.

197.57

84+00

EMBANKMENT 83 CU. YD.
EARTH EXCAVATION 44 CU. YD.

197.45

83+50

51+00 Lt. Main St.
84+50 Lt. Ramp "H"
165' x 2' Ditch (Sodded)

197.82

82+93
C.B. TYPE "F" Rt. W/Bit Apron
15' x 41' RCP Roadway
15' x 16' ACCMP Lt.
Pipe Connector Lt.

83+00

Sta. 82+51.60 Ramp "H" - Sta. 52+31.54 Main St.

Ramp "H"

Sta. 83+05.71 Ramp "G" - Sta. 51+53.74 Main St.

200.69

82+50

82+00
30' x 12' ACCMP Lt. W/Metal Endwall
30' x 32' RCP Roadway
30' x 20' ACCMP Rt.
Pipe Connector Rt.
Hand Laid Hdrap at Outlet

EARTH EXCAVATION 761 CU. YD.

200.93

82+00

82+00 Lt.
"V" Ditch 3' Deep

EARTH EXCAVATION 513 CU. YD.

201.65

Sta. 81+50 Ramp "G" - Limit of Work

81+50

81+50
"V" Ditch 2' Deep Lt. & Rt.

81+00

Ramp "H"

REG. NO.	STATE	PROJECT NO.	SHEET NO.
1	MAINE	4-88-6184	5

FILE NO.	DATE	DR
7-10-60		
7-11-60		
7-12-60		
7-13-60		
7-14-60		
7-15-60		
7-16-60		
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7-31-60		

[illegible]